



City of Sandy

Agenda

Planning Commission Meeting

Meeting Location: City Hall- Council Chambers, 39250 Pioneer Blvd., Sandy, Oregon 97055

Meeting Date: Monday, August 26, 2019

Meeting Time: 7:00 PM

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1. ROLL CALL

2. APPROVAL OF MINUTES

- | | | |
|------|--|---------|
| 2.1. | May 28, 2019 Planning Commission Meeting Minutes
Planning Commission - 28 May 2019 - Minutes - Pdf | 3 - 9 |
| 2.2. | June 24, 2019 Planning Commission Meeting Minutes
Planning Commission - 24 Jun 2019 - Minutes - Pdf | 10 - 15 |
| 2.3. | July 22, 2019 Planning Commission Meeting Minutes
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3. REQUESTS FROM THE FLOOR - CITIZEN COMMUNICATION ON NON- AGENDA ITEMS

4. OLD BUSINESS

- | | | |
|------|--|----------|
| 4.1. | Public Hearing
18-030 DR/VAR/CUP Funtime RV | 25 - 187 |
|------|--|----------|

Based on the language of the code as written, staff recommends the Planning Commission hold a public hearing to take public testimony regarding the proposal. Staff recommends the Planning Commission deny the requested conditional use, 100 percent outdoor product display or storage of merchandise. Additionally, staff recommends Planning Commission deny the requested Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard.

The Planning Commission has also been presented with the alternative interpretation (Exhibit M) of the phrase “except as specifically authorized as a permitted accessory use” to consider. This interpretation provides an alternative that if supported by the

Commissioners, would provide justification for the Planning Commission to approve the requested conditional use, 100 percent outdoor product display or storage of merchandise. Should the Planning Commission choose to make this interpretation staff notes the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens the appearance of the RVs from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C). In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92 as well as adds a design element to enhance the pedestrian environment and “village” scale Chapter 17.90 strives for.

[18-030 DR/VAR/CUP Funtime RV - Pdf](#)

5. NEW BUSINESS

6. ITEMS FROM COMMISSION AND STAFF

7. ADJOURN



MINUTES
Planning Commission Meeting
Tuesday, May 28, 2019 City Hall- Council
Chambers, 39250 Pioneer Blvd., Sandy,
Oregon 97055 6:30 PM

COMMISSIONERS PRESENT: Don Carlton, Commissioner, Ron Lesowski, Commissioner, Hollis MacLean-Wenzel, Commissioner, Jerry Crosby, Commissioner, John Logan, Commissioner, Chris Mayton, Commissioner, and Todd Mobley, Commissioner

COMMISSIONERS ABSENT:

STAFF PRESENT: Kelly O'Neill, Planning Director, Emily Meharg, Associate Planner, and James Cramer, Associate Planner

MEDIA PRESENT:

1. Roll Call

2. Consent Agenda

2.1. Approval of Minutes

Motion: To approve minutes for March 19, 2019

Moved By: Commissioner Lesowski

Seconded By: Commissioner MacLean-Wenzel

Yes votes: Commissioners Carlton, Logan, Mobley and Chairman Crosby

No votes: None

Abstentions: Commissioner Mayton since he was excused from this meeting.

The motion passed.

Motion: To approve minutes for April 8, 2019

Moved By: Commissioner Ron Lesowski

Seconded By: Commissioner Logan

Yes Votes: Commissioner Don Carlton, Mobley, Mayton, and Chairman Crosby

No Votes: None

Abstentions: None

3. Requests From the Floor - Citizen Communication on Non- Agenda Items

4. PRESENTATION

Transit Master Plan Update (Andi Howell and Michelle Poyourow)

Poyourow said the first phase of the master plan has been completed which consisted of gathering information from the public. Poyourow said she analyzed 10 years' worth of data that consisted of ridership per service hour on all services that Sandy offers. She also reviewed potential high ridership possibilities.

Poyourow shared information regarding a stakeholders advisory group composed of 18 people that met in April of 2019 to discuss ridership goals vs. coverage goals for the City of Sandy. 11 out of the 18 people said higher ridership was a more important goal and the remaining 7 people felt higher coverage was more important. With regards to in-town trips, stakeholders saw fixed routes as a higher priority for higher ridership. The public input was requested via online and paper copy surveys regarding walking vs. waiting. The public expressed that there was a preference for service designed to minimize waiting by asking them to walk a little farther. With this information Poyourow went on to explain that this would mean that routes can be concentrated into fewer lines and in turn offer higher frequencies with possibilities of quicker trips.

In October a Final plan will be developed. Poyourow emphasized the importance of having future developments be transit oriented. Poyourow asked for Planning Commissioners to provide feedback or questions at this point.

Planning and Building Director Kelly O'Neill Jr. added that the Transportation Systems Plan (TSP) revision is in final phases of negotiations. O'Neill mentioned that Andi Howell, Transit Operations Director, would serve on the Technical Advisory Committee for the TSP revision.

5. OLD BUSINESS

6. NEW BUSINESS

6.1. 18-034 ANN Bird Annexation

Staff Report - 0142

Chairman Crosby opened the public hearing on File No. 18-034 ANN (Bird Annexation) at 7:15 p.m. Crosby called for any abstentions, conflicts of interest, ex-parte contact, challenges to the jurisdiction of the Planning Commission, or any challenges to any individual member of the Planning Commission.

Commissioner Todd Mobley recused himself due to conflict of interest. Crosby proceeded to call for the staff report.

Staff Report:

Associate Planner Emily Meharg summarized the staff report and addressed the

background, factual information, public comments staff received, applicable criteria, and presented a brief slide show. There was discussion and clarification about locations for utilities. Meharg finished her report with the summary and conclusion.

Applicant Presentation:

No applicant present.

Proponent Testimony:

None

Opponent Testimony:

None

Staff Recap:

None

Applicant Recap:

None

Discussion:

There was discussion on the Flood, Slope, Hazard (FSH) analysis. Commissioner Hollis MacLean-Wenzel asked if the annexation meant that the city would need to provide utilities. Meharg affirmed that it would not and that the future developer of the site would need to pay for the utilities.

Commissioner Carlton suggested to move forward and send the recommendation to City Council.

Commissioner Lesowski asked about pros and cons, taxes and fees, is there a downside for the City to annex?

It was pointed out that the property is inside the UGB and the question is only WHEN not IF the property will be made part of the city limits. Staff clarified that the property will be zoned Single Family Residential (SFR) and the FSH overlay will be applied.

Motion: To Close Public Hearing at 7:35 p.m.

Moved by: Commissioner Carlton

Seconded by: Commissioner Lesowski

No votes: None

Abstentions: None

The motion passed.

Motion: To recommend approval of File No.18-034 ANN (Bird Annexation) with the

recommendations as stated by Planning staff to City Council.

Moved by: Commissioner Carlton

Seconded by: Commissioner Mayton

Yes votes: Commissioner Carlton, Lesowski, MacLean-Wenzel, Logan, Mayton and Chairman Crosby

No votes: None

Abstentions: None (Todd Mobley had recused himself)

The motion passed.

Moved by Don Carlton, seconded by Chris Mayton

Staff Report - 0142

Staff recommends the Planning Commission hold a public hearing to take testimony on the proposed annexation and forward a recommendation to City Council. If the Planning Commission recommends approval of the annexation request, we suggest adding a condition of approval that the standards and criteria of the FSH overlay will apply to development of the property in accordance with the standards in effect at the time of development.

CARRIED.

6.2. Type III Design Review - Fun Time RV (18-030 DR/CUP/VAR)

Staff Report - 0143

Chairman Crosby opened the public hearing on File No. 18-030 Fun Time RV at 7:37 p.m. Crosby called for any abstentions, conflicts of interest, ex-parte contact, challenges to the jurisdiction of the Planning Commission, or any challenges to any individual member of the Planning Commission. Crosby proceeded to call for the staff report.

Staff Report:

Associate Planner James Cramer summarized the request which included a Design Review, a Conditional Use for 100 percent outdoor product display or storage of merchandise and a Special Variance to eliminate the required 50 percent building frontage standard. Cramer presented staff's recommendation of denial of the Conditional Use based on the limitations of Subsection 17.90.110(J)(1) as well as denial of the requested Special Variance based on the guiding principles of Subsection 17.90.00(C) and the design compatibility identified within Subsection 17.90.00(D).

Applicant Presentation:

The applicant's representative, Tracy Brown of Tracy Brown Planning Consultants, LLC, spoke in favor of the application and requested a continuance be granted for the applicant to have adequate time to respond to staff's recommendation. Mr. Brown added the site was previously approved for a similar use (i.e. the expansion of the existing Fun Time RV site to incorporate additional display of RVs for sale).

Discussion:

There was discussion on the language presented in Subsection 17.90.110(J)(1) and how it applies to the proposal. Commissioners questioned the previous approval mentioned by Mr. Brown to which Mr. O'Neill, Planning and Building Director, confirmed a previous approval was granted for a similar use however after confirming the interpretation with City Attorney, David Doughman, it appeared the code may not have been applied correctly. It was then requested that the Planning Commission be briefed or provided a copy of the previous approval for reference. A suggestion to incorporate additional elements, such as landscaping and fencing was presented to improve the pedestrian environment along the frontages of the site.

Motion: To Close Public Hearing at approximately 7:45 p.m.

Moved by: Not recorded

Seconded by: Not recorded

The motion passed.

Motion: To continue File No.18-030 DR (Fun Time RV Design Review) to the August 26, 2019 Planning Commission meeting.

Moved by: Commissioner Lesowski

Seconded by: Commissioner Logan

Yes votes: Commissioner Carlton, Lesowski, MacLean-Wenzel, Logan, Mayton, Mobley and Chairman Crosby

No votes: None

Abstentions: None

The motion passed.

Moved by Ron Lesowski, seconded by John Logan

Staff Report - 0143

*Staff recommends the Planning Commission hold a public hearing to take public testimony regarding the proposal. Staff recommends the Planning Commission **deny** the requested conditional use, 100 percent outdoor product display or storage of merchandise. Additionally, staff recommends Planning Commission **deny** the requested Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard.*

Contrary to the staff recommendation if the Planning Commission decides to approve the requested minor conditional use and/or special variance staff recommends it be conditioned on the applicant redesign the site's landscaping plan to include landscape strips to be a minimum of 7.5-feet wide or include a wheel stop to protect the landscaping. In addition, staff has concluded the proposed landscaping does not meet the intent of 17.92.50(A) therefore the applicant shall resubmit a landscape plan that provides a combination of trees, shrubs, or evergreen ground cover of at least 75

percent of the required landscaping area for staff review and approval.

CARRIED.

7. Items from Commission and Staff

7.1.

There was general discussion about current developments in Sandy. O'Neill mentioned that the solar farm west of Shorty's Corner is supposed to be screened from view. Continued brief discussion regarding Transit Master Plan with interest in moving people around town more effectively.

8. Adjourn

8.1.

Motion: To adjourn
Moved By: Commissioner MacLean-Wenzel
Seconded By: Commissioner Mobley
Yes votes: All Ayes
No votes: None
Abstentions: None
The motion passed.

Chairman Crosby adjourned the meeting at 7:49 p.m.



Chair, Jerry Crosby



Planning Director, Kelly O'Neill Jr

Draft



MINUTES
Planning Commission Meeting
Monday, June 24, 2019 City Hall- Council
Chambers, 39250 Pioneer Blvd., Sandy,
Oregon 97055 7:00 PM

COMMISSIONERS PRESENT: Don Carlton, Commissioner, Ron Lesowski, Commissioner, Hollis MacLean-Wenzel, Commissioner, Jerry Crosby, Commissioner, John Logan, Commissioner, and Chris Mayton, Commissioner

COMMISSIONERS ABSENT: Todd Mobley, Commissioner

STAFF PRESENT: James Cramer, Associate Planner

ATTORNEY PRESENT: David Doughman

1. Roll Call

2. Approval of Minutes

2.1. Planning Commission Minutes

Motion: To approve the minutes for June 10, 2019 with correction to the signature and Section 5.1

Moved By: Commissioner MacLean-Wenzel

Seconded By: Commissioner Mayton

Yes votes: Commissioners Carlton, MacLean-Wenzel and Logan

No votes: None

Abstentions: Chairman Crosby since he was excused from this meeting.

The motion passed.

3. Requests From the Floor - Citizen Communication on Non- Agenda Items

4. OLD BUSINESS

4.1. 18-057 DR/CUP Dutch Bros Design Review/Variance/Conditional Use Permit

Staff Report - 0135

Associate Planner James Cramer went over the staff recommendations and what has changed from the previous land use hearing.

Staff recommends the Planning Commission approve the proposed conditional use, a drive-through facility, as well as the proposed Mainor Partition with modifications as recommended in the staff report. Staff also recommends the approval of the three variances/adjustments with associated conditions.

Applicant Testimony:

Braden Bernards

504 NW 18th Ave, Portland, OR 97209

Mr. Bernards provided an overview of the project. He explained the proposed design and how it came about. Updated on issues discussed by the Commission and community at the April public hearing. Property has gone almost 30 years changing hands, yet it is still empty and nothing had come to fruition. After purchasing the site with due diligence, applicant met with staff and worked closely with them to develop a proposal. Staff suggested the patio and civic space for gathering and comfortable access for all transportation modes.

Bearing in mind the goal of developing the east-side of Sandy as a commercial corridor, they opted to partition the northside of the property rather than make it only parking area.

During the site design, they used an F-350 as a model for turning radius, as the Commission had requested. Applicant wanted to build as close to the property line as possible and were able to do so by including a drive-thru canopy that was designed with 'Sandy Style' heavy timbers and stone, with a pitched roof. Raised pedestrian crossings that will connect the sidewalk to the patio will ensure that pedestrians are within line of sight for motorists in the drive-thru.

Mr. Bernards addressed the changes and updates to the proposal including but not limited to the following:

- added bike parking and benches on Proctor Blvd.
- removed Hogan Cedars and Maples due to the height concern and worry about attracting beetles
- adjusted the stormwater system so it is solely located in the parking lot
- updated the lighting plan to utilize soft lighting
- Verified that rooftop equipment will be screened and not visible from the ground
- stated the existing trees on the property have been surveyed by Oregon Tree Care and 11 must be removed due to poor health
- submitted a traffic impact analysis finding there is no anticipated conflict between 7-11 traffic and the proposed ingress
- DEQ has submitted a letter with their findings
- the fence to the north of the property will be fixed to preserve a peaceful

backyard during and after construction

Mr. Bernards also explained the applicant has come to an agreement with All-County Surveyors.

It was explained that the applicant submitted the updated plans and narrative to City staff.

Following the changes, staff issued a new staff report with 28 conditions of approval. The applicant has agreed to all conditions.

PGE has determined they cannot compel Mattress World to switch to an underground system so the applicant will grant them an easement. Applicant will submit a formal letter from PGE.

Applicant expressed excitement to be a part of Sandy.

Damien Hall

Attorney with Ball Janik LLP, 101 SW Main Street Suite 1100, Portland, OR 97204

Commended the City staff and Mr. Bernards for working together on taking a site that has a lot of constraints and molding the objectives of the applicant and Sandy Style. Referred to the exhibits for more information on the updated narrative, conditional use and variance standards. The project provides significant screening and opportunities to improve the pedestrian experience. The applicant has gone above and beyond to ensure there is adequate room for stacking and parking so that the traffic on HWY 26 will not be impacted. There is a traffic study and analysis to confirm this. Since this location will be so visible from the HWY, they have made sure Sandy Style will be highly present in the building design.

Mr. Hall then explained that customers will interact with a person to order so there will be no noise from reader boards and that noise from cars on-site won't be distinguishable from regular traffic on HWY 26. DEQ will make sure stormwater is treated by "best practice" standards for handling any contaminated media and to make sure any of that contamination is not released into the environment. The applicant is working with DEQ to ensure air quality will be minimally impacted.

Went over the variances requested and how the plan meets the code.

1. Place core building 18 ft (code mandates 10 ft.) off the property line. The layout of the property allows for only one ingress and egress, at 18 feet setback the building will allow enough drive way to wrap around to accommodate the anticipated traffic. The canopy was offered as a compromise as it will embrace "Sandy Style" and will be within the setback.
2. Flat roof for the core building. The need for this arises from the small footprint of the building.
3. Reduce the required building frontage on the street. Need arises from the

small building footprint. The intent of the code shall be met because parking has been placed behind the building and the frontage is maximized by the patio and the drive-thru.

Proponent Testimony:

None

Opponent Testimony:

None

Staff Recap:

Associate Planner Crammer will consult with PGE and the wording of the condition related to overhead powerlines.

Motion to close the public hearing

Moved: Commissioner Lesowski

Seconded: Commissioner Carlton

Carried

Hearing closed at 7:55 pm.

Discussion:

Commissioner Carlton congratulated the applicant on the presentation and working with all stake-holders. Commissioner MacLean-Wenzel added that she was impressed with their ability to work with stakeholders in the community. Commissioner Mayton stated that he hadn't been enthusiastic after the first presentation, but his doubts have been laid to rest with this revised presentation and updated submittal.

Motion: to approve File No. 18-057 DR/CUP with the partition, conditional use, and each of the three variances.

Moved By: Commissioner Carlton

Seconded By: Commissioner Mayton

Yes votes: Commissioners Carlton, Lesowski, MacLean-Wenzel, Logan, Mayton and Chairmen Crosby

No votes: None

Absent: Commissioner Mobley.

The motion passed.

Moved by Don Carlton, seconded by Chris Mayton

Staff Report - 0135

Staff recommends the Planning Commission hold a public hearing to take public testimony regarding the proposal. Staff recommends the Planning Commission approve the proposed conditional use, a drive-through facility, as

well as the proposed Mainor Partition with modifications as recommended in this report including but not limited to the following (required order of submission and completion of improvements/inspections will be detailed in the final order):

Staff recommends the approval of the following 3 variances/adjustments with associated conditions:

a) Variance A: To construction primary structure setback 18 feet from the subject property's front (south) property line when Subsection 17.42.30 limits front setbacks to a maximum of 10 feet for properties within the C-1 zone district. Staff recommends the Planning Commission approve the requested special variance to increase the maximum front (south) yard setback to 18 feet with the condition the proposed, or approved alternative, landscaping, pedestrian access, civic space canopy and drive-through canopy are implemented to the development.

b) Variance B: To construct a flat roof with parapet features on the proposed primary structure when Subsection 17.90.110(C)(1) requires a 6:12 roof pitch on all new buildings with a span of 50-feet or less. Staff recommends the Planning Commission approve the Special Variance to the roof pitch standard and allow the primary structure to incorporate a flat roof with parapets with the condition the secondary features retain the proposed 6:12 roof pitch, or approved alternative, and the primary roof design complies with the standards set forth within Subsection 17.90.100(C)(8)(b) – Flat Roof.

c) Variance C: To reduce the required 50 percent building frontage standard of Subsection 17.90.110(D)(1) to 23 percent. Staff recommends the Planning Commission approve the Special Variance to reduce the building frontage to 23 percent with the condition that the proposed civic spaces, or approved alternative, be incorporated at time of development.

CARRIED.

5. NEW BUSINESS

None

6. Items from Commission and Staff

Next Meeting July 22

7. Adjourn

Motion: To adjourn

Moved By: Commissioner MacLean-Wenzel

Seconded By: Commissioner Logan
Yes votes: All Ayes
No votes: None
Abstentions: None
The motion passed.
Chairman Crosby adjourned the meeting at 8:04 p.m.



Chair, Jerry Crosby



Planning Director, Kelly O'Neill Jr

Draft



MINUTES
Planning Commission Meeting
Monday, July 22, 2019 City Hall- Council
Chambers, 39250 Pioneer Blvd., Sandy,
Oregon 97055 7:00 PM

COMMISSIONERS PRESENT: Don Carlton, Commissioner, Ron Lesowski, Commissioner, Hollis MacLean-Wenzel, Commissioner, Jerry Crosby, Commissioner, John Logan, Commissioner, Chris Mayton, Commissioner, and Todd Mobley, Commissioner

COMMISSIONERS ABSENT:

STAFF PRESENT: Kelly O'Neill, Planning Director and James Cramer, Associate Planner

MEDIA PRESENT:

1. Roll Call

2. Approval of Minutes

2.1. April 22, 2019 Planning Commission Meeting Draft Minutes.

Moved by John Logan, seconded by Hollis MacLean-Wenzel

Motion: To approve minutes for April 22, 2019 with the change Commissioner Mayton noted

Moved By: Commissioner Logan

Seconded By: Commissioner MacLean-Wenzel

Yes votes: All Ayes

No votes: None

Abstentions: None

The motion passed.

CARRIED.

3. Requests From the Floor - Citizen Communication on Non- Agenda Items

4. OLD BUSINESS

4.1. 18-026 ANN - Bloom Annexation

Staff Report - 0163

Crosby opened the public hearing on File No. 18-026 ANN (Bloom Annexation) at 7:03 p.m. Crosby called for any abstentions, conflicts of interest, ex-parte contact, challenges to the jurisdiction of the Planning Commission, or any challenges to any individual member of the Planning Commission. No challenges were made, and no declarations were made by the Planning Commissioners.

Staff Report:

Associate Planner James Cramer summarized the staff report and addressed the background, factual information, public comments staff received, applicable criteria, and presented a brief slide show.

Applicant Presentation:

No applicant present.

Proponent Testimony:

None

Opponent Testimony:

Donna Lazenby, 19271 Averill Parkway, Sandy, OR 97055

Concerns about traffic.

James Cusick, 38806 Jerger Street, Sandy, OR 97055

Concerns about an existing easement and stormwater runoff.

Staff Recap:

Cramer, Doughman and O'Neill addressed concerns from the two people making public comments.

Applicant Recap:

None

Discussion:

The Commissioners added some context to the public testimony. Commissioner Carlton explained that the Commissioners are reviewing the proposal, but not doing the development. Commissioner Lesowski stated that stormwater and traffic concerns will be evaluated further before the property is developed. Commissioner Mobley and MacLean-Wenzel thanked the public for attending the meeting and encouraged people to participate in the Transportation System Plan (TSP) process and Urban Forestry code modifications.

Motion: To Close Public Hearing at 7:27 p.m.

Moved by: Commissioner Carlton

Seconded by: Commissioner Mobley
No votes: None
Abstentions: None
The motion passed.

Motion: To recommend approval of File No.18-026 ANN (Bloom Annexation) with the recommendations as stated by Planning staff to City Council.

Moved by: Commissioner Lesowski

Seconded by: Commissioner Mayton

Yes votes: Commissioner Carlton, Lesowski, MacLean-Wenzel, Logan, Mobley, Mayton and Chairman Crosby

No votes: None

Abstentions: None

The motion passed.

Moved by Ron Lesowski, seconded by Chris Mayton

Staff Report - 0163

Staff recommends the Planning Commission hold a public hearing to take testimony on the proposed annexation and forward a recommendation to City Council. If the Planning Commission recommends approval of the annexation request, we suggest adding the following conditions:

1. *Prior to the future development of the subject property the standards and criteria of the Flood & Slope Hazard (FSH) Overlay District (Chapter 17.60) shall be applied to the subject property.*
2. *Prior to the future development of the subject property the Flood & Slope Hazard (FSH) Overlay District map shall be updated to include the subject property.*
3. *Prior to the future development of the subject property the development shall be limited to no more than 43 single family lots or 388 average daily trips.*
4. *Prior to the future development of the subject property an applicant, or representative, shall confirm the conditions associated with Case File No. Z0169-19-HL have been fulfilled (Exhibit Q).*

CARRIED.

4.2. 18-046 DR/VAR Stow-A-Way Mini Storage

Staff Report - 0164

Crosby opened the public hearing on File No. 18-046 DR (Stow-A-Way Mini Storage) at 7:38 p.m. Crosby called for any abstentions, conflicts of interest, ex-parte contact,

challenges to the jurisdiction of the Planning Commission, or any challenges to any individual member of the Planning Commission. Crosby stated he was not present at the first evidentiary hearing on June 10, 2019 but has reviewed the materials and meeting video and feels comfortable weighing in on the matter.

Staff Report:

Associate Planner James Cramer summarized the staff report and addressed the background, factual information, public comments staff received, applicable criteria, and presented a brief slide show.

Applicant Presentation:

Michael Robinson, Attorney with Schwabe Williamson and Wyatt, 1120 NW Couch Street, Portland, OR 97209

Mr. Robinson stated that the units do not allow for a pitched roof but does not believe that flat roofs as proposed impacts nearby properties. The property is heavily screened, and it is very difficult to see the storage units from Highway 26. He stated the purpose of the I-2 zoning district is to have less visible properties and development. Robinson said the area for the storage units is within 50 feet of the top of bank but is in the same location as previous outdoor storage. He stated the applicant is fine with restoring the area to the west of the storage units with native vegetation and installing a fence.

Proponent Testimony:

None

Opponent Testimony:

Susan Drew, 37770 HWY 26, Sandy, OR 97055

Ms. Drew stated she was happy to hear that staff is recommending the storage units meet the setback requirements to the top of bank for No Name Creek. Also, she would like people to apply for requests prior to coming into City Hall to ask for forgiveness.

Staff Recap:

Cramer and O'Neill commented on Mr. Robinson's letter that he submitted and also the deviations, variances, and adjustments that are being requested.

Applicant Recap:

Mr. Robinson explained the map and setbacks to top of bank. He suggested that additional vegetation is installed in the area not impacted by the paved surface or buildings. Robinson said he agrees with staff that this site lends itself to flat roofs especially because of the location of the buildings. He also stated that a decision tonight will not set precedence for future projects. The top of bank setback is a line and the addition of the buildings did not impact existing native vegetation.

Applicant - Lacy Renard, 22611 Van Curen Road, Eagle Creek, OR 97022

Ms. Renard explained the reason the structures were installed without seeking planning and building approval. She said that the existing storage facility was consistently full and she had a list of people they were turning down for storage units. She stated that the material that was previously on gravel within 50 feet of No Name Creek was potentially more harmful to the wetland than the storage units would be.

Susan Drew asked if the units are on a foundation. Ms. Renard stated the units are on concrete runners or footings.

Commissioner Mayton asked if the containers are mobile. Ms. Renard stated the units are not permanently fixed to the ground.

Discussion:

Commissioner Mayton asked if we go back in time and act like the containers are not there would we allow a modification to the top of bank setback requirements? Commissioner Carlton stated that if there is a space to install the structures at 50 feet from the top of bank then yes we would require the structures at 50 feet from the top of bank. Mayton then stated he agrees with staff for the most part in regard to the setback from top of bank, but he does not care about the roof slope. Carlton stated the Sandy Style colors should apply. Staff explained the I-2 zoning district does not require adherence to the Sandy Style color palette.

Lesowski asked if the existing fence is going to be modified per the submitted site plan? Is Exhibit C in the packet correct with the proposed fence location?

Applicant - Chris Warnock, 37330 Ruben Lane, Sandy, OR 97055

The fence has been there since about 1992. The fence shown on Exhibit C (site plan) to the south of X-C is not correct. Mr. Robinson added that an additional condition could be that Exhibit C is revised to reflect the existing improvements and proposed improvements.

Summary of main items during discussion:

Design Deviations

Approve existing siding - No concerns from Commissioners

Approve not having a primary entrance facing a public street - No concerns from Commissioners

Approve not having a connection between the right-of-way and building interior - No concerns from Commissioners

Approve the elimination of sheltered overhangs - No concerns from Commissioners

Deviation to lighting standards - No longer applicable as the applicant has stated they will comply with lighting regulations in Chapter 15 of the Sandy Municipal Code.

Special Variances

Setback of 30 feet to the property line - No concerns with from Commissioners

Deny flat roofs as constructed - The Commissioners stated they are fine with the flat roofs and determined they were not in favor of staff's recommendation to modify the roofs.

FSH Overlay Adjustment

Deny setback adjustment to the Top of Bank for No Name Creek – Commissioners Carlton and Mayton both stated that they are not comfortable allowing the setback adjustment as it does not meet the code criterion. O'Neill then provided a compromise and suggested the following adjustment: X-B can remain, new asphalt as proposed could be installed, and the portion of X-C in the setback would need to be moved and replaced with native vegetation. Warnock stated they could definitely relocate three of the storage units (8 feet wide each) from X-C in another section of the site outside of the Restricted Setback area to No Name Creek and plant vegetation as recommended.

Motion: To Close Public Hearing at 9:28 p.m.

Moved by: Commissioner Lesowski

Seconded by: Commissioner Maclean-Wenzel

No votes: None

Abstentions: None

The motion passed.

Motion: To approve File No.18-046 DR (Stow-A-Way) with the following motion:

Approve the design deviations nos. 1-4 as presented in the staff report, approve a special variance for the property line setback and replace gravel located in the Restricted Development Area (RDA), approve a special variance for a flat roof, and approve an adjustment to the FSH Overlay by allowing the relocation of the three most west units of X-C elsewhere on site but the adjustment allows for X-B and asphalt in the RDA with the condition that additional gravel will be replaced with native vegetation. The motion also included the requirement to improve the area between new units and existing pavement with asphalt/concrete and provide an internal circulation plan detailing how the site will limit access to Buildings X-A, X-B, and X-C. All other conditions and findings in the staff report are applicable.

Moved by: Commissioner Carlton

Seconded by: Commissioner Logan

Yes votes: Commissioner Carlton, Lesowski, MacLean-Wenzel, Logan, Mobley, Mayton and Chairman Crosby

No votes: None

Abstentions: None

The motion passed.

Moved by Don Carlton, seconded by John Logan

Staff Report - 0164

Design Deviations:

1. **Approve** the requested Design Deviation from Subsection 17.90.130(C)(3).
2. **Approve** the requested Design Deviation from Subsection 17.90.130(E)(1) to allow the development to not include a primary entry facing a public street or designated pedestrian way.
3. **Approve** the requested Design Deviation from Subsection 17.90.130(E)(3) to not include an entrance connecting directly between the right-of-way and the building interior.
4. **Approve** the requested deviation to eliminate sheltered overhangs or porticos at pedestrian entrances for Units A, B and C
5. **N/A.** The applicant has indicated within the submitted narrative (Exhibit N) the criteria of Subsection 17.90.130(H) will be met. Condition 2 in this staff report requires the applicant to provide the materials needed to determine compliance which shall be completed prior to the issuance of the final Certificate of Occupancy.

Requested Special Variances:

1. **Approve** the requested special variance (referenced as Variance A within Staff Analysis) to reduce the front (west) yard setback for Unit B to 18-feet and **approve** Unit C to be setback 24-feet from the front (west) property line **with the condition** the applicant replaces the gravel located within the Restrictive Development Area (adjacent to Unit C) with native vegetation and erect a fence along the newly identified Restrictive Development Area setback (25 feet) to discourage future encroachment and bring the site closer into compliance.
2. Staff recommends the Planning Commission make one of the following conditions regarding Variance B:
 - A. **Deny** the request to eliminate the requirement of Subsection 17.90.130(D), or
 - B. **Approve** a special variance to reduce the required roof pitch **with the condition** the structures (Units A, B and C) incorporate sloped roofs with pitches equal to the existing structures on site (IE congruent with the existing Stow-A-Way Mini Storage structures).

Requested FSH Overlay Adjustment:

1. Staff recommends the Planning Commission **deny** the Type III FSH Overlay Adjustment request as criterion 1, 2 and 5 have not been met.

Should the Planning Commission choose to approve the request staff would recommend the approval be **conditioned** upon the applicant

replacing the gravel located within the Restrictive Development Area with native vegetation and erect a fence along the newly identified Restrictive Development Area setback (25 foot) to discourage future encroachment and bring the site closer into compliance.

Staff recommends approval be **conditioned** on the applicant completing option A or B below:

- A. *Improve all driveways, aisles and turnarounds associated with onsite maneuvering for both the existing and proposed storage units/buildings located on the subject property, or*
- B. *Provide an improved aisle between the proposed pavement and existing pavement along with an internal circulation plan as to how the site will limit access to Buildings X-A, X-B and or X-C to the improved aisle(s).*

CARRIED.

5. NEW BUSINESS

6. Items from Commission and Staff

6.1.

O'Neill presented upcoming items on the August 26th Planning Commission meeting. Carlton asked questions about the new House Bill 2001 regarding housing.

7. Adjourn

Motion: To adjourn

Moved By: Commissioner MacLean-Wenzel

Seconded By: Commissioner Mobley

Yes votes: All Ayes

No votes: None

Abstentions: None

The motion passed.

Chairman Crosby adjourned the meeting at 9:45 p.m.





Chair, Jerry Crosby



Planning Director, Kelly O'Neill Jr

Draft



Staff Report

Meeting Date: August 26, 2019
From James Cramer, Associate Planner
SUBJECT: 18-030 DR/VAR/CUP Funtime RV

Background:

At the May 28, 2019 Planning Commission meeting File No. 18-030 DR/VAR/CUP Funtime RV was approved to be continued to the August 26, 2019 Planning Commission meeting. Jerry Fisk submitted an application seeking approval for a minor conditional use permit to increase the subject property's outdoor product display/storage of merchandise to 100 percent when Subsection 17.42.10(C)(8) limits the amount of outdoor product display/storage on a site to 10 percent as an accessory use. The application has been elevated to a Type III review and must obtain Planning Commission's approval. In addition, no building is proposed to be on the subject property and the applicant seeks a Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard. The site will be used to store and display recreational vehicles in association with the retail establishment Funtime RV on the adjacent property to the west.

Recommendation:

Based on the language of the code as written, staff recommends the Planning Commission hold a public hearing to take public testimony regarding the proposal. Staff recommends the Planning Commission deny the requested conditional use, 100 percent outdoor product display or storage of merchandise. Additionally, staff recommends Planning Commission deny the requested Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard.

The Planning Commission has also been presented with the alternative interpretation (Exhibit M) of the phrase "except as specifically authorized as a permitted accessory use" to consider. This interpretation provides an alternative that if supported by the Commissioners, would provide justification for the Planning Commission to approve the requested conditional use, 100 percent outdoor product display or storage of merchandise. Should the Planning Commission choose to make this interpretation staff notes the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens the appearance of the RVs from the public street while enhancing the pedestrian environment and adding to the intended "village" scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C). In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92 as well as adds a design element to enhance the pedestrian environment and "village" scale Chapter 17.90 strives for.

Code Analysis:

See attached Revised Staff Report

Budgetary Impact:
None.

**PLANNING COMMISSION
REVISED STAFF REPORT
TYPE III LAND USE PROPOSAL**

~~Strikethrough~~ shall represent removed/updated text.
Red Text shall represent new text.

SUBJECT: File No. 18-030 DR/VAR/CUP Funtime RV

AGENDA DATE: ~~May 28, 2019~~ **August 26, 2019**

DEPARTMENT: Planning Division

STAFF CONTACT: James A. Cramer, Associate Planner

Application Submitted: July 10, 2018
Additional Submittal Items: February 7, 2019
Application Complete: March 5, 2019
120-Day Deadline: July 3, 2019

EXHIBITS:

Applicant's Submittals

- A. Land Use Application
- B. Notification List and Mailing Labels
- C. Project Narrative
- D. Civil Plans
 - Sheet C1: Cover Sheet
 - Sheet C2: Existing Conditions/Demo Plan
 - Sheet C3: Site, Grading Plan & Landscaping Plan
 - Sheet C4: Concept RV Display Layout
 - Sheet C5: Beers Avenue Plan & Landscaping Plan
 - Sheet C6: Utility Plan
 - Sheet C7: Details – City of Sandy
 - Sheet C8: Details – City of Sandy
 - Sheet C9: Details – ODOT
 - Sheet C10: Details – Detention Facility
- E. Preliminary Stormwater Report
- F. Photometric Analysis and Fixture Cut Sheet
- G. Pre-Application Notes and Staff Correspondence

Agency Comments

- H. City Engineer (March 22, 2019)
- I. Oregon Department of Transportation ODOT (March 22, 2019)
- J. Public Works Director (March 25, 2019)

Supplemental Documents Provided by Staff

- K. File No. 09-001 CUP Final Order
- L. Final Order 14-026 (Fred's RV Variance)
- M. City Attorney Interpretation

Applicant's Submittal Continued

N. Civil Plans

- Sheet C1: Cover Sheet
- Sheet C2: Existing Conditions/Demo Plan
- Sheet C3: Site, Grading Plan & Landscaping Plan
- Sheet C4: Concept RV Display Layout
- Sheet C5: Beers Avenue Plan & Profile
- Sheet C6: Utility Plan
- Sheet C7: Detail Sheet – City of Sandy
- Sheet C8: Detail Sheet – City of Sandy
- Sheet C9: Detail Sheet – ODOT
- Sheet C10: Detail Sheet – Site
- Sheet C11: Detail Sheet – Detention Facility

I. BACKGROUND

A. PROCEEDING

Type II Minor Conditional Use, Type II Design Review, and one Type III Special Variance.

The proposal includes the three applications/requests identified above, which vary in review types (Type II and Type III) therefore the Planning & Building Director has elevated all applications to the highest number procedure for review (Type III) per the allowance of Subsection 17.18.00.

B. FACTUAL INFORMATION

1. PROJECT NAME: Funtime RV Design Review/Special Variance/Conditional Use Permit
2. APPLICANT/REPRESENTATIVE: Tracy Brown with Tracy Brown Planning Consultants, LLC
3. OWNER: CVP – Jerry Fisk with Red One Holdings, LLC
4. LEGAL DESCRIPTION: T2S R4E Section 13 BC, Tax Lot 3900
5. SITUS ADDRESS: 38477 Proctor Blvd.
6. PROPERTY LOCATION: Located on the northwest corner on the Proctor Blvd./US HWY 26 and Beers Ave. intersection.
7. PROPERTY SIZE: The overall site is approximately 13,985 square feet (0.32 acres).

8. PROPOSAL: Jerry Fisk submitted an application seeking approval for a minor conditional use permit to increase the subject property's outdoor product display/storage of merchandise to 100 percent when Subsection 17.42.10(C)(8) limits the amount of outdoor product display/storage on a site to 10 percent as an accessory use. The application has been elevated to a Type III review and must obtain Planning Commission's approval. In addition, no building is proposed to be on the subject property and the applicant seeks a Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard. The site will be used to store and display recreational vehicles in association with the retail establishment Funtime RV on the adjacent property to the west.
9. COMPREHENSIVE PLAN DESIGNATION: Retail/Commercial
10. ZONING DISTRICT DESIGNATION: C-1, Central Business District
11. SERVICE CONSIDERATIONS: The proposal does not include connecting to any public facilities except for stormwater system. The proposed stormwater system will connect to the public line located near the corner of Pleasant Street and Beers Avenue. Due to the nature of the development the applicant does not propose making improvements to or connecting to water, sanitary sewer or fiber services. Staff finds this to be acceptable at this time and notes that any future development on this site will be required to meet the standards of Chapter 17.84.60.
12. RESPONSE FROM GOVERNMENTAL AGENCIES, UTILITY PROVIDERS, CITY DEPARTMENTS AND THE GENERAL PUBLIC:
 - a. City Manager – No comments received
 - b. Public Works Director – Exhibit J
 - c. Sandy Police Department – No comments received
 - d. Transit Director – No comments received
 - e. City Engineer– Exhibit H
 - f. PGE – No comments received
 - g. Fire District No. 72 – No comments received
 - h. Oregon Department of Transportation (ODOT) – Exhibit I

C. APPLICABLE CRITERIA:

Sandy Municipal Code (SMC); 17.12 Procedures for Decision Making; 17.18 Processing Applications; 17.20 Public Hearings; 17.22 Notices; 17.28 Appeals; 17.42 Central Business District, C-1; 17.66 Adjustments and Variances; 17.68 Conditional Uses; 17.84 Improvements with Development; 17.90 Design Standards; 17.92 Landscaping and Screening; 17.98 Parking, Loading, and Access Requirements; and Chapter 15.30 Dark Sky Ordinance

D. BACKGROUND INFORMATION/HISTORY

The subject property was issued a permit (Permit No. 1010) in September of 1999 for the demolition of a house in conjunction with a Fire Department controlled burning. Since the home's demolition the site has been vacant and is generally flat with an approximate 7-foot elevation between the southeast and northeast corners of the property. Currently the site has a maintained grass covering throughout the property and a split-rail fence extending the length of the south property line fronting Proctor Blvd./US HWY 26. Additionally, the site has

incorporated a wire fencing material with wooden posts extending the length of the east property line fronting Beers Ave. which is currently unimproved.

The adjacent property to the west, 38415 Proctor Blvd./US HWY 26, received a Conditional Use Permit (File No. 09-001 CUP, Exhibit K) to operate an automobile, truck, marine and recreational vehicle repair facility which included retail sales of pre-owned automobiles, trucks, and parts and accessories. The business associated with this approval was known as Fred's RV. Funtime RV has since taken ownership of the site and continued the use. **On December 19, 2014 the Planning Commission approved a special variance for the subject property to display and sell RV's for a maximum period of 12 months on the existing unimproved property to be associated with Fred's RV (File No. 14-026, Exhibit L). Condition B.1 stated, "Approval of the Special Variance expires 12 months from the date of issuance of this Final Order (December 19, 2015). Any use of the property for RV sales or any other use beyond this date requires submittal and approval of a new variance application or design review approval and completion of required onsite improvements."** The existing owner of Funtime RV has made the request for a Minor Conditional Use permit to increase the subject property's outdoor product display/storage of merchandise to 100 percent as well as a Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard. The revised proposal includes landscaping and an enhanced fence design on the south and east property lines to provide a buffer between the public right-of-way and the proposed use of the property, outdoor display/storage of merchandise.

Per the submitted narrative, the project is *"proposed to expand the area of recreational vehicle parking and storage for the Funtime RV business located on the four lots directly west of the subject property."* After consulting with the City's attorney, staff determined the property would have to complete one of the following options:

- A. Review the subject property as a separate site and request a special variance to Subsection 17.90.110(D)(1) eliminating the required building frontage for the site, or
- B. Review the property as a whole including the four (4) contiguous properties to the west under the same ownership (i.e. the existing Funtime RV site), and apply the existing development standards to the entire site.

When presented to the applicant and their representative, option A was the chosen path.

E. PROCEDURAL CONSIDERATIONS

This request includes a Type II Minor Conditional Use, Type II Design Review, and one Type III Special Variance. The proposal includes the three applications/requests identified above, which vary in review types (Type II and Type III) therefore the Planning Director has elevated all applications to the highest number procedure for review (Type III) per the allowance of Subsection 17.18.00.

Prior to the May 28, 2019 Planning Commission hearing the application completed the following steps to ensure the application met local and state requirements for a complete application:

- July 10, 2018 Application Received
- July 12, 2018 Incompleteness Letter Sent

- August 15, 2018 Pre-Application Meeting Held as required by the Director
- August 28, 2018 Incompleteness Letter Sent
- November 21, 2018 Meeting held with Jerry Fisk (owner)
- February 7, 2019 Resubmittal Received
- March 5, 2019 Completeness Letter Sent

At the May 28, 2019 Planning Commission meeting the applicant's representative, Tracy Brown with Tracy Brown Planning Consultants, requested this application (18-030 DR/CUP/VAR) be continued to a future Planning Commission meeting. After hearing an abbreviated staff presentation, an applicant presentation and public testimony was sought, the Planning Commission voted unanimously (7:0) to keep the hearing open and continue it to the August 26, 2019 Planning Commission hearing.

Staff met with the property owner, the manager of the Sandy Funtime RV location and Tracy Brown on June 19, 2019, per the request of Mr. Brown, to discuss the proposal, staff's interpretation and alternatives to consider. At this meeting staff recommended the applicant consider proposing additional frontage improvements to enhance the pedestrian environment to better conform to the design guidelines and compatibility standards identified within Chapter 17.90 of the Sandy Development Code.

F. PROCEDURAL CONSIDERATIONS

This file is being processed as a Type III Design Review because there is a special variance associated with the special variance. Notification of the proposal was mailed to property owners within 300 feet of the subject property and to affected agencies on April 30, 2019 with a courtesy notice sent August 14, 2019. A legal notice was published in the Sandy Post on Wednesday, May 8, 2019. Since the meeting was continued to a date certain the second hearing date was not published in the Sandy Post.

II. ANALYSIS OF CODE COMPLIANCE

CHAPTER 17.42 – CENTRAL BUSINESS DISTRICT (C-1)

This district is intended to provide the community with a mix of retail, personal services, offices and residential needs of the community and its trade area in the city's traditional commercial core. This district is not intended for intensive automobile or industrial uses. This district is intended to provide the principal focus for civil and social functions within the community.

This commercial district is intended for civic uses and to provide all basic services and amenities required to keep the downtown the vital center of our community. While the district does not permit new low-density building types, it is not intended to preclude dwelling units in buildings containing commercial activities. All development and uses shall be consistent with the intent of the district, as well as compatible with the space, access and exposure constraints and opportunities of the central city.

RESPONSE: *The proposal includes developing the subject property solely as outdoor product display or storage of merchandise for recreational vehicles. As noted within this subsection, the C-1 zone district is not intended for intensive automobile or industrial use. It can be interpreted that this section of the code refers to uses that reasonably produce an increase in the number of vehicular trips and/or*

increase in vehicular traffic. The submitted narrative (Exhibit C, Page 3) includes analysis from the applicant regarding “intensive automobile” uses. Within this analysis “intensive automobile” uses is interpreted to include “those uses such as drive-through restaurants or coffee kiosks that generate high traffic volumes.” Within the analysis of Subsection 17.68.20(C), staff confirms it is unlikely the proposed use, outdoor product display or storage of merchandise, will generate additional trips to the site and therefore analysis of traffic impacts was not required of the applicant.

*~~In addition, after~~After review of the permitted uses within Subsection 17.42.10, staff has determined an **alternative interpretation** ~~it can be interpreted~~ that “intensive automobile” use also refers to uses that are directly related to auto oriented uses such as auto dealerships. This analysis is supported in that all commercial uses allowed within the C-1 zone district require a building as detailed within Subsection 17.42.10 of this report. As detailed in this subsection, all development and uses shall be consistent with the intent of the district therefore staff finds that the proposed use of the site solely as outdoor display/storage is not consistent with the intent of the district and therefore, should be denied.*

Additional analysis on compliance with development intent can be found within Chapters 17.66, 17.68 and 17.90 of this report.

17.42.10 PERMITTED USES

RESPONSE: *Within the submitted narrative (Exhibit C) the applicant has identified the intended primary use of subject property is to be for “recreational vehicle parking and storage for the Funtime RV business located on the four lots directly west of the subject property” Subsection 17.42.10(B)(1)(a) permits retail sales of recreational vehicles in buildings with up to 30,000 square feet of gross floor area. After consultation with the City attorney, staff determined this standard finds that a building is required to be associated with the retail sales of recreational vehicles. In other words, the sale of recreational vehicles without an associated building is not a primary use permitted outright.*

Additionally, Subsection 17.42.10(C)(8) identifies outdoor product display or storage of merchandise covering no more than 10 percent of the total retail sales area as an accessory use permitted outright. To increase the amount of permitted outdoor product display or storage of merchandise covering greater than 10 percent of the total retail sales area, the applicant is required to obtain approval of a Minor Conditional Use. The applicant has applied for a Minor Conditional Use for to increase the percentage of outdoor product display or storage of merchandise to 100 percent, further analysis of this request can be found within Chapters 17.42.20 and 17.68 of this report.

17.42.20 MINOR CONDITIONAL USES AND CONDITIONAL USES

RESPONSE: *Outdoor product display or storage of merchandise covering greater than 10 percent of the total retail sales area is identified as a Minor Conditional Use within Subsection 17.42.20(A)(3). To be approved, an applicant is required to complete a Type II approval process and meet the regulations of Chapter 17.68 – Conditional Uses, which are represented within this staff report. Section 17.90.110 states that if “a conflict exists between the requirements of Chapter 17.90 and any other code provision, Chapter 17.90 shall prevail.*

While Subsection 17.42.20(A)(3) provides the opportunity for a property owner to request an increase in outdoor display or storage of merchandise, Subsection 17.90.110(J) speaks to external storage and screening requirements associated with properties within the C-1 zone district. The intent of this section

is to promote land use compatibility and aesthetics, particularly where development abuts public spaces. Subsection 17.90.110(J)(1) states that exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited.

As described above, the C-1 zone district permits as an accessory use “outdoor display or storage of merchandise covering no more than 10% of the total retail sales area.” Therefore, 17.90.110(J)(1) in isolation would allow a modest 10% of outdoor storage on the site as that amount is “specifically authorized as a permitted accessory use” pursuant to 17.42.10(C)(8). However, that is all that 17.90.110(J)(1) would permit. Any amount of storage beyond that modest 10% accessory threshold “is prohibited” under 17.90.110(J)(1). This is true despite the fact that the C-1 zone would allow storage exceeding 10% as a minor conditional use, as the applicant proposes here.

There are two reasons for this. First, the C-1 zone district allowance for more than 10% outdoor storage through a conditional use process is in conflict with Chapter 17.90.110(J)(1). Again, 17.90.110(J)(1) prohibits outdoor storage “except as specifically authorized as a permitted accessory use.” It does not say “or as may be authorized through a conditional use permit.” As mentioned above, if there is a conflict between Chapter 17.90 and another section of the code (here, Chapter 17.42), the standard in Chapter 17.90 prevails.

Second, under any reading of 17.90.110(J)(1), the storage an applicant seeks must be “accessory” to the primary permitted use of the site. As it relates to this site-specific application, the applicant proposes the retail sales area on this site to consist of 100 percent outdoor storage. On this site, the storage is not an accessory use but rather the primary, exclusive use the applicant proposes.

For these reasons, the application for a minor conditional use should be denied.

The above interpretation was completed based on the language of the code as written. According to the City Attorney when an inconsistency exists between criteria in a development code, courts will defer to a local government’s interpretation if it is “plausible” considering the express language of the relevant provisions and the purposes behind them. This standard recognizes that, in many cases, no single interpretation of inconsistent code provisions is correct to the exclusion of all other interpretations. As such, the Planning Commission could consider another interpretation of 17.90.110(J), even if it is not necessarily the “most plausible.”

In this case, the inconsistency involves the phrase “except as specifically authorized as a permitted accessory use” and the fact that any inconsistency is resolved in favor of Chapter 17.90’s criteria. Staff and the City Attorney believe that the most plausible interpretation is the one offered above: the code at 17.42.10(C)(8) specifically authorizes outdoor storage not exceeding 10 percent as a permitted accessory use. Therefore, pursuant to that interpretation, Section 17.90.110(J) prohibits outdoor storage exceeding 10 percent, as is the case in this application.

However, another interpretation could be that the phrase “except as specifically authorized as a permitted accessory use” means not as code language specifically authorizes, but instead as the Planning Commission or City Council specifically authorize in a given case. Under that interpretation, the Planning Commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the Planning Commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use.

17.42.30 DEVELOPMENT STANDARDS

Commercial	
Lot Area	No minimum
Lot Dimension	No minimum
Setbacks	No minimum; maximum 10 ft.
Lot Coverage	No maximum
Landscaping	10% minimum (includes required civic space in Section 17.90.110.)
Structure Height	45 ft. maximum
Off-Street Parking	See Chapter 17.98
Design Review Standards	See Section 17.90.110

RESPONSE: *The proposal includes a Special Variance request to eliminate the required on-site building therefore no structures are proposed in association with the intended use, outdoor product display and storage of merchandise. The ~~submitted~~ updated landscape plan (Exhibit D, Sheet C3) (Exhibit N, Sheet C3) indicates the site will incorporate 10 percent (~~1,398~~ 1,399 square feet) of landscaping on the site.*

CHAPTER 17.66 – ADJUSTMENTS AND VARIANCES

17.66.60 VARIANCES

Variations are a means of requesting a complete waiver or major adjustment to certain development standards. They may be requested for a specific lot or as part of a land division application. The Type II variance process is generally reserved for major adjustments on individual lots, while variations to development standards proposed as part of a land division are processed as a Type III application (requiring a public hearing).

RESPONSE: *The applicant has requested a Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard.*

17.66.80 TYPE III SPECIAL VARIANCES

The Planning Commission may grant a special variance waiving a specified provision under the Type III procedure if it finds that the provision is unreasonable and unwarranted due to the specific nature of the proposed development. In submitting an application for a Type III Special Variance, the proposed development explanation shall provide facts and evidence sufficient to enable the Planning Commission to make findings in compliance with the criteria set forth in this section while avoiding conflict with the Comprehensive Plan.

One of the following sets of criteria shall be applied as appropriate.

- A. The unique nature of the proposed development is such that:
 - 1. The intent and purpose of the regulations and of the provisions to be waived will not be violated; and

2. Authorization of the special variance will not be materially detrimental to the public welfare and will not be injurious to other property in the area when compared with the effects of development otherwise permitted.
- B. The variance approved is the minimum variance needed to permit practical compliance with a requirement of another law or regulation.
- C. When restoration or replacement of a nonconforming development is necessary due to damage by fire, flood, or other casual or natural disaster, the restoration or replacement will decrease the degree of the previous noncompliance to the greatest extent possible.

Special Variance Request:

Subsection 17.90.110(D)(1) requires each building to be oriented to a public street or civic space. This standard is met when at least 50 percent of the subject site's street frontage is comprised of building(s) placed within 10 feet of a sidewalk or an approved civic space and not more than 20 percent of the off-street parking on a parcel as required by SDC 17.98, tract or area of land is located between a building's front façade and the adjacent street(s).

Request: There do not appear to be any unique characteristics of the subject property that differ from neighboring properties and the site planning is of the applicant's making so a Type II Variance request would have to be denied. Therefore, the applicant requests a Type III Special Variance to eliminate the building that 17.90.110(D)(1) requires in order to meet the frontage and orientation standards of this subsection.

- A. The unique nature of the proposed development is such that:
1. The intent and purpose of the regulations and of the provisions to be waived will not be violated; and
 2. Authorization of the special variance will not be materially detrimental to the public welfare and will not be injurious to other property in the area when compared with the effects of development otherwise permitted.

RESPONSE: *As identified by the applicant within the submitted narrative (Exhibit C) and within Subsection 17.90.110(D) the intent of the building orientation is to maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style.*

Subsection 17.90.00 identifies objectives when evaluating Design reviews. Subsection 17.90.110(B) states an objective is to encourage aesthetically pleasing development. To support this objective 17.90.00(C) identifies five guiding principles to Sandy Style. Of the five items, four (Items No. 1, 3, 4 & 5) speak to building design, orientation, materials, landscaping, architectural style and/or building practices. Subsection 17.90.110(D) identifies elements incompatible with the Sandy Style and permits a reviewing body to deny, or require modifications to, a project when any of the items (1-11) identified within this subsection are found in a proposal.

As identified within 17.42.10 of this report, the C-1 zone district requires a building associated with the retail sales of the recreational vehicle sales. Within the submitted narrative (Exhibit C) the applicant has identified the intended primary use of subject property is to be for “recreational vehicle parking and storage” for recreational vehicles. To accomplish this the proposal includes paving 90 percent (12,586.5 square feet or .29 acres) of the site to use as a display area. Criteria 17.90.00(D)(3) identifies that excessive surface parking lot paving is incompatible with Sandy Style. Additionally, Subsection 17.90.00(D)(11) identifies outdoor storage as incompatible with the Sandy Style. The incompatibility of the applicant’s proposal for outdoor storage is further supported by 17.90.110(J)(1), as discussed above.

The proposal includes incorporating a 5-foot wide planter strip along the east and south property lines/frontages with a 25.5-foot by 25.5-foot planter area located in the subject property’s southwest corner. Within these landscaping areas the proposal includes a total of 9 Dwarf Burning Bushes, 3 J.P.M. Rhododendrons, both proposed to be 5-gallon plantings as well as 5, 7-foot tall or 1.5-inch caliber Kousa Dogwood trees. The areas not occupied by these plantings will consist of Kinnikinnick Groundcover and bark dust. In addition, the Landscape Plan (Exhibit N, Sheet C3) includes the installation of an ornamental fence. The proposed fence includes square column wraps at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed columns are to be wrapped with a stone veneer in Bucks County color which is consistent with the desired Sandy Style masonry standards for new development within the C-1 zone district. Between the columns the proposal includes installing two, horizontal trex rails (2-inch by 6-inch) to complete an ornamental fence bordering the subject property. The proposed rails are proposed to be brown which is also in keeping with the Sandy Style. **The applicant shall submit the proposed trex rail color for staff’s final approval. The proposed color shall be compatible with Appendix C, Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the choice.** In addition to the onsite landscape improvements the proposal includes closing an existing ingress/egress curb cut (vehicular entrance) on Proctor Blvd/US HWY 26 to include new curb, gutter, ground covering, a Chanticleer Pear tree and improved sidewalks while on Beers Ave. a new sidewalk system is proposed with new pavement and 2, 48-inch squared tree wells each to include a Red Barron Crabapple tree at 7-foot tall or 1.5-inch caliber. **Red Barron Crabapple are proposed within the Beers Ave. right-of-way which is an approved street tree, however staff recommends the applicant choose an alternative tree species that is not a fruit bearing tree in order to prevent safety concerns from falling fruit as well as to eliminate potential maintenance concern due to fruit splatter.** ~~total of eight Red Twig Dogwood (5 gallon) plants with native grass mix at 1 pound per 1,000 square feet within the areas not occupied by the proposed plants. Additionally, the proposal includes installing a 3 foot tall chain link fence dividing the proposed pavement and landscaped areas parallel to the west property line. The intent of the proposed landscaping and fencing is to define and enhance the visual appearance of the site. Staff finds the proposed landscaping provides a decent combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element that softens the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92. however, staff does not believe the proposed landscaping provides a combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of the proposed off street activity from the public street which is required per the regulations of Chapter 17.92.~~ Further analysis regarding landscaping and screening can be found within Chapter 17.92 of this report.

Staff finds that eliminating the required building frontage and/or civic space required for the site would violate the intent and purpose of the requirement. ~~In addition, the request to not have a building on the site does not emphasize the “Village” scale described in the guiding principles of Subsection 17.90.00(C).~~ The removal of the required building eliminates the ability for the site to incorporate required Sandy Style elements and therefore does not support the guiding principles of Subsection 17.90.00(C). Eliminating the required building and/or civic space and permitting an increase in paved area to be used for outdoor storage is incompatible with Sandy Style per Subsections 17.90.00(D)(3) and 17.90.00(D)(11) and therefore is a basis for denial. ~~Additionally, staff does not believe the proposed landscaping provides a combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of off-street parking areas/activity from the public street which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C).~~ *As mentioned above, and within this report, staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C).*

- B. The variance approved is the minimum variance needed to permit practical compliance with a requirement of another law or regulation.

RESPONSE: *The requested variance is not necessary to comply with another law or regulation.*

- C. When restoration or replacement of a nonconforming development is necessary due to damage by fire, flood, or other casual or natural disaster, the restoration or replacement will decrease the degree of the previous noncompliance to the greatest extent possible.

RESPONSE: *The proposal is not in association with the restoration or replacement of a nonconforming development due to damage by fire, flood, or other casual or natural disaster, the restoration.*

RECOMENDATION: *The intent of the building orientation standard is to maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style.*

Staff finds the removal of the required building eliminates the ability for the site to incorporate required Sandy Style elements and therefore does not support the intent of the building orientation standard (Section 17.90.110(D)) or the guiding principles of Subsection 17.90.00(C). Specifically, removal of the required building does not emphasize a “village” scale as described in Subsection 17.90.00(C)(3) nor does it allow for the expression of Cascadian architectural elements, as described in 17.90.00(C)(4) and therefore violates the intent of the Sandy Style requirement. In addition, eliminating the required building frontage required for the site and introducing additional pavement for outdoor product display or storage of material violates Subsections 17.90.00(D)(3) and 17.90.00(11), both of which are defined as incompatible with the Sandy Style and are considered grounds for denial.

~~While the proposal includes landscaping to enhance and emphasize the pedestrian environment staff believes the landscaping does not meet the requirements of Chapter 17.92 therefore violating guiding principle 17.90.00(C)(1). Additionally, staff points out the request is not needed to comply with any other law or regulation and that granting the requested special variance effectively eliminates the property from having to incorporate many of the design elements identified within Chapter 17.90 further violating the intent of the requirement.~~ *The proposed onsite and offsite landscaping provides a combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C). The proposal is not required to comply with any other law or regulation and granting the requested special variance effectively eliminates the property from having to incorporate many of the Sandy Style design elements identified within Chapter 17.90.*

Staff recommends the Planning Commission deny the requested special variance to eliminate the required building frontage required by Subsection 17.90.100(D)(1) as granting the request will eliminate the Sandy Style aesthetic sought through the principles and guidelines of Chapter 17.90 Design Standards. *Alternatively, should the Planning Commission choose to approve the requested variance staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.*

CHAPTER 17.68 – CONDITIONAL USES

17.68.00 INTENT

Certain uses listed in each zoning district require special review to determine what their effects may be to the surrounding properties, neighborhood, and community as a whole. The Minor Conditional Use Permit (Type II) and Conditional Use Permit (Type III) processes provide an opportunity to allow a use when potential adverse effects can be mitigated or deny a use if concerns cannot be resolved.

It is the intent of this chapter to permit minor conditional uses or conditional uses that are consistent with the Comprehensive Plan, subject to procedures and criteria intended to mitigate potentially negative impacts.

Procedures and review criteria for conditional development are established for the following purpose:

- A. Permit certain types of public and private development that provides a community service in locations related to their service areas.
- B. Permit commercial development in locations related to its service area.
- C. Ensure that a conditional use is compatible with its immediate area and the affected part of the community

17.68.10 PROCEDURES

The applicant submitted a formal application on July 10, 2018 and received an incompleteness notice on July 12, 2018 due to no pre-application meeting being held prior to submission. After two meetings were held with the applicant, August 15, 2018 and November 21, 2018, the City received a resubmitted application packet on February 7, 2019. The submission ultimately received confirmation of completeness on March 5, 2019 thus confirming the proposal has met the requirements of Subsection 17.68.10.

The complete proposal included multiple applications with varying review types (Type II and Type III) and the Planning Director chose to elevate all applications to the highest number procedure for review (Type III) per the allowance of Subsection 17.18.00.

17.68.20 REVIEW CRITERIA

The Planning Commission (Conditional Use Permit) through a Type III process may approve an application, approve with modifications, approve with conditions, or deny an application for a conditional use permit after a public hearing. The applicant must submit evidence substantiating that all requirements of this code relative to the proposed use are satisfied and consistent with the purposes of this chapter, policies of the Comprehensive Plan, and any other applicable policies and standards adopted by the City Council.

The following criteria and compatibility factors shall be considered:

- A. The use is listed as either a minor conditional use or conditional use in the underlying zoning district or has been interpreted to be similar in use to other listed conditional uses.

***RESPONSE:** The subject property is zoned Central Business District (C-1). “Outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area” is listed as a Minor Conditional Use within the C-1 zone district. However, the applicant must still demonstrate that “all the requirements of this code relative to the proposed use are satisfied.” As discussed above and throughout this staff report, 17.90.110(J)(1) ultimately prohibits outdoor storage covering more than 10 percent of this site. Section 17.90.110 states that if “a conflict exists between the requirements of Chapter 17.90 and any other code provision, Chapter 17.90 shall prevail; therefore, the application should be denied based on the use limitation of 17.90.110(J)(1).*

The above interpretation was completed based on the language of the code as written. According to the City Attorney when an inconsistency exists between criteria in a development code, courts will defer to a local government’s interpretation if it is “plausible” considering the express language of the relevant provisions and the purposes behind them. This standard recognizes that, in many cases, no single interpretation of inconsistent code provisions is correct to the exclusion of all other interpretations. As such, the Planning Commission could consider another interpretation of 17.90.110(J), even if it is not necessarily the “most plausible.”

In this case, the inconsistency involves the phrase “except as specifically authorized as a permitted accessory use” and the fact that any inconsistency is resolved in favor of Chapter 17.90’s criteria. Staff and the City Attorney believe that the most plausible interpretation is the one offered above: the code at 17.42.10(C)(8) specifically authorizes outdoor storage not exceeding 10 percent as a permitted accessory use. Therefore, pursuant to that

interpretation, Section 17.90.110(J) prohibits outdoor storage exceeding 10 percent, as is the case in this application.

However, another interpretation could be that the phrase “except as specifically authorized as a permitted accessory use” means not as code language specifically authorizes, but instead as the Planning Commission or City Council specifically authorize in a given case. Under that interpretation, the Planning Commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the Planning Commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use.

- B. The characteristics of the site are suitable for the proposed use considering the size, shape, location, topography, and natural features.

RESPONSE: *The subject property is relatively flat with a minimum elevation change of approximately 7-feet between the southeast and northeast corners of the property lending itself suitable for the proposed use based on topography. The size and shape lend itself to accommodating the proposed use as the applicant has demonstrated the ability to plan the site accordingly to their needs. No natural features are observed on the subject property.*

The subject property is located within the C-1 zone district which is intended to provide the community with a mix of retail, personal services, offices and residential needs of the community and its trade area in the city's traditional commercial core. A review of the permitted primary uses within Chapter 17.42.10 identifies that all commercial uses within the C-1 zone district are required to have a building associated with development while the permitted accessory use of outdoor display or storage of merchandise is limited to 10 percent of the retail sales area. In addition, the C-1 zone district is intended to provide the principal focus for civil and social functions within the community, which includes emphasizing a “village” scale development with Cascadian design elements to enhance the pedestrian experience.

*The proposal includes a request for a Special Variance (Chapter 17.66 of this report) to eliminate the required building and use the site for “recreational vehicle parking and storage.” Should the Minor Conditional Use be approved the applicant will pave **approximately** 90 percent (12,586.5 square feet or .29 acres) of the site to use as a display area for recreational vehicles. Criteria 17.90.00(D)(3) identifies that excessive surface parking lot paving is incompatible with Sandy Style. Additionally, Subsection 17.90.00(D)(11) identifies outdoor storage as incompatible with the Sandy Style.*

While Subsection 17.92.110 exempts recreational vehicle sales from screening outdoor storage, Subsection 17.42.10(C)(8) limits outdoor product display of goods/merchandise to 10 percent of the retail sales area within the C-1 zone district without obtaining a Minor Conditional Use. Comparing the two subsections it is interpreted that the proposed use of 100 percent outdoor storage is incompatible with the C-1 zone district based on allowed uses within the base zone. Furthermore, Subsection 17.90.110(J)(1) strictly prohibits external storage of merchandise unless specifically authorized as a permitted accessory use. The applicant's request is to have outdoor product display and storage as the sole (and therefore

primary) use on the site and not as a permitted accessory use; therefore, the request must meet the requirements of Subsection 17.90.110(J)(1) unless the Planning Commission finds the alternative interpretation (Exhibit M) to be “plausible” in which case they may approve the proposal with or without conditions as they determine necessary to meet the overall intent of the code.

The proposed elimination of the required building and increased pavement on site within the C-1 zone district eliminates the ability for the site to incorporate required Sandy Style elements and therefore is not suitable for the site as it relates to the overall intent and applicable requirements associated with neighboring properties. However, the adjacent property to the west is the existing location of the Funtime RV retail business and the subject property is intended to be an extension of that use, therefore the proposal is in keeping with the character of the adjacent property to the west. ~~neighboring properties. Additionally, While Subsection 17.90.00(D)(3) identifies excessive surface parking lot paving as incompatible with Sandy Style, staff finds the proposal is in keeping with the character of the adjacent property to the west. and therefore grounds for denial.~~

- C. The proposed use is timely considering the adequacy of the transportation systems, public facilities and services existing or planned for the area affected by the use.

RESPONSE: It is unlikely the proposed use, outdoor product display or storage of merchandise, will generate additional trips to the site and therefore analysis of traffic impact was not required of the applicant. The proposal does not include connecting to any public facilities except for the stormwater system. The proposed stormwater system will connect to the public line located near the corner of Pleasant Street and Beers Avenue. Due to the nature of the development the applicant does not propose making improvements to or connecting to water, sanitary sewer or fiber services. Staff finds this to be acceptable at this time and notes that any future development on this site will be required to meet the standards of Chapter 17.84.60.

- D. The proposed use will not alter the character of the surrounding area in a manner which substantially limits, precludes, or impairs the use of surrounding properties for the primary uses listed in the underlying zoning district.

RESPONSE: All abutting parcels to the subject site are within City limits and are zoned Central Business District (C-1), which permits the same uses and requires the same site and building design regulations. The site is located towards the west end of Sandy’s downtown center.

Adjacent properties are as follows:

North	Multi-Family Residential
East	Beers Avenue Right-of-Way
	Retail – Timberline Meat
South	Proctor Blvd./US HWY 26 Right-of-Way
	Eating/Drinking Establishment – Taco Time
West	Retail Sales – Funtime RV

The proposed use is outdoor product display or storage of merchandise to be associated with the retail business located on the neighboring four properties, Funtime RV. As mentioned within Item B above, staff does not believe the proposed use creates an additional mix of use for the central business district. In addition, the proposed increased amount of pavement on site violates approval criteria 17.90.00(D)(3) as well as eliminates the ability for the site to incorporate required Sandy Style elements. In addition, Subsection 17.90.00(D)(11) indicates that visible outdoor storage is incompatible with Sandy Style. **Therefore, staff concludes that the proposed use is not compatible with the intended surrounding uses and should be denied based on the interpretation of the written code.**

Alternatively, should the Planning Commission choose to follow the alternative interpretation (Subsection 17.68.20(A) and/or Exhibit M) staff finds the proposed landscaping and ornamental fence provides for a more enhanced pedestrian environment and adds to the intended "village" scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C).

- E. The proposed use will not result in the use of land for any purpose which may create or cause to be created any public nuisance including, but not limited to, air, land, or water degradation, noise, glare, heat, vibration, or other considerations which may be injurious to the public health, safety, and welfare.

RESPONSE: While the adjacent property to the west has historically been used as a recreational vehicle retailer and repair services since issued a Conditional Use permit in 2009, the proposed use has the ability to add air, noise and glare pollutants on to neighboring properties due to the nature of the merchandise being sold. The west adjacent property is the existing Fun Time RV location, therefore it is reasonable to conclude they will not be negatively affected by the proposed use as well as the properties to the east and south as they are separated by right-of-way which generate additional noise, glare and pollution due to traffic. Both the existing and proposed sites will be directly adjacent south and west from a multi-family residential dwelling which has the ability to be most effected by vehicle discharge from the subject property. **Should the Planning Commission choose to approve the Minor Conditional Use, staff recommends they include a condition for the site to include a 5-foot in depth landscape buffer to include evergreen plantings at a minimum of 6-feet tall to be located the length of the north property line in order to create an additional buffer for the neighboring, north, residential development.**

- F. The proposed use will be reasonably compatible with existing or planned neighboring uses based on review of the following:

1. Basic site design (organization of uses on the site)

RESPONSE: The proposal includes a variance to eliminate the required building associated with retail as well as increase the outdoor product display or storage of merchandise above the allowed maximum. **This use is not compatible with surrounding allowed uses in that it creates an excessive surface paving area which has been determined to be incompatible with the Sandy Style design per Subsection 17.90.00(D)(3) and is therefore grounds for denial. In addition, Subsection 17.90.00(D)(11) indicates that visible outdoor storage is incompatible with Sandy Style therefore it is concluded that the proposed use is not compatible with the intent of the code and should be denied or modified. In addition,**

17.90.110(J)(1) prohibits outdoor storage exceeding 10 percent of the site. Alternatively, should the Planning Commission find the alternative interpretation (17.68.20(A) and/or Exhibit M) to be “plausible”, they may approve the proposal with or without conditions as they determine necessary to meet the overall intent of the code.

2. Visual elements (scale, structural design and form, materials, and so forth)

RESPONSE: The proposal includes a request for a variance to eliminate the required building associated with retail as well as a request for a minor conditional use to increase the outdoor product display or storage of merchandise above the allowed maximum. By eliminating the required building, the proposal eliminates the ability to meet the building design elements of Chapter 17.90 and therefore the guiding principles of 17.90.00(C). As a result, the proposal creates an excessive surface paving area for the outdoor storage of recreational vehicles, which has been determined to be incompatible with the Sandy Style design per Subsections 17.90.00(D)(3) and 17.90.00(D)(11) and is therefore grounds for denial.

~~The proposal includes 5-foot wide landscaping strips on the east and south property lines as well as a 25.5-foot by 25.5-foot planter area located in the subject property’s southwest corner. Within these landscaping areas the proposal includes native grass mixture throughout with a total of eight Red Twig Dogwood plants. Additionally, the proposal includes installing a 3-foot tall chain link fence dividing the proposed pavement and landscaped areas parallel to the west property line. The intent of the landscaping and fencing is to define and enhance the visual appearance of the site; however, staff does not believe the proposed landscaping provides a combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of the proposed off-street activity from the public street, which is required per the regulations of Chapter 17.92. Further analysis regarding landscaping and screening can be found within Chapter 17.92 of this report.~~ **Staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens the appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.**

3. Noise

RESPONSE: The proposal has ~~minimal~~ **enhanced** landscaping on the east and south property lines; however, due to the noise generated by Proctor Blvd./US HWY 26 and Beers Avenue it is unlikely that the proposed use will create any additional noise nuisance for these properties. Additionally, the adjacent property to the west is the existing site of Fun Time RV therefore it is reasonable to conclude they will not be negatively affected by noise. The adjacent property to the north is a multi-family residential development therefore it is reasonable to conclude the proposed use will increase noise permeating onto the property. Potential noise concerns include engine ignitions and/or idling, reversing indicators, mechanical equipment associated with recreation vehicles being stored as well as patron and/or employee discussions. While the recreational vehicles proposed to be stored onsite have the potential of creating additional noise to the site it also can soften some of the traffic noise generated by US HWY 26 **to the adjacent residential development to the north. Should**

the Planning Commission choose to approve the Minor Conditional Use, staff recommends they include a condition for the site to include a 5-foot wide landscape buffer to include evergreen plantings at a minimum of 6-feet tall to be located the length of the north property line in order to create an additional buffer for the neighboring, residential development to the north.

4. Noxious odors

RESPONSE: *The adjacent property to the west is the existing site of Fun Time RV therefore it is reasonable to conclude they will not be negatively affected by noxious odors. The properties to the east and south are separated from the subject property by right-of-way and are adjacent to US HWY 26 and Beers Avenue therefore it is reasonable to conclude that the proposed use will not have a substantial impact on them regarding noxious odors.*

*The adjacent property to the north is a multi-family residential development therefore it is reasonable to conclude the proposed use will increase the amount of noxious odors permeating onto the property due to engine exhaust. **Should the Planning Commission choose to approve the Minor Conditional Use, staff recommends they include a condition for the site to include a 5-foot wide landscape buffer to include evergreen plantings at a minimum of 6-feet tall to be located the length of the north property line in order to create an additional buffer for the neighboring, residential development to the north***

5. Lighting

RESPONSE: *The proposal includes exterior lighting of the parking lot. Detailed analysis of the proposed lighting can be found within Section 17.90.100(H) and Chapter 15.30 of this report.*

6. Signage

RESPONSE: *The applicant will be required to obtain a permit for any proposed signage.*

7. Landscaping for buffering and screening

RESPONSE: *The intent of the landscaping and fencing is to define and enhance the visual appearance of the site. The proposal includes the minimum landscaping requirements for the C-1 zone district (10 percent). The proposal includes 5-foot wide landscaping strips on the east and south property lines as well as a 25.5-foot by 25.5-foot planter area located in the subject property's southwest corner. Within these landscaping areas the proposal includes a total of 9 Dwarf Burning Bushes, 3 J.P.M. Rhododendrons, both proposed to be 5-gallon plantings as well as 5, 7-foot tall or 1.5-inch caliber Kousa Dogwood trees. The areas not occupied by these plantings will consist of Kinnikinic Groundcover and bark dust. In addition, the Landscape Plan (Exhibit N, Sheet C3) includes the installation of an ornamental fence. The proposed fence includes square column wraps at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed columns are to be wrapped with a stone veneer in Bucks County color which is consistent with the desired Sandy Style masonry standards for new development within the C-1 zone district. Between the columns the proposal includes installing two, horizontal trex rails (2-inch by 6-inch) to complete an ornamental fence bordering the subject property. The proposed rails are proposed to be brown which is also in keeping with the Sandy Style. **Should the proposed fencing be approved the applicant shall submit the proposed trex rail color for staff's final approval. The proposed color shall be compatible with Appendix C,***

Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the choice. In addition to the onsite landscape improvements the proposal includes closing an existing ingress/egress curb cut (vehicular entrance) on Proctor Blvd/US HWY 26 to include new curb, gutter, ground covering, a Chanticleer Pear tree and improved sidewalks while on Beers Ave. a new sidewalk system is proposed with new pavement and 2, 48-inch squared tree wells each to include a Red Barron Crabapple tree at 7-foot tall or 1.5-inch caliber. Red Barron Crabapple are proposed within the Beers Ave. right-of-way which is an approved street tree, however staff recommends the applicant choose an alternative tree species that is not a fruit bearing tree in order to prevent safety concerns from falling fruit as well as to eliminate potential maintenance concern due to fruit splatter. includes native grass mixture throughout with a total of eight Red Twig Dogwood plants. Additionally, the proposal includes installing a 3 foot tall chain link fence dividing the proposed pavement and landscaped areas parallel to the west property line. The chain link fence includes a gate at the north end of the property for ingress/egress access.

Staff finds the proposed landscaping provides a decent combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element that softens the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92. Staff does not believe the proposed landscaping provides a combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of the proposed off-street activity from the public street which is required per the regulations of Chapter 17.92. Further analysis regarding landscape and screening can be found within Chapter 17.92 of this report.

Additionally, Subsection 17.90.110(J) speaks to external storage and screening requirements associated with properties within the C-1 zone district. The intent of this section is to promote land use compatibility and aesthetics, particularly where development abuts public spaces. Within this Chapter, Subsection 17.90.110(J)(1) states that exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited. The C-1 zone district permits “Outdoor display or storage of merchandise covering no more than 10% of the total retail sales area” as an accessory use; however, the proposed Minor Conditional Use is to have 100 percent outdoor display or storage of merchandise. While the property is being associated with the neighboring business it is being reviewed on its own merit resulting in the proposed use, outdoor product display or storage of merchandise, being the primary use of the property rather than accessory.

Due to the requested outdoor display or storage of merchandise percentage the proposed use can not be classified as a permitted accessory use and therefore should be denied based on Subsection 17.90.110(J)(1). Alternatively, should the Planning Commission find the alternative interpretation (17.68.20(A) and/or Exhibit M) to be “plausible”, they may approve the proposal with or without conditions as they determine necessary to meet the overall intent of the code.

8. Traffic

RESPONSE: *It is unlikely the proposed use, outdoor product display or storage of merchandise, will generate additional trips to the site and therefore analysis of traffic impact was not required of the applicant.*

9. Effects on off-street parking

RESPONSE: *The proposal should not adversely affect on-street or off-street parking capacity. The proposed use is outdoor product display or storage of merchandise to be associated with the retail business located on the neighboring four properties, Funtime RV, which has off-street parking to accommodate both patrons and employees of the business. Additional analysis of onsite parking has been detailed within Section 17.98 of this report.*

10. Effects on air quality and water quality

RESPONSE: *The proposed improvements has the ability to generate air and water pollutants due to vehicle discharge. **The project shall comply with all applicable state and federal environmental standards.***

RECOMENDATION: *The C-1 zone district is intended to provide the community with a mix of retail, personal services, offices and residential needs of the community and its trade area in the city's traditional commercial core. While intending on catering to the needs of stakeholders the C-1 zone district is also intended to provide the principal focus for civil and social functions within the community. To support this intent Subsection 17.90.00(C) has identified a set of principles to support the civil and social functions of the community through the implementation of the Sandy Style aesthetic while Subsection 17.90.00(D) identifies elements that are incompatible with the Sandy Style which are cause for denial or modification.*

The proposed minor conditional use is to increase the percentage of outdoor product display or storage of merchandise to 100 percent to use the site in association with the adjacent property to the west, Funtime RV. Should the Minor Conditional Use be approved the applicant will pave 90 percent (12,586.5 square feet or .29 acres) of the site to use as a display area for recreational vehicles. Criteria 17.90.00(D)(3) identifies that excessive surface parking lot paving is incompatible with Sandy Style. Additionally, Subsection 17.90.00(D)(11) identifies outdoor storage as incompatible with the Sandy Style. Furthermore, Subsection 17.90.110(J)(1) states that exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited. While the property is associated with the neighboring business it is being reviewed on its own merit resulting in the proposed use, outdoor product display or storage of merchandise, being the primary use of the property rather than accessory.

Due to the approval criteria of Subsection 17.90.00(D)(3) and 17.90.00(D)(11), staff has determined the proposed use does not meet several of the conditional use criteria for approval. As detailed within this chapter, criterion B, D and F are not met due to the proposed use and associated development's incompatibility with the surrounding uses and character as it relates to site design, visual elements, and landscaping provisions. In addition, 17.90.110(J)(1) prohibits outdoor storage exceeding 10 percent of the site; therefore, staff recommends the PC deny the minor conditional use as presented.

However, another interpretation (Exhibit M) could be that the phrase "except as specifically authorized as a permitted accessory use" means not as code language specifically authorizes, but instead as the Planning Commission or City Council specifically authorize in a given case. Under

that interpretation, the Planning Commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the Planning Commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use.

Should the Planning Commission choose to find the alternative interpretation (17.68.20(A) and/or Exhibit M) to be “plausible”, they may approve the proposal with or without conditions as they determine necessary to meet the overall intent of the code. Staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens the appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.

CHAPTER 17.84 – IMPROVEMENTS REQUIRED WITH DEVELOPMENT

This chapter provides general information regarding improvements required in association with development, and it clarifies the timing, extent, and standards for public and private improvements.

17.84.20 TIMING OF IMPROVEMENTS

- A. All improvements required by the standards in this chapter shall be installed concurrently with development, as follows:
 - 1. Where a land division is proposed, each proposed lot shall have required public and franchise utility improvements installed or financially guaranteed in accordance with the provisions of Chapter 17 prior to approval of the final plat.
 - 2. Where a land division is not proposed, the site shall have required public and franchise utility improvements installed or financially guaranteed in accordance with the provisions of Chapter 17 prior to temporary or final occupancy of structures.

RESPONSE: *The submission does not include a proposed land division therefore the site shall have required public and franchise utility improvements installed or financially guaranteed in accordance with the provisions of Chapter 17 prior to temporary or final occupancy of structures. **Preliminary approval does not connote utility or public improvements plan approval which will be reviewed and approved separately upon submittal of public improvement construction plans.***

17.84.30 PEDESTRIAN AND BICYCLIST IMPROVEMENTS

- A. Sidewalks shall be required along both sides of all arterial, collector, and local streets, as follows:
 - 1. Sidewalks shall be a minimum of 5 ft. wide on local streets. The sidewalks shall be separated from curbs by a tree planting area that provides separation between sidewalk and curb, unless modified in accordance with Subsection 3 below.
 - 2. Sidewalks along arterial and collector streets shall be separated from curbs with a planting area, except as necessary to continue an existing curb-tight sidewalk. The planting area shall be

- landscaped with trees and plant materials approved by the City. The sidewalks shall be a minimum of 6 ft. wide.
3. Sidewalk improvements shall be made according to city standards, unless the city determines that the public benefit in the particular case does not warrant imposing a severe adverse impact to a natural or other significant feature such as requiring removal of a mature tree, requiring undue grading, or requiring modification to an existing building. Any exceptions to the standards shall generally be in the following order.
 - a) Narrow landscape strips
 - b) Narrow sidewalk or portion of sidewalk to no less than 4 feet in width
 - c) Eliminate landscape strips
 - d) Narrow on-street improvements by eliminating on-street parking
 - e) Eliminate sidewalks
 4. The timing of the installation of sidewalks shall be as follows:
 - a) Sidewalks and planted areas along arterial and collector streets shall be installed with street improvements, or with development of the site if street improvements are deferred.
 - b) Sidewalks along local streets shall be installed in conjunction with development of the site, generally with building permits, except as noted in (c) below.
 - c) Where sidewalks on local streets abut common areas, drainageways, or other publicly owned or semi-publicly owned areas, the sidewalks and planted areas shall be installed with street improvements.

RESPONSE: *This proposal indicates that Beers Avenue will include an eight-foot wide curb-tight sidewalk with street trees in 48" x 48" tree wells 30 feet on center. The applicant shall update the plans to include brick paver strips and scoring pattern matching the existing sidewalks in the Urban Renewal Area along Beers Avenue. The existing curb cut on Proctor is not in use and has been abandoned for some time therefore the curb cut shall be eliminated and replaced with standard Type C curb per 17.100.90(C) SMC.*

Per ODOT requirements (Exhibit I) the applicant shall install metal tube supports and signs per ODOT Standard Drawing TM490, Crosswalk Closure Detail (Exhibit I) on both the north and south sides of the west leg of Proctor Blvd./US HWY 26 and Beers Avenue.

The applicant shall update and submit plans indicating all of Proctor Blvd. frontage includes a six-foot wide sidewalk adjacent to the property line with the remaining right-of-way developed as a planter strip with approved street trees 30 feet on center per 17.84.30(A)2 SMC.

Additionally, this proposal includes upgrading the existing ADA ramp located at the northwest corner of the Proctor/US HWY 26 and Beers Avenue intersection. The ADA ramp shall be replaced with ADA-compliant ramps and the new curb return transition to the improvements on Beers Avenue shall mirror the existing curb extension on the east side of Beers Avenue.

Street tree analysis has been completed and is documented within Section 17.92 of this report.

- B. Safe and convenient pedestrian and bicyclist facilities that strive to minimize travel distance to the extent practicable shall be provided in conjunction with new development within and between new subdivisions, planned developments, commercial developments, industrial areas, residential areas, public transit stops, school transit stops, and neighborhood activity centers such as schools and parks, as follows:

1. For the purposes of this section, “safe and convenient” means pedestrian and bicyclist facilities that: are reasonably free from hazards which would interfere with or discourage travel for short trips; provide a direct route of travel between destinations; and meet the travel needs of pedestrians and bicyclists considering destination and length of trip.
2. To meet the intent of “B” above, right-of-ways connecting cul-de-sacs or passing through unusually long or oddly shaped blocks shall be a minimum of 15 ft. wide with 8 feet of pavement.
3. 12 feet wide pathways shall be provided in areas with high bicycle volumes or multiple use by bicyclists, pedestrians, and joggers.
4. Pathways and sidewalks shall be encouraged in new developments by clustering buildings or constructing convenient pedestrian ways. Pedestrian walkways shall be provided in accordance with the following standards:
 - a) The pedestrian circulation system shall be at least five feet in width and shall connect the sidewalk on each abutting street to the main entrance of the primary structure on the site to minimize out of direction pedestrian travel.
 - b) Walkways at least five feet in width shall be provided to connect the pedestrian circulation system with existing or planned pedestrian facilities which abut the site but are not adjacent to the streets abutting the site.
 - c) Walkways shall be as direct as possible and avoid unnecessary meandering.
 - d) Walkway/driveway crossings shall be minimized. Internal parking lot design shall maintain ease of access for pedestrians from abutting streets, pedestrian facilities, and transit stops.
 - e) With the exception of walkway/driveway crossings, walkways shall be separated from vehicle parking or vehicle maneuvering areas by grade, different paving material, painted crosshatching or landscaping. They shall be constructed in accordance with the sidewalk standards adopted by the City. (This provision does not require a separated walkway system to collect drivers and passengers from cars that have parked on site unless an unusual parking lot hazard exists).
 - f) Pedestrian amenities such as covered walk-ways, awnings, visual corridors and benches will be encouraged. For every two benches provided, the minimum parking requirements will be reduced by one, up to a maximum of four benches per site. Benches shall have direct access to the circulation system.

RESPONSE: *The proposal includes sidewalk infrastructure that meets the dimensional standards of the code. The site includes a gate located within the northeast portion the property. The ingress/egress of this access point has the potential to create conflict between pedestrians and vehicle access into the site at this location. ~~The applicant shall complete one of the following options:~~*

- ~~A. A gate is proposed at the proposed Beers Ave. driveway approach to the sales lot. The proposed driveway throat shall be deep enough to allow an RV and tow vehicle to pull into the site without interfering with traffic on Beers Ave. or blocking the sidewalk, or~~
- ~~B. A gate is proposed at the entrance to the sales lot. In order to prevent an RV and tow vehicle entering or exiting the site from interfering with traffic on Beers Ave. or blocking the sidewalk the gate shall be motorized and remotely operated to allow entrance and exit to and from the approach without stopping in the public right-of-way.~~

In order to provide safety for pedestrians the applicant shall demonstrate compliance with street frontage improvements including lighting and storm drainage per 17.84.80 (G) SMC upon submission of construction plans. Detailed analysis of the proposed pedestrian and bicyclist improvements shall be completed during the construction plans review process.

17.84.40 TRANSIT AND SCHOOL BUS TRANSIT REQUIREMENTS

- A. Development sites located along existing or planned transit routes shall, where appropriate, incorporate bus pull-outs and/or shelters into the site design. These improvements shall be installed in accordance with the guidelines and standards of the transit agency. School bus pull-outs and/or shelters may also be required, where appropriate, as a condition of approval for a residential development of greater than 50 dwelling units where a school bus pick-up point is anticipated to serve a large number of children.
- B. New developments at or near existing or planned transit or school bus transit stops shall design development sites to provide safe, convenient access to the transit system, as follows:
 - 1. Commercial and civic use developments shall provide a prominent entrance oriented towards arterial and collector streets, with front setbacks reduced as much as possible to provide access for pedestrians, bicycles, and transit.
 - 2. All developments shall provide safe, convenient pedestrian walkways between the buildings and the transit stop, in accordance with the provisions of 17.84.30 B.

RESPONSE: *The site is located within the Central Business District (C-1) of the City and is near the existing SAM Transit Station available to the public. With improved sidewalk and crossing infrastructure, staff does not believe the proposal warrants any additional amenities or additional improvements.*

17.84.50 STREET REQUIREMENTS

- A. Traffic evaluations may be required of all development proposals in accordance with the following:
 - 1. A proposal establishing the scope of the traffic evaluation shall be submitted for review to the City Engineer. The evaluation requirements shall reflect the magnitude of the project in accordance with accepted traffic engineering practices. Large projects should assess all nearby key intersections. Once the scope of the traffic evaluation has been approved, the applicant shall present the results with and an overall site development proposal. If required by the City Engineer, such evaluations shall be signed by a Licensed Professional Civil Engineer or Licensed Professional Traffic Engineer licensed in the State of Oregon.
 - 2. If the traffic evaluation identifies level-of-service conditions less than the minimum standard established in the Transportation System Plan, improvements and funding strategies mitigating the problem shall be considered concurrent with a development proposal.

RESPONSE: *It is unlikely the proposed use, outdoor product display or storage of merchandise, will generate additional trips to the site and therefore analysis of traffic impact was not required of the applicant.*

- D. Development sites shall be provided with access from a public street improved to City standards in accordance with the following:

1. Where a development site abuts an existing public street not improved to City standards, the abutting street shall be improved to City standards along the full frontage of the property concurrent with development.
2. Half-street improvements are considered the minimum required improvement. Three-quarter-street or full-street improvements shall be required where traffic volumes generated by the development are such that a half-street improvement would cause safety and/or capacity problems. Such a determination shall be made by the City Engineer.
3. To ensure improved access to a development site consistent with policies on orderly urbanization and extension of public facilities the Planning Commission or Director may require off-site improvements concurrent with development. Off-site improvement requirements upon the site developer shall be reasonably related to the anticipated impacts of the development.
4. Reimbursement agreements for $\frac{3}{4}$ street improvements (i.e., curb face to curb face) may be requested by the developer per Chapter 12 of the SMC.
5. A $\frac{1}{2}$ street improvement includes curb and pavement 2 feet beyond the center line of the right-of-way. A $\frac{3}{4}$ street improvement includes curbs on both sides of the side and full pavement between curb faces.

RESPONSE: *The proposed street network adjacent to the subject property's east frontage is proposed to be improved and detailed analysis of the existing right-of-way infrastructure shall be submitted and reviewed during the construction plans review process. The proposal includes a gate located within the northeast portion the property. The ingress/egress of this access point has the potential to create conflict between pedestrians and vehicle access into the site at this location. ~~The applicant shall complete one of the following options:~~*

- ~~*A. A gate is proposed at the proposed Beers Ave. driveway approach to the sales lot. The proposed driveway throat shall be deep enough to allow an RV and tow vehicle to pull into the site without interfering with traffic on Beers Ave. or blocking the sidewalk, or*~~
- ~~*B. A gate is proposed at the entrance to the sales lot. In order to prevent an RV and tow vehicle entering or exiting the site from interfering with traffic on Beers Ave. or blocking the sidewalk the gate shall be motorized and remotely operated to allow entrance and exit to and from the approach without stopping in the public right-of-way.*~~

Per ODOT review (Exhibit I) the applicant shall close the access (i.e. curb cut) to Proctor Blvd./US HWY 26 and replace with curb and sidewalk to ODOT and ADA Standards. In addition, the applicant shall record a cross access easement to the adjacent properties with state highway frontage with the County Assessor to facilitate shared access. Any work to be completed within the highway right-of-way (Proctor Blvd./US HWY 26) requires an ODOT Miscellaneous Permit to be completed, including tree planting.

17.84.60 PUBLIC UTILITY EXTENSIONS

- A. All development sites shall be provided with public water, sanitary sewer, and storm drainage.
- B. Where necessary to serve property as specified in "A" above, required public utility installations shall be constructed concurrent with development.
- C. Off-site public utility extensions necessary to fully serve a development site and adjacent properties shall be constructed concurrent with development.

- D. As necessary to provide for orderly development of adjacent properties, public utilities installed concurrent with development of a site shall be extended through the site to the edge of adjacent property(ies).
- E. All public utility installations required with development shall conform to the City’s facilities master plans.
- F. Private on-site sanitary sewer and storm drainage facilities may be considered provided all the following conditions exist:
 - 1. Extension of a public facility through the site is not necessary for the future orderly development of adjacent properties;
 - 2. The development site remains in one ownership and land division does not occur (with the exception of land divisions that may occur under the provisions of 17.84.50 F above);
 - 3. The facilities are designed and constructed in accordance with the Uniform Plumbing Code and other applicable codes, and permits and/or authorization to proceed with construction is issued prior to commencement of work.

RESPONSE: *The proposal does not include connecting to any public facilities with the exception of stormwater system to the public line located near the corner of Pleasant Street and Beers Avenue. Due to the nature of the development the applicant does not propose making improvements to or connecting to water, sanitary sewer or fiber services for the subject site. Staff finds this to be acceptable at this time and notes that any future development on this site will be required to meet the standards of Chapter 17.84.60.*

CHAPTER 17.90 – Design Standards

17.90.00 INTENT

RESPONSE: *This section of Chapter 17.90 details the objectives, guidelines, consideration and incompatible criteria associated with development within the City of Sandy as it pertains to the Design Reviews. The proposal includes a Type II Design review which has been elevated to a Type III by the Planning Director per the allowance of Subsection 17.18.00. Staff finds this chapter an integral part of the Design Review process as it details the specific requirements associated with individual zone districts to ensure the intent of the zone district implementation of the Sandy Style is achieved. Additional analysis with regards to the proposed use/development and its compatibility with the required design standards can be found within Chapters 17.66 and 17.68 of this report.*

17.90.10 APPLICABILITY

The provisions of this chapter apply to all zones and uses as follows except as specified in Sections 17.90.10(B), (C), (D), (E), and (F) below:

- A. All construction within a Commercial or Industrial Zoning District or a non-residential use in a Residential Zoning District including the following:
 - 1. New construction;
 - 2. Replacement of a building that is destroyed as specified in Section 17.08.30;
 - 3. Addition to an existing building;
 - 4. Exterior alterations other than general maintenance on an existing building;
 - 5. Site improvements including changes to landscaping, parking, civic spaces, etc.

RESPONSE: *The proposal includes site improvements with regards to landscaping and pavement for the outdoor display or storage of merchandise therefore the standards of Chapter 17.90 apply to the subject property. The submitted narrative (Exhibit C) explains that “because the proposal is limited in scope and does not include construction or renovation of a building, the majority of code standards in this Chapter are not applicable and have not been addressed in the narrative.”*

While the proposal has requested a Special Variance to eliminate the required building, it is important for analysis of the code to be completed to ensure the intent of Chapter 17.90 is met.

17.90.110 DOWNTOWN AND VILLAGE COMMERCIAL (C-1 AND C-3) DESIGN STANDARDS

Development in the C-1 and C-3 districts shall conform to all of the following standards, as applicable. Where a conflict exists between the requirements of this Chapter and any other code provision, this Chapter shall prevail.

RESPONSE: *The subject property is located within the C-1 zone district.*

A. Site Layout and Vehicle Access

Intent: To provide for compact, walkable development, and to design and manage vehicle access and circulation in a manner that supports pedestrian safety, comfort and convenience. (Figures 17.90.110-C and 17.90.110-D)

1. All lots shall abut or have cross access to a dedicated public street. **RESPONSE:** *The subject property abuts Proctor Blvd./US HWY 26 and Beers Avenue.*
2. All lots that have access to a public alley shall provide for an additional vehicle access from that alley. **RESPONSE:** *N/A*
3. Off-street parking shall be located to the rear or side of buildings with no portion of the parking lot located within required setbacks or within 10-feet of the public right-of-way, as shown in Figure 17.90.110(C). When access must be provided directly from a public right-of-way, driveways for ingress or egress shall be limited to one per 150 ft. For lots with frontage of less than 150 ft. or less, shared access may be required. **RESPONSE:** *The proposal does not include additional parking and has requested a Special Variance to eliminate the required building. Further analysis regarding the requested variance can be found within Chapter 17.66 of this report.*
4. Adjacent parking lots shall be connected to one another when the City determines it is practicable to do so. Developments shall avoid creating barriers to inter-parcel circulation. **RESPONSE:** *The subject property and the adjacent property to the west, Fun Time RV, will remain under the same ownership and function as one site. The proposal includes paving the subject property to the west property line where an approximate 27 feet wide access way, located in the southern portion of the site, will allow for cross access/circulation between the properties.*
5. Urban design details, such as raised or painted pedestrian crossings and similar devices incorporating changes in paving materials, textures or color, shall be used to calm traffic and protect pedestrians in parking areas. **RESPONSE:** *The proposal does not include interior pedestrian or civic space.*

6. Where openings occur between buildings facing Proctor Boulevard or Pioneer Boulevard, pedestrian ways shall connect the street sidewalk to any internal parking areas. Development shall avoid creating barriers to pedestrian circulation. **RESPONSE:** *The proposal includes a Special Variance request to eliminate the required on-site building.*
7. Parking lots may include public alley accessed garages at the rear property line, except where a setback is required for vision clearance or to conform to other city standards. **RESPONSE:** *N/A*
8. Raised walkways or painted crossings from the public street sidewalk to the building entrance(s) are required. Crosswalks through parking lots and drive aisles shall be constructed of a material contrasting with the road surface or be painted (e.g., colored concrete inlay in asphalt). **RESPONSE:** *The proposal includes a Special Variance request to eliminate the required on-site building.*
9. Joint use of access points and interconnections and cross-over easements between parcels shall be required, where the City determines it is practicable and necessary. A development approval may be conditioned to require a joint use access easement and interconnecting driveways or alleys to comply with access spacing and other applicable code requirements. **RESPONSE:** *As detailed in Item No. 4 above, the subject property and the adjacent property to the west, Fun Time RV, will remain under the same ownership and function as one site. The proposal includes paving the subject property to the west property line where an approximate 27 feet wide access way, located in the southern portion of the site, will allow for cross access/circulation between the properties. The applicant has expressed the belief that a cross-over easement does not make sense for the site. Due to the fact that both properties are under the same ownership and the proposal is intended on being an extension of the Fun Time RV business, staff does not believe a cross-over easement is necessary at this time.*
10. Connection to Adjacent Properties: The location of any real improvements to the property must provide for a future street and pedestrian connection to adjacent properties where the City determines this is practicable and necessary. **RESPONSE:** *No future street concept required for this proposal.*
11. Through lots and corner lots may be permitted with two access points, one onto each abutting street, where necessary to serve a centralized, shared parking facility. Such access points must conform to the above access spacing requirements and parking must be internalized to the property. **RESPONSE:** *The proposal includes one access point that will serve as a secondary access point with a gate to eliminate the ability for continuous traffic at this location.*
12. Free-standing buildings shall be connected to one another with a seamless pedestrian network that provides access to building entrances and civic spaces. **RESPONSE:** *The proposal has requested a Special Variance to eliminate the required building. Further analysis regarding the requested variance can be found within Chapter 17.66 of this report.*

B. Building Facades, Materials, and Colors

Intent: To provide building façades, materials and colors consistent with the Sandy Style. For purposes of interpreting the Sandy Style, representative illustrations and photos are provided. (Figures 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, 17.90.110-I, Color Palettes (Appendices C and D), and photos (Appendix E)

1. **Articulation.** The Sandy Style includes asymmetrical building forms, which by definition require buildings to be articulated, varied, and provide visual interest. This standard is met by dividing elevations of a structure visible from an abutting public street or pedestrian way into smaller areas or planes to minimize the appearance of bulk as viewed from the street as follows:

a. All elevations visible from an abutting public street or pedestrian way shall be divided into distinct planes no more than 30 lineal feet long to include the following:

1. Wall planes meeting this standard shall include a feature or variation in the wall plane that projects or recedes at least six (6) inches from the adjacent plane, for a length of at least four (4) feet. Changes in plane may include but are not limited to recessed entries, bays, secondary roof forms (e.g., gables, lower roof sheds, dormers and towers), canopies, awnings, projections, recesses, alcoves, pergolas, porticos, roof overhangs, or other features consistent with the Sandy Style.
2. Wall planes shall incorporate at least one visually contrasting and complementary change in materials or changes in texture or patterns, including trim, moldings, or other ornamental devices.
3. The lower and upper floors of multi-story buildings shall be clearly delineated by using pedestrian shelters, change in siding materials, heavy timber or natural wood accents (e.g., brackets, paneling or other detailing).

***RESPONSE:** The proposal has requested a Special Variance to eliminate the required building. As a result the proposal will not incorporate articulate façade to create a varying visual interest for the pedestrian environment.*

2. **Pedestrian Shelters.** Buildings must incorporate pedestrian shelters, as follows:

***RESPONSE:** The proposal has requested a Special Variance to eliminate the required building therefore does not include pedestrian shelters.*

3. **Building Materials.** Exterior building materials shall convey an impression of strength and durability consistent with the Sandy Style, as follows:

- a. Buildings on the same site shall be architecturally unified. This provision shall apply to new construction, additions, and remodeling such that buildings are related in architectural style and share some common elements, such as color scheme, materials, roof forms, and/or detailing. Unity does not mean repetition or mirroring of building elevations.
- b. Strong base materials such as natural stone (e.g., basalt, granite, river stone), split- faced rusticated concrete block, or brick are required. Cultured stone may be allowed if it has a stone texture and is similar in appearance and durability to natural stone. A building's base must extend at least 36 inches but not more than 60 inches above the adjacent finished grade and be included on those sides of the building visible from the abutting public street. If the site contains a grade differential making construction of a minimum 36-inch base impracticable, the reviewing body may allow portions of the base to be less than 36-inches.
- c. Foundations shall be designed to match the scale of the building being supported. Examples include sheathing the foundation structure with base materials and wall siding.
- d. Siding shall consist of wood, composite-wood (e.g., concrete fiberboard, panels or shingles), stone, brick, split-faced or rusticated concrete block, or a combination of these materials. Stucco, synthetic stucco, and metal are permitted only as specified below. Vinyl, plastic or similar siding is not permitted.

- 1) Where wood siding is used, it shall consist of horizontal (e.g., lap, v-groove, or tongue-and-groove) siding, vertical (board and batten) siding, shingles, or combinations thereof. Vertical grooved (i.e., T1-11) sheet siding and similar materials are prohibited.
 - 2) Where board-and-batten siding is used, battens shall be a minimum of 2-inches wide x 1-inch deep and spaced 24 inches apart or closer; rough-sawn boards (specialty panel) are preferred over panels having a resin overlay.
 - 3) Where masonry siding is used, it shall consist of brick, stone, or rusticated concrete block, and must incorporate decorative patterns over not less than 15 percent of every elevation where it is used. Examples of decorative patterns include multi-toned masonry units, such as brick, stone, or cast stone, in layered or geometric patterns or split-faced concrete block to simulate rusticated stone-type construction. Changes in pattern should be used to accentuate breaks in building stories, corners, windows, structural bays, and building tops (e.g., parapets where flat roofs are allowed).
 - 4) Where metal siding is used, it shall be used as an accent only, comprising not more than 20 percent of the surface area of the building elevation (e.g., wainscoting or other accent paneling). Metal must be architectural grade and have a non-reflective (burnished or painted) finish conforming to the approved Color Palette. Metal may also be used for flashing, gutters, downspouts, brackets, lighting, and signage and similar functional elements.
 - 5) Where stucco or synthetic stucco is used, it shall be used as an accent only, comprising not more than 20 percent of the surface area of the building elevation.
- a. Building elevations facing a public right-of-way or civic space shall incorporate at least three (3) of the following features: Using these features may also address other code requirements, such as those related to building articulation, change in relief, pedestrian shelters, and storefront elements.
1. Exposed, heavy timbers;
 2. Exposed natural wood color beams, posts, brackets and/or trim (e.g., eaves or trim around windows);
 3. Natural wood color shingles (e.g., used as siding or to accent gable ends);
 4. Metal canopies;
 5. Heavy metal brackets (e.g., cast iron or similar appearance), which may be structural brackets or applied as cosmetic detailing; *and*,
 6. Similar features, consistent with the Sandy Style.
- b. Materials required on elevations visible from an abutting public street must turn the building corner and incorporate appropriate transitions onto elevations not requiring these materials for a distance of not less than two (2) feet.

RESPONSE: *The proposal has requested a Special Variance to eliminate the required building. Guiding principles of Subsection 17.90.00(C) call for expression of Cascadian architectural elements into new buildings with the intention of celebrating Sandy as well as emphasizing a “village” scale and character in new development. Additionally, Subsection 17.90.00(D)(7) has identified that “building materials or colors that do not conform to this code (Chapter 17.90)” are incompatible with Sandy Style and may be denied or required to be modified. Staff has determined that eliminating the building, and therefore not incorporating the required building materials and colors, would violate the above-mentioned guiding principle as well as Subsection 17.90.00(D)(7)*

which is grounds for denial; however, the proposal includes incorporating enhanced landscaping and an ornamental fence along the south and east street frontages to enhance the pedestrian environment and help create the “village” scaled atmosphere Chapter 17.90 seeks to achieve which is a factor when determining whether the proposal meets the intent of the design standards.

4. Colors. Building exteriors shall comply with the following standards:

- a) Permitted colors include warm earth tones (tans, browns, reds, grays and greens) conforming to the Color Palette provided in Appendix C.
- b) High-intensity primary colors, metallic colors and black, may be utilized as trim and detail colors only, not to exceed one (1) percent of the surface area of any elevation. Such color shall not be used as primary wall colors.
- c) Day-glow colors, highly reflective colors, and similar colors are not permitted.

RESPONSE: *The proposal has requested a Special Variance to eliminate the required building. Guiding principles of 17.90.00(C) call for the incorporation of Cascadian architecture into new buildings and exterior remodels. Additionally, Subsection 17.90.00(D)(7) has identified that “building materials or colors that do not conform to this code (Chapter 17.90)” are incompatible with Sandy Style and may be denied or required to be modified. Staff has determined that eliminating the building, and therefore not incorporating the required building materials and colors, would violate the above-mentioned guiding principle as well as be incompatible with Subsection 17.90.00(D)(7) which ~~is~~ **may be** grounds for denial.*

The proposal includes the incorporation of an ornamental fence along the south and east street frontages to enhance the pedestrian environment and help create the “village” scaled atmosphere. The proposed ornamental fence includes square columns at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed fence includes square column wraps at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed columns are to be wrapped with a stone veneer in Bucks County color which is consistent with the desired Sandy Style masonry standards for new development within the C-1 zone district. Between the columns the proposal includes installing two, horizontal trex rails (2-inch by 6-inch) to complete an ornamental fence bordering the subject property. The proposed rails are proposed to be brown which is also in keeping with the Sandy Style. Should the proposed fencing be approved the applicant shall submit the proposed trex rail color for staff's final approval. The proposed color shall be compatible with Appendix C, Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the choice. Staff has determined that by incorporating the proposed fencing with Sandy Style materials and colors the proposal provides compatible Canadian design elements that ultimately meet the intent of this section of the code and can be granted at this time.

C. Roof Pitch, Materials, and Parapets

Intent: To provide roof forms and detailing consistent with the Sandy Style. For purposes of interpreting the Sandy Style, representative illustrations and photos are provided. (See Figures 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, 17.90.110-I and representative photos in Appendix E)

- 1) Except as provided in subsections 17.90.110(C)(8), below, pitched (gabled or hipped) roofs are required on all new buildings with a span of 50-feet or less. Gable and hipped roof forms must achieve a pitch not less than the following:

Zoning District	Primary Roof Forms (Minimum)	Secondary Roof Forms (minimum)
C-1, C-3	6:12	4:12

- 2) As provided above, “Primary Roof Forms” are those that individually comprise 20 percent or more of the total surface area of a roof elevation. Secondary roof forms (e.g., dormers, towers, cupolas, etc.) are those that comprise less than 20 percent of the roof elevation. See also, Section 17.74.20 Vertical Projections.
- 3) When practicable, buildings shall be oriented so the gable end of the roof faces the abutting street.
- 4) Pitched roof surfaces visible from an abutting public street shall provide a secondary roof form (e.g. dormer) in the quantity specified below. Secondary roof forms may be located anywhere on the roof, although grouping these features is preferred.

Roof Length	Number of Secondary Roof Forms
30 – 40 feet	1
41 – 80 feet	2
81 feet and greater	4

- 5) Visible roof materials must be wood shingle or architectural grade composition shingle, slate, or concrete tile. Metal with standing or batten seam may also be used conforming to the Color Palette in Appendix D.
- 6) All roof and wall-mounted mechanical, electrical, communications, and service equipment, including satellite dishes and vent pipes, shall be screened from view from public rights-of-way and civic spaces by parapets, walls or by other approved means. Roof plans and elevations must show proposed equipment locations, approximate dimensions, and line of sight from public rights-of-way and civic spaces. The reviewing body may require additional equipment setbacks, screen walls, or other mitigation to ensure compliance.
- 7) A-frame buildings and Mansard-style roofs are not permitted.
- 8) Exception to Pitched Roof: When a building requires a roof span greater than 50-feet, or the internal function of the building or a portion of the building makes construction of a pitched roof impractical, the reviewing body may allow an alternative roof form. An alternative roof form includes an “applied pitched roof” or flat roof constructed over the building or portion of the building as specified below. An example when a pitched roof is considered impractical would be the need to have large rooftop stove vents over the kitchen portion of a restaurant. Roof forms constructed under this exception shall comply with the standards below.

b. Flat Roof: Flat roofs shall comply with the following standards:

- 1) Sandy Style stepped parapets and detailed coursing shall be provided on those elevations visible from an abutting public street. Parapets shall be varied so that the length of a parapet does not exceed 30 feet without a change in the parapet height of at least 2 feet or as necessary to hide rooftop equipment.
- 2) Average parapet height shall not exceed 15 percent of the supporting wall height, and the maximum parapet height shall not at any point exceed one-third (1/3) of the height of the supporting wall;
- 3) A cornice projecting at least six (6) inches from the building face shall be provided at the roofline of all elevations visible from abutting public rights-of- ways and pedestrian ways;
- 4) Parapet corners shall be stepped and the parapet be designed to emphasize the center or primary entrance(s), unless the primary entrance is at the corner of the building.

RESPONSE: *The proposal has requested a Special Variance to eliminate the required building. Guiding principles of 17.90.00(C) call for the incorporation of Cascadian architecture into new buildings with the intention of celebrating Sandy as well as emphasizing a “village” scale and character in new development. Additionally, Subsection 17.90.00(D)(9) has identified that “strongly thematic architectural styles, forms, colors, materials and/or detailing that do not conform to the Sandy Style” may be denied or required to be modified. **Staff has determined that eliminating the building, and therefore not incorporating the required architectural style, would violate the above-mentioned guiding principle as well as Subsection 17.90.00(D)(9) which is *may be grounds for denial.****

D. Building Orientation and Entrances

Intent: To maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style. (Figures 17.90.110-A, 17.90.110-B, 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, 17.90.110-I and representative photos in Appendix E)

1. Buildings shall be oriented to a public street or civic space. This standard is met when at least 50 percent of the subject site’s street frontage is comprised of building(s) placed within 10 feet of a sidewalk or an approved civic space and not more than 20 percent of the off- street parking on a parcel as required by SDC 17.98, tract or area of land is located between a building’s front façade and the adjacent street(s).
2. Where parking is placed between a front façade and a street, a landscaped berm and/or architectural features, such as a knee wall, colonnade, arbor, trellis and/or similar device, shall be placed behind the sidewalk to partially screen the parking area from the sidewalk. The partial screen shall be designed to achieve at least 50 percent opacity at the time of installation, with openings for walkways connecting to the building’s primary entrance.
3. Ground floor spaces shall face a public street and shall be connected to it by a direct pedestrian route (i.e., avoid out-of-direction travel). Where the reviewing authority determines that facing the building to a street is not practical, it may require the building to face a civic space.

4. Buildings located at the intersection of two streets shall use a corner building entrance; where a corner entrance is not practical due to the internal functioning of the building space or due to physical constraints of the site (e.g., topography, accessibility, or similar circumstances), a building entrance must be provided within 40 feet of the corner. The building corner must use detailing that emphasizes the corner location and is consistent with the Sandy Style. Examples of acceptable detailing include a rounded or chamfered (beveled) corner, weather protecting canopy, plaza, sculpture, and/or similar pedestrian-oriented features.
5. Upper story residential units shall have an entrance separate from the ground floor (commercial) space and conform to applicable building codes.
6. Buildings shall provide at least two elevations where the pedestrian environment is “activated”. An elevation is “activated” when it meets the window transparency requirements in subsection 17.90.110(E), below, and contains a customer entrance with a pedestrian shelter extending at least five (5) feet over an adjacent sidewalk, walkway or civic space. Where providing a customer entrance on two (2) elevations is not practical, the reviewing body may allow a single entrance.
7. Primary entries shall face a public street or a civic space and shall be spaced not more than 30 feet apart on average. Ancillary shops shall provide entries every 30 feet, on average.
8. Primary entrances shall be architecturally emphasized and visible from the abutting public right-of-way or civic space and shall be sheltered with a canopy, overhang, or portico with a depth of at least five (5) feet. Architectural emphasis should be provided by a gabled shelter where practical, consistent with the Sandy Style. Detailing around the base of the building, such as stonework, benches or art, should also be used to emphasize an entrance.

RESPONSE: *The proposal has requested a Special Variance to eliminate the required building. Should the variance be granted, the proposal will be considered in conformance with the above standard. While the proposal includes sidewalks along both Proctor Blvd./US HWY 26 and Beers Avenue the elimination of the required building also reduces the ability for the proposal to be consistent with the Sandy Style as there is not an opportunity to incorporate the design elements the guiding principles in Subsection 17.90.00(C) identify. The Special Variance request is discussed in detail in Chapter 17.66 of this document.*

E. Windows

Intent: To promote business vitality, public safety and aesthetics through effective window placement and design, consistent with the Sandy Style. (See Figures 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, and 17.90.110-I, and representative photos in Appendix E.

- 1) **Unified Design.** Building plans must provide for unity in window placement and design so that all sides of a building relate to one another and multiple buildings on a development site relate to one another.
- 2) **Ground Floor Windows.** The ground floor elevation of all new buildings shall contain display areas, windows, and doorways along street frontages and where the building abuts a civic space as follows: Lots with multiple street frontages are required to meet this standard on only two frontages.

Building Size	Percentage Windows Required
0 - 10,000 sq. ft.	40 percent of ground floor elevation
Greater than 10,000 sq. ft.	25 percent of ground floor elevation

- a. Windows shall contain clear glass to allow views to interior activity or display areas. The bottom edge of windows shall be no less than three (3) feet above the adjacent finished grade. Where the internal functions of a building preclude windows at this height, the reviewing body may approve locating windows above or below this height. Display boxes affixed to a building’s exterior are not counted in meeting the above standard.
- b. Windows shall be square or vertically oriented and may consist of vertically stacked or horizontally banked window units. Windows located over a door or transom windows may be horizontally oriented.
- c. Windows with any dimension exceeding six (6) feet shall be divided into smaller panes (e.g., 2 foot by 2 foot grid) with real divided panes, vinyl inserts or applied dividers.
- d. Windows shall have trim or moldings at least three (3) inches in width around them, or have reveals of at least three (3) inches in depth. Casings shall consist of a drip cap, head casing, side casings, and/or sills.

4) **Prohibited Windows.** The following window types are prohibited:

Building Size	Percentage Windows Required
0 - 10,000 sq. ft.	40 percent of ground floor elevation
Greater than 10,000 sq. ft.	25 percent of ground floor elevation

- 1. Darkly tinted windows, mirrored windows, and similar windows are prohibited adjacent to street sidewalks, civic spaces and walkways.
- 2. Glass curtain windows are not permitted facing public right-of-ways, except where the reviewing body finds that such windows are consistent with the Sandy Style.

RESPONSE: *The applicant has requested a Special Variance to eliminate the required building. Should the Minor Conditional Use be approved the property will incorporate pavement to display recreational vehicles for sale on approximately 90 percent of the subject property. The display of recreational vehicles will promote a sense of business vitality within the C-1 zone district; however, it will not incorporate, nor is it consistent with, Sandy Style as there is not an opportunity to incorporate the design elements identified in the guiding principles in Subsection 17.90.00(C).*

F. Landscaping and Streetscape Design

Intent: To promote business vitality, public safety and aesthetics through effective landscaping and streetscape design, consistent with the Sandy Style; and to provide for a pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style. (Figures 17.90.110-A, 17.90.110-B, 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, 17.90.110-I, and Downtown Sandy Streetscape Design)

- 1. The provisions of Chapter 17.92, Landscaping and Screening General Standards shall apply except in the C-1 Zoning District where conformance with the Downtown Sandy Streetscape Design, as illustrated in Appendix F is required.
- 2. Where any conflict arises between provisions of the Sandy Streetscape Design and other city standards (e.g., sidewalk width, materials, or similar specifications), the Streetscape Design shall

prevail. All applicable provisions of Chapter 17.92 Landscaping and Screening General Standards must be met, except as modified by the Downtown Sandy Streetscape Design.

RESPONSE: *The proposal includes incorporating a 5-foot wide planter strip along the east and south property lines/frontages with a 25.5-foot by 25.5-foot planter area located in the subject property's southwest corner. Within these landscaping areas the proposal includes a total of eight Red Twig Dogwood (5 gallon) plants with native grass mix at 1-pound per 1,000 square feet within the areas not occupied by the proposed plants. Additionally, the proposal includes installing a 3-foot tall chain link fence dividing the proposed pavement and landscaped areas parallel to the west property line. Within these landscaped area a total of 9 Dwarf Burning Bushes, 3 J.P.M. Rhododendrons, both proposed to be 5-gallon plantings as well as 5, 7-foot tall or 1.5-inch caliber Kousa Dogwood trees. The areas not occupied by these plantings will consist of Kinnikinnick Groundcover and bark dust. In addition, the Landscape Plan (Exhibit N, Sheet C3) includes the installation of an ornamental fence. The proposed fence includes square column wraps at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed columns are to be wrapped with a stone veneer in Bucks County color which is consistent with the desired Sandy Style masonry standards for new development within the C-1 zone district. Between the columns the proposal includes installing two, horizontal trex rails (2-inch by 6-inch) to complete an ornamental fence bordering the subject property. The proposed rails are proposed to be brown which is also in keeping with the Sandy Style. **Should the proposed fencing be approved the applicant shall submit the proposed trex rail color for staff's final approval. The proposed color shall be compatible with Appendix C, Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the choice.** In addition to the onsite landscape improvements the proposal includes closing an existing ingress/egress curb cut (vehicular entrance) on Proctor Blvd/US HWY 26 to include new curb, gutter, ground covering, a Chanticleer Pear tree and improved sidewalks while on Beers Ave. a new sidewalk system is proposed with new pavement and 2, 48-inch squared tree wells each to include a Red Barron Crabapple tree at 7-feet tall or 1.5-inch caliber. **Red Barron Crabapple are proposed within the Beers Ave. right-of-way which is an approved street tree, however staff recommends the applicant choose an alternative tree species that is not a fruit bearing tree in order to prevent safety concerns from falling fruit as well as to eliminate potential maintenance concern due to fruit splatter.** The intent of the landscaping and fencing is to define and enhance the visual appearance of the site; staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended "village" scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92. ~~The intent of the landscaping and fencing is to define and enhance the visual appearance of the site; however, staff does not believe the proposed landscaping provides a combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of the proposed off-street activity from the public street, which is required per the regulations of Chapter 17.92. Further analysis regarding landscaping and screening can be found within Chapter 17.92 of this report.~~*

G. Civic Space

Intent: To connect buildings to the public realm and create comfortable and attractive gathering places and outdoor seating areas for the public, consistent with Sandy's Downtown Streetscape Design. (See Figures 17.90.110-H and 17.90.110-I).

1. Not less than three (3) percent of the ground floor area of every development shall be improved as civic space.
2. All civic spaces shall have dimensions of not less than eight (8) feet across and have a surface area of not less than 64 square feet. No civic space is required if the size of this space results in an area of less than 64 square feet.
3. Civic space improvements may include plazas, private extensions of sidewalks and walkways (i.e., to accommodate outdoor seating), public art, pedestrian-scale lighting, bus waiting areas, tourist amenities (e.g., way finding signs as approved by the city) or similar pedestrian amenities as approved through Design Review.
4. The highest priority locations for civic space are those areas with the highest pedestrian activity (e.g., street corners and mid-block pedestrian access ways) that have a western or southern exposure.
5. Unless impractical, civic spaces shall abut a public right-of-way or otherwise be connected to and visible from a public right-of-way by a sidewalk or pedestrian access way; access ways shall be identifiable with a change in paving materials (e.g., pavers inlaid in concrete or a change in pavement scoring patterns and/or texture) or painted. Where a right-of-way connection is not possible, the owner must provide a public access way easement to the civic space. Civic spaces shall not be gated or closed to public access, unless otherwise required by the city.
6. Exceptions: Building additions and remodels subject to Type I Design Review are not required to set aside or improve civic space, though they are encouraged to do so.

RESPONSE: *The proposal has requested a Special Variance to eliminate the required building. Guiding principles of 17.90.00(C) call for emphasis on a "village" scale and character in new development. ~~Eliminating~~ While it can be interpreted that eliminating the building and civic space would violate the above-mentioned guiding principles, the proposed landscaping and ornamental fencing described in Subsection 17.90.110(F) of this report support the "village" scale character and Sandy Style aesthetic the design guidelines have established as well as creates a more comfortable and attractive pedestrian experience.*

H. Lighting

Intent: To promote business vitality, public safety and aesthetics through effective outdoor lighting, consistent with the Sandy Style.

1. Streetscape lighting shall conform to the Downtown Sandy Streetscape Design and the requirements of Chapter 15.30, Dark Sky Ordinance.
2. Exterior lighting must be an integral part of the architectural design and must complement any ornamental street lighting and remain in context with the overall architectural character of the district. On-site light fixtures conforming to the Sandy Style are encouraged.
3. Lighting must be adequate for safety purposes. Walkways, parking lots, and building entrances should be illuminated at 1.5 – 2.0 foot candles.

RESPONSE: *The proposal included on site lighting (Exhibit F) for the proposed outdoor display area. The measurements provided exceed the 1.5- 2.0 foot candle limitation. **The applicant shall***

provide staff with an updated photometric plan limiting the foot candle illumination not to exceed City limitations unless Planning Commission interprets the foot candle limitation differently.

I. Safety and Security

Intent: To promote natural surveillance of public spaces for safety and security.

1. Locate windows in a manner that enables tenants, employees and police to watch over pedestrian, parking and loading areas.
2. In commercial, public and semipublic development, including civic spaces, locate windows in a manner that enables surveillance of interior activity from the public right-of-way.
3. Provide street address numbers measuring a minimum of six (6) inches high, which clearly locates buildings and their entries for patrons and emergency services.
4. Locate, orient and select on-site lighting to facilitate surveillance of on-site activities from the public right-of-way and other public areas. (See also, subsection H Lighting.)

RESPONSE: *The proposed paved area is intended to display and store recreational vehicles for sale. Onsite lighting is proposed to be installed throughout the site to facilitate internal as well as external surveillance.*

J. External Storage and Screening

Intent: To promote land use compatibility and aesthetics, particularly where development abuts public spaces.

1. Exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited.
2. Where such storage is allowed, it must be screened from view from public rights of way and civic spaces.
3. Mechanical, electrical, communications equipment including meters and transformers, and service and delivery entrances and garbage storage areas shall be screened from view from public rights-of-way and civic spaces.
4. Trash collection and recycling storage areas must be located within the structure or otherwise screened from view in an enclosed facility. Such facilities must be screened from view from public rights-of-way and civic spaces behind a screening wall constructed to match the materials used on the primary building(s) on the subject site.
5. Exceptions to the above provisions may be allowed through Design Review where no other practical alternative exists and such equipment is made to be visually subordinate to the proposed building and landscape, for example, through the use of common materials for screening walls or landscape berms. The reviewing body may require additional setbacks, screening walls or other mitigation, for aesthetic reasons and to minimize odors or noise impacts on adjoining properties, public rights-of-way or civic spaces.

RESPONSE: *The proposal includes a Minor Conditional Use request to increase the percentage of “outdoor product display or storage of merchandise” to 100 percent of the retail sales area of the site to be associated with the adjacent property to the west, Funtime RV. Should the Minor Conditional Use be approved the property will incorporate pavement to display recreational vehicles for sale on 90 percent of the subject property. Subsection 17.90.00(D)(3) identifies*

*excessive surface parking lot paving as incompatible with Sandy Style while Subsection 17.90.00(D)(11) indicates that visible outdoor storage is incompatible with Sandy Style. **Therefore it is concluded that the proposed use is not compatible with the intent of the code and should be denied or modified. Furthermore, Subsection 17.90.110(J)(1) states that exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited. While the property is being associated with the neighboring business it is being reviewed on its own merit; therefore, the proposed use, “outdoor product display or storage of merchandise,” is considered the property’s primary use rather than accessory and therefore can not be permitted.***

However, another interpretation could be that the phrase “except as specifically authorized as a permitted accessory use” means not as code language specifically authorizes, but instead as the Planning Commission or City Council specifically authorize in a given case. Under that interpretation, the Planning Commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the Planning Commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use.

Alternatively, should the Planning Commission find the alternative interpretation (17.68.20(A) and/or Exhibit M) to be “plausible”, they may approve the proposal with or without conditions as they determine necessary to meet the overall intent of the code. Staff finds the proposed landscaping provides a decent combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element that softens the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.

CHAPTER 17.92 – LANDSCAPING AND SCREENING

17.92.10 GENERAL PROVISIONS

1. Where landscaping is required by this Code, detailed planting plans shall be submitted for review with development applications. No development may commence until the Director or Planning Commission has determined the plans comply with the purposes clause and specific standards in this chapter. All required landscaping and related improvements shall be completed or financially guaranteed prior to the issuance of a Certificate of Occupancy.
2. Appropriate care and maintenance of landscaping on-site and landscaping in the adjacent public right-of-way is the right and responsibility of the property owner, unless City ordinances specify otherwise for general public and safety reasons. If street trees or other plant materials do not survive or are removed, materials shall be replaced in kind within 6 months.
3. Significant plant and tree specimens should be preserved to the greatest extent practicable and integrated into the design of a development. Trees of 25-inches or greater circumference measured at a height of 4-1/2 ft. above grade are considered significant. Plants to be saved and methods of protection shall be indicated on the detailed planting plan submitted for approval. Existing trees may be considered preserved if no cutting, filling, or compaction of the soil takes place between the trunk of the tree and the area 5-ft. outside the tree’s drip line. Trees to be

- retained shall be protected from damage during construction by a construction fence located 5 ft. outside the dripline.
4. Planter and boundary areas used for required plantings shall have a minimum diameter of 5- ft. (2-1/2 ft. radius, inside dimensions). Where the curb or the edge of these areas are used as a tire stop for parking, the planter or boundary plantings shall be a minimum width of 7-1/2 ft.
 5. In no case shall shrubs, conifer trees, or other screening be permitted within vision clearance areas of street, alley, or driveway intersections, or where the City Engineer otherwise deems such plantings would endanger pedestrians and vehicles.
 6. Landscaped planters and other landscaping features shall be used to define, soften or screen the appearance of off-street parking areas and other activity from the public street. Up to 35 percent of the total required landscaped area may be developed into pedestrian amenities, including, but not limited to sidewalk cafes, seating, water features, and plazas, as approved by the Director or Planning Commission.
 7. Required landscaping/open space shall be designed and arranged to offer the maximum benefits to the occupants of the development as well as provide visual appeal and building separation.
 8. Balconies required for entrances and exits shall not be considered as open space except where such exits and entrances are for the sole use of the unit.
 9. Roofed structures shall not be included as open space except for open unenclosed public patios, balconies, gazebos, or other similar structures or spaces.
 10. Driveways and parking areas shall not be included as open space.
 11. All areas not occupied by paved roadways, walkways, patios, or buildings shall be landscaped.
 12. All landscaping shall be continually maintained, including necessary watering, weeding, pruning and replacing.

RESPONSE: *The applicant has submitted an **updated** Landscaping Plan (**Exhibit N, Sheet C3**) (~~Exhibit D, Sheets C3 & C5~~) with associated key notes detailing the proposed landscaping. The proposal includes incorporating a 5-foot wide planter strip along the east and south property lines/frontages with a 25.5-foot by 25.5-foot planter area located in the subject property's southwest corner. Item 4 above states that where a curb or the edge of a landscaped area is used as a wheel stop for parking, the planter or boundary plantings shall be a minimum width of 7.5-feet. **While the proposed paved area is not a parking lot by definition, but rather is considered an outdoor display area, the intention is to store recreational vehicles on site; therefore, it is recommended that the applicant redesign the landscape strips to be a minimum of 7.5-feet wide or include a wheel stop to protect the landscaping.** The proposal is to display (i.e. park/store) RVs upon the property and the intention of this criteria is to protect landscape buffers while creating a barrier between parked vehicles and pedestrians, therefore it is reasonable to conclude that either a wheel stop, or increased planter depth of 7.5-feet is required. While the proposal does not include either of these elements it does propose incorporating an ornamental fence to enhance the "village" scale and provide the separation that this section of the code intends to achieve. Staff supports the alternative to incorporate the ornamental fence as opposed to wheel stops and/or an increased planter depth. Should landscaping be damaged by RVs it will need to be replaced.*

17.92.20 MINIMUM IMPROVEMENTS - LANDSCAPING AND SCREENING

The minimum landscaping area of a site to be retained in landscaping shall be as follows:

ZONING DISTRICT OR USE	PERCENTAGE
R-3	25%

Manufactured Home Park	20%
C - 1 Central Business District	10%
C - 2 General Commercial	20%
C - 3 Village Commercial	10%
I - 1 Industrial Park	20%
I - 2 Light Industrial	15%
I - 3 Heavy Industrial	10%

RESPONSE: *The subject property is zoned Central Business District, C-1. Section 17.92.20 requires that a minimum of 10 percent of the site be landscaped in the C-1 zone district. The submitted Landscape Plan (Exhibit D, Sheet C3) identifies a total of 1,399 square feet of landscaped area, which equates to a total of 10 percent of the site (13,985/1,399). Staff has included a condition that should the Planning Commission choose to approve the Minor Conditional Use, the applicant shall include a condition for the site to include a 5-foot wide landscape buffer to include evergreen plantings at a minimum of 6-feet tall to be located the length of the north property line in order to create an additional buffer for the neighboring, residential development to the north. This would result in an increased landscape percentage for the overall property and ultimately exceeding the required 10 percent minimum required for the C-1 zone district.*

17.92.30 REQUIRED TREE PLANTINGS

Planting of trees is required for all parking lots with 4 or more parking spaces, public street frontages, and along private drives more than 150 feet long. Trees shall be planted outside the street right-of-way except where there is a designated planting strip or City adopted street tree plan.

The City maintains a list of appropriate trees for street tree and parking lot planting situations. Selection of species should be made from the city-approved list. Alternate selections may be approved by the Director following written request. The type of tree used shall determine frequency of trees in planting areas. Trees in parking areas shall be dispersed throughout the lot to provide a canopy for shade and visual relief.

Area/Type of Planting	Canopy	Spacing
Street Tree	Medium	30 ft. on center
Street Tree	Large	50 ft. on center

- Trees may not be planted:
 - Within 5 ft. of permanent hard surface paving or walkways, unless specific species, special planting techniques and specifications approved by the Director are used.
 - Unless approved otherwise by the City Engineer:
 - * Within 10 ft. of fire hydrants and utility poles
 - * Within 20 ft. of street light standards
 - * Within 5 ft. from an existing curb face
 - * Within 10 ft. of a public sanitary sewer, storm drainage or water line
- Where the Director determines the trees may be a hazard to the public interest or general welfare.

- Trees shall be pruned to provide a minimum clearance of 8 ft. above sidewalks and 12 ft. above street and roadway surfaces.

RESPONSE: Section 17.92.30 requires trees to be planted along public street frontages. The proposal indicates that Beers Avenue will include an eight-foot wide curb-tight sidewalk with street trees in 48" x 48" tree wells 30 feet on center. The proposed trees to be planted within the tree wells on this frontage are Red Barron Crabapple species at 7-feet tall and/or 1.5 caliper.

The existing curb cut on Proctor Blvd./US HWY 26 is not in use and has been abandoned for some time therefore **the curb cut shall be eliminated, and ground cover shall be installed.** The proposal includes the planting of a Chanticleer Pear Tree **centered in between two existing trees within the right-of-way.** 30 feet on center from the tree adjacent to the west. This results in an approximately 45 foot from center separation from the existing tree that will be adjacent to the east of the proposed pear tree. **To create visual symmetry, the applicant shall relocate the proposed tree centered between the existing trees located along the Proctor Blvd./US HWY 26 of the subject property.**

All trees shall include tree ties with twine, loosely tied so as not to damage the trunk and shall be removed after one growing season (or a maximum of 1 year).

17.92.40 IRRIGATION

Landscaping shall be irrigated, either with a manual or automatic system, to sustain viable plant life.

RESPONSE: All landscape areas will be irrigated using either manual or automatic system per the applicant's narrative (Exhibit C). **The applicant shall demonstrate compliance with this proposal at time of building permit.**

17.92.50 TYPES AND SIZES OF PLANT MATERIALS

- At least 75% of the required landscaping area shall be planted with a suitable combination of trees, shrubs, or evergreen ground cover except as otherwise authorized by Chapter 17.92.10 F.
- Plant Materials. Use of native plant materials or plants acclimatized to the Pacific Northwest is encouraged where possible.
- Trees shall be species having an average mature spread of crown greater than 15 feet and having trunks which can be maintained in a clear condition with over 5 feet of clear wood (without branches). Trees having a mature spread of crown less than 15 feet may be substituted by grouping the same so as to create the equivalent of a 15-foot crown spread.
- Deciduous trees shall be balled and burlapped, be a minimum of 7 feet in overall height or 1 1/2 inches in caliper measured 6 inches above the ground, immediately after planting. Bare root trees will be acceptable to plant during their dormant season.
- Coniferous trees shall be a minimum five feet in height above ground at time of planting.
- Shrubs shall be a minimum of 1 gallon in size or 2 feet in height when measured immediately after planting.
- Hedges, where required to screen and buffer off-street parking from adjoining properties shall be planted with an evergreen species maintained so as to form a continuous, solid visual screen within 2 years after planting.

- H. Vines for screening purposes shall be a minimum of 1 gallon in size or 30 inches in height immediate after planting and may be used in conjunction with fences, screens, or walls to meet physical barrier requirements as specified.
- I. Groundcovers shall be fully rooted and shall be well branched or leafed. If used in lieu of turf in whole or in part, ground covers shall be planted in such a manner as to provide complete coverage in one year.
- J. Turf areas shall be planted in species normally grown as permanent lawns in western Oregon. Either sod or seed are acceptable. Acceptable varieties include improved perennial ryegrasses and fescues used within the local landscape industry.
- K. Landscaped areas may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, fences, rock groupings, bark dust, decorative hard paving and gravel areas, interspersed with planted areas. The exposed area developed with such features shall not exceed 25% of the required landscaped area. Artificial plants are prohibited in any required landscape area.

RESPONSE: *The submitted Landscape Plan (Exhibit D, Sheet C3) includes 5-foot wide landscaping strips on the east and south property lines as well as a 25.5-foot by 25.5-foot planter area located in the subject property's southwest corner. Within these landscaped areas a total of 9 Dwarf Burning Bushes, 3 J.P.M. Rhododendrons, both proposed to be 5-gallon plantings as well as 5, 7-foot tall or 1.5-inch caliber Kousa Dogwood trees. The areas not occupied by these plantings will consist of Kinnikinnick Groundcover and bark dust. Within these landscaping areas the proposal includes native grass mixture throughout with a total of eight Red Twig Dogwood plants. The Red Twig Dogwood plant is classified as a shrub and will be 5-gallons in size each. Subsection 17.92.50(A) requires at least 75% of the required landscaping area shall be planted with a suitable combination of trees, shrubs, or evergreen ground cover that softens or screens the appearance of the proposed off-street activity from the public street.*

Staff finds the proposed landscaping provides a combination of trees, shrubs, and ground cover that covers 75% of the landscaping area and softens the appearance of the proposed RVs from the public street while enhancing the pedestrian environment and adding to the intended "village" scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.

Staff has concluded the proposed landscaping does not meet the intent of 17.92.50(A) therefore the applicant shall resubmit a landscape plan that provides a combination of trees, shrubs, or evergreen ground cover of at least 75 percent of the required landscaping area for staff review and approval.

17.92.70 LANDSCAPING BETWEEN PUBLIC RIGHT-OF-WAY AND PROPERTY LINES

Except for portions allowed for parking, loading, or traffic maneuvering, a required setback area abutting a public street and open area between the property line and the roadway in the public street shall be landscaped. That portion of the landscaping within the street right-of-way shall not count as part of the lot area percentage to be landscaped.

RESPONSE: *The proposal includes incorporating a 5-foot wide planter strip along the east and south property lines/frontages with a 25.5-foot by 25.5-foot planter area located in the subject*

property's southwest corner. This area is within the subject property's boundaries leaving no unlandscaped area between the property lines and public right-of-way.

17.92.80 BUFFER PLANTING - PARKING, LOADING AND MANUEVERING AREAS

Buffer plantings are used to reduce building scale, provide transition between contrasting architectural styles, and generally mitigate incompatible or undesirable views. They are used to soften rather than block viewing. Where required, a mix of plant materials shall be used to achieve the desired buffering effect.

Buffering is required in conjunction with issuance of construction permits for parking areas containing 4 or more spaces, loading areas, and vehicle maneuvering areas. Boundary plantings shall be used to buffer these uses from adjacent properties and the public right-of-way. On-site plantings shall be used between parking bays, as well as between parking bays and vehicle maneuvering areas. A balance of low-lying ground cover and shrubs, and vertical shrubs and trees shall be used to buffer the view of these facilities. Decorative walls and fences may be used in conjunction with plantings but may not be used by themselves to comply with buffering requirements. Exception: truck parking lots are exempt from parking bay buffer planting requirements.

RESPONSE: *The C-1 zone district is intended to provide the principal focus for civil and social functions while Subsection 17.90.00(D)(11) indicates that visible outdoor storage is incompatible with Sandy Style. Therefore, it is concluded that the proposed use is not compatible with the intent of the code and should provide buffer planting if approved. The proposal includes a 5-foot wide planter strip along the east and south property lines/frontages with a 25.5-foot by 25.5-foot planter area located in the subject property's southwest corner all of which has incorporated a combination of trees, shrubs, and ground cover that is expected to provide a landscaped buffer from the pedestrian pathways in conjunction with the ornamental fencing described within Subsection 17.92.90 of this report. ~~however, as required by Subsection 17.92.50 of this report, at least 75 percent the required landscaped area is not proposed to be planted with a combination of trees, shrubs, and evergreen cover nor is the proposed landscaping adequate to buffer the outdoor storage proposed which has been determined to be incompatible with the C-1 zone district per Subsection 17.90.00(D)(11). The applicant shall resubmit a landscape plan that provides a combination of trees, shrubs, or evergreen ground cover of for at least 75 percent of the required landscaping area for staff review and approval.~~*

17.92.90 SCREENING (HEDGES, FENCES, WALLS, BERMS)

Screening is uses where unsightly views or visual conflicts must be obscured or blocked and where privacy and security are desired. Fences and walls used for screening may be constructed of wood, concrete, stone, brick, and wrought iron, or other commonly used fencing/wall materials. Acoustically designed fences and walls are also used where noise pollution requires mitigation.

- A. Height and Opacity. Where landscaping is used for required screening, it shall be at least 6 ft. in height and at least 80 percent opaque, as seen from a perpendicular line of sight, within 2 years following establishment of the primary use of the site.
- B. Chain Link Fencing. A chain link fence with slats shall qualify for screening only if a landscape buffer is also provided in compliance with Section 17.92.00 above.

- C. Height Measurement. The height of hedges, fences, walls, and berm shall be measured from the lowest adjoining finished grade, except where used to comply with screening requirements for parking, loading, storage, and similar areas. In these cases, height shall be measured from the finished grade of such improvements. Screening is not permitted within vision clearance areas.
- D. Berms. Earthen berms up to 6 ft. in height may be used to comply with screening requirements. Slope of berms may not exceed 2:1 and both faces of the slope shall be planted with ground cover, shrubs, and trees.

RESPONSE: *The Landscape Plan (Exhibit N, Sheet C3) includes the installation of an ornamental fence. The proposed fence includes square column wraps at approximately 2-feet wide, standing 4-foot tall not including a 3-inch thick beveled concrete column cap. The proposed columns are to be wrapped with a stone veneer in Bucks County color which is consistent with the desired Sandy Style masonry standards for new development within the C-1 zone district. Between the columns the proposal includes installing two, horizontal trex rails (2-inch by 6-inch) to complete an ornamental fence bordering the subject property. The proposed rails are proposed to be brown which is also in keeping with the Sandy Style. **Should the proposed fencing be approved the applicant shall submit the proposed trex rail color for staff's final approval. The proposed color shall be compatible with Appendix C, Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the choice.** ~~The proposal includes installing a 3 foot tall chain link fence dividing the proposed pavement and landscaped area parallel to the west property line. Per Item B above a chain link fence is only permitted when a landscape buffer is provided in accordance with Chapter 17.92 which staff has concluded has not been met. The applicant shall resubmit a landscape plan that provides a combination of trees, shrubs, or evergreen ground cover for at least 75 percent of the required landscaping area for staff review and approval.~~*

CHAPTER 17.98 – PARKING, LOADING, AND ACCESS REQUIREMENTS

17.98.20 OFF-STREET PARKING REQUIREMENTS

RESPONSE: *Within the submitted narrative (Exhibit C) the applicant has identified the intended primary use of the subject property is to be for recreational vehicle parking and storage for the Funtime RV business located on the four lots directly west of the subject property. Using aerial photography, it can be observed that the existing Funtime RV site has approximately 18 standard parking spaces available for both patrons and employees of the site. The proposal includes a special variance to eliminate the required building and no additional employees have been identified therefore no increase in intensity associated with the parking demand will occur. The existing parking on the existing Funtime RV site is adequate to serve both the existing and proposed use of the site.*

17.98.40 SHARED USE OF PARKING FACILITIES

- A. Except for residential uses, required parking facilities may be located on an adjacent parcel of land or separated only by an alley, provided the adjacent parcel is maintained in the same ownership as the use it is required to serve.

RESPONSE: *The proposal indicates the intention of utilizing existing parking located on the four lots directly west of the subject property known as Funtime RV. The property is separated only by a property line and maintained under the same ownership therefore the proposed parking*

arrangement is permitted. Should ownership of either parcel be transferred each property will have to demonstrate it can independently conform to applicable parking standards at time of review.

17.98.130 PAVING

- A. Parking areas, driveways, aisles and turnarounds shall be paved with concrete, asphalt or comparable surfacing, constructed to city standards for off-street vehicle areas.
- B. Gravel surfacing shall be permitted only for areas designated for non-motorized trailer or equipment storage, propane or electrically powered vehicles, or storage of tracked vehicles.

RESPONSE: *Per Exhibit N, Sheet C3 ~~Exhibit D, Sheet C3~~ the proposal will incorporate new asphalt paving surface to be used for the outdoor product display or storage of merchandise which is proposed for the subject site.*

17.98.140 DRAINAGE

Parking areas, aisles and turnarounds shall have adequate provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way and abutting private property.

RESPONSE: *Section 17.98.140 contains requirements for drainage. All stormwater runoff shall be treated, detained and discharged in conformance with Section 13.18 and 13.20 of the Sandy Municipal Code (SMC) and the latest edition of the City of Portland Stormwater Management Manual (including Section 1.10 of the Source Control Manual).*

17.98.150 LIGHTING

Artificial lighting shall be provided in all required off-street parking areas. Lighting shall be directed into the site and shall be arranged to not produce direct glare on adjacent properties. Light elements shall be shielded and shall not be visible from abutting residential properties. Lighting shall be provided in all bicycle parking areas so that all facilities are thoroughly illuminated and visible from adjacent sidewalks or vehicle parking lots during all hours of use.

RESPONSE: *The proposal included on site lighting (Exhibit F) for the proposed outdoor display area. The measurements provided exceed the 1.5- 2.0 foot candle limitation. **The applicant shall provide staff with updated plans limiting the foot candle illumination not to exceed City limitations unless Planning Commission interprets the foot candle limitation differently.***

17.98.160 BICYCLE PARKING FACILITIES

Multi-family developments, industrial, commercial and community service uses, transit transfer stations, and park and ride lots shall meet the following standards for bicycle parking facilities. The intent of this section is to provide secure bicycle parking that is visible from a building's primary entrance and convenient to bicyclists.

- A. Location.

1. Bicycle parking shall be located on-site, convenient to primary building entrances, and have direct access to both the public right-of-way and to the main entrance of the principal structure.
2. Bicycle parking areas shall be visible from building interiors where possible.
3. For facilities with multiple buildings or parking lots, bicycle parking shall be located in areas of greatest use and convenience to bicyclists.
4. If the bicycle parking area is located within the vehicle parking area, the bicycle facilities shall be separated from vehicular maneuvering areas by curbing or other barrier to prevent damage to parked bicycles.
5. Curb cuts shall be installed to provide safe, convenient access to bicycle parking areas.

B. Bicycle Parking Space Dimensions.

1. Each required bicycle parking space shall be at least 2 1/2 feet by 6 feet. If covered, vertical clearance of 7 feet must be provided.
2. An access aisle of at least 5 feet wide shall be provided and maintained beside or between each row of bicycle parking. Vertical or upright bicycle storage structures are exempted from the parking space length.

C. Security.

1. Bicycle parking facilities shall offer security in the form of either a lockable enclosure in which the bicycle can be stored or a stationary object (i.e., a “rack”) upon which the bicycle can be located.
2. Racks requiring user-supplied locks shall accommodate both cable and U-shaped locks. Racks shall be designed and installed to permit the frame and both wheels to be secured, with removal of the front wheel, or the frame and one wheel to be secured, if both wheels remain on the bicycle.
3. Bicycle racks shall be securely anchored to the ground or a structure and shall be designed to hold bicycles securely by means of the bicycle frame.
4. All outdoor bicycle parking facilities shall provide adequate shelter from precipitation where possible.

C. Signing. Where bicycle facilities are not directly visible and obvious from the public right-of- way, entry or directional signs shall be provided to direct bicyclists from the public right-of- way to the bicycle parking facility.

E. Exemptions. Temporary street side sales and temporary uses such as fireworks stands, Christmas tree sales lots, single-family and two-family residences are exempt from the standards.

***RESPONSE:** The proposal includes a special variance to eliminate the required building and no additional employees have been identified therefore no increase in intensity associated with the parking demand will occur. The existing parking on the existing Funtime RV site is adequate to serve both the existing and proposed use of the site.*

CHAPTER 15.30 – DARK SKY ORDINANCE

All exterior lighting is required to conform to the requirements of this chapter.

RESPONSE: *The proposal included on site lighting (Exhibit F) for the proposed outdoor display area. The measurements provided exceed the 1.5- 2.0 foot candle limitation. **The applicant shall provide staff with updated plans limiting the foot candle illumination not to exceed City limitations unless Planning Commission interprets the foot candle limitation differently.** The dark sky ordinance requires that all new lighting be full-cutoff and downward facing. Based on recommendations from the Audubon Society of Portland and the International Dark-Sky Association, **all proposed lighting shall be full-cutoff and not exceed 3,000 Kelvins.***

III. SUMMARY AND CONCLUSION

Jerry Fisk submitted an application seeking approval for a minor conditional use permit to increase the subject property's outdoor product display/storage of merchandise to 100 percent when Subsection 17.42.10(C)(8) limits the amount of outdoor product display/storage on a site to 10 percent as an accessory use. The request has been made by the applicant as the proposed use is associated with the neighboring four properties, Funtime RV, and a building is not needed on this site. Most importantly, the applicant's request is to have outdoor product display and storage as the sole (and therefore primary) use on the site and not as a permitted accessory use; therefore, the request must be denied as Subsection 17.90.110(J)(1) prohibits exterior storage of merchandise and/or materials except as an accessory use. While the C-1 zone district would allow storage exceeding 10% as a minor conditional use, as the applicant proposes here, Section 17.90.110 states that if "a conflict exists between the requirements of Chapter 17.90 and any other code provision, Chapter 17.90 shall prevail; therefore, the application should be denied based on the use limitation of 17.90.110(J)(1). In addition, based on the approval criteria of Subsection 17.90.00(D)(3) and 17.90.00(D)(11) as well as the use limitations of Subsection 17.90.110(J)(1), staff has determined the proposed use does not meet several of the conditional use criteria for approval. As detailed within Chapter 17.68, criterion B, D and F have been determined not to have been met **based on the existing language and subsequent interpretation of the code**, due to the proposed use and associated development's incompatibility with the surrounding uses and character as it relates to site design, visual elements, and landscaping provisions.

In addition, no building is proposed to be on the subject property and the applicant seeks a Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard. The site will be used to store and display recreational vehicles in association with the retail establishment Funtime RV on the adjacent property to the west. To accommodate the display/storage of merchandise the property will include 90 percent pavement with 10 percent landscaping. The intent of the building orientation standard is to maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style.

Staff finds the proposed omission of the required building eliminates the ability for the site to incorporate required Sandy Style elements and therefore does not support the intent of the building orientation standard (Section 17.90.110(D)) or the guiding principles of Subsection 17.90.00(C). Specifically, removal of the required building does not emphasize a "village" scale as described in Subsection 17.90.00(C)(3) nor does it allow for the expression of Cascadian architectural elements, as described in 17.90.00(C)(4) and therefore violates the intent of the Sandy Style requirement. In addition, eliminating the required building frontage required for the site and introducing additional pavement for outdoor product display or storage of materials (in this case RVs) violates Subsections 17.90.00(D)(3)

and 17.90.00(11), both of which are defined as incompatible with the Sandy Style and are considered grounds for denial. In addition, by eliminating the required building the Sandy Style features associated with buildings found in Subsection 17.90.110 will not be applicable therefore the importance of an enhanced streetscape to achieve the guiding principles of Subsection 17.90.00(C) are important to review for compliance.

The above interpretation was completed based on the language of the code as written. However, another interpretation could be that the phrase “except as specifically authorized as a permitted accessory use” means not as code language specifically authorizes, but instead as the Planning Commission or City Council specifically authorize in a given case. Under that interpretation, the Planning Commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the Planning Commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use. Staff finds the proposed landscaping provides a decent combination of trees, shrubs, and ground cover that softens appearance of the proposed off-street activity from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development. In addition to the landscaping, the proposed ornamental fence provides an additional element that softens the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92.

IV. RECOMMENDATION

Staff Based on the language of the code as written, staff recommends the Planning Commission hold a public hearing to take public testimony regarding the proposal. Staff recommends the Planning Commission **deny** the requested conditional use, 100 percent outdoor product display or storage of merchandise. Additionally, staff recommends Planning Commission **deny** the requested Type III Special Variance from Subsection 17.90.110(D)(1) to eliminate the required 50 percent building frontage standard.

The Planning Commission has also been presented with the alternative interpretation (Exhibit M) of the phrase “except as specifically authorized as a permitted accessory use” to consider. This interpretation provides an alternative that if supported by the Commissioners, would provide justification for the Planning Commission to approve the requested conditional use, 100 percent outdoor product display or storage of merchandise. Should the Planning Commission choose to make this interpretation staff notes the proposed landscaping provides a combination of trees, shrubs, and ground cover that softens the appearance of the RVs from the public street while enhancing the pedestrian environment and adding to the intended “village” scaled development which is required per the regulations of Chapter 17.92 and addressed within the guiding principles of Subsection 17.90.00(C). In addition to the landscaping, the proposed ornamental fence provides an additional element of screening of the proposed use, outdoor product display or storage of merchandise, which assists in the development complying with the design regulations of Chapter 17.92 as well as adds a design element to enhance the pedestrian environment and “village” scale Chapter 17.90 strives for.

CONDITIONS:

Based on code review of the proposal, staff has identified several items either requiring additional information, modification and/or staff review should the Planning Commission approve the requested Minor Conditional Use Permit and/or Special Variance. Prior to submittal of a Building Permit the applicant shall complete the following:

~~In addition to the above conditions of approval, modifications as recommended in this report including but not limited to the following (required order of submission and completion of improvements/inspections will be detailed in the final order):~~

1. All public utility installations shall conform to the City's facilities master plans. The applicant shall pay plan review, inspection, and permit fees as determined by the Public Works Director.
2. The applicant shall ~~update the plans to include~~ **include** brick paver strips and scoring pattern matching the existing sidewalks in the Urban Renewal Area along Beers Avenue.
3. The existing curb cut on Proctor Blvd. shall be eliminated and replaced with standard Type C curb per 17.100.90(C) **of the SMC**. ~~To create visual symmetry, the applicant shall relocate the proposed tree centered between the existing trees located along the Proctor Blvd./US HWY 26 of the subject property.~~ All trees shall include tree ties with twine, loosely tied so as not to damage the trunk and shall be removed after one growing season (or a maximum of 1 year).
4. All landscape areas shall be irrigated using either manual or automatic system per the applicant's narrative (Exhibit C). The applicant shall demonstrate compliance with this proposal at time of building permit. Irrigation requirements of Subsection 17.92.40 shall be met.
5. **As determined necessary by City staff and ODOT;** The applicant shall install metal tube supports and signs per ODOT Standard Drawing TM490, Crosswalk Closure Detail (Exhibit I) on both the north and south sides of the west leg of Proctor Blvd./US HWY 26 and Beers Avenue.
6. The applicant shall update and submit plans indicating all of Proctor Blvd. frontage includes a six-foot wide sidewalk adjacent to the property line with the remaining right-of-way developed as a planter strip.
7. The existing ADA ramp located at the northwest corner of the Proctor/US HWY 26 and Beers Avenue intersections shall be replaced with ADA-compliant ramps and the new curb return transition to the improvements on Beers Avenue shall mirror the existing curb extension on the east side of Beers Avenue.
8. ~~The applicant shall complete one of the following options:~~
 - ~~A gate is proposed at the proposed Beers Ave. driveway approach to the sales lot. The proposed driveway throat shall be deep enough to allow an RV and tow vehicle to pull into the site without interfering with traffic on Beers Ave. or blocking the sidewalk, or~~
 - ~~A gate is proposed at the entrance to the sales lot. In order to prevent an RV and tow vehicle entering or exiting the site from interfering with traffic on Beers Ave. or blocking the sidewalk the gate shall be motorized and remotely operated to allow entrance and exit to and from the approach without stopping in the public right-of-way.~~
8. The applicant shall demonstrate compliance with street frontage improvements including lighting and storm drainage per 17.84.80 (G) of the SMC upon submission of construction plans. Detailed analysis of the proposed pedestrian and bicyclist improvements shall be completed during the construction plans review process.

- ~~9. The applicant shall record a cross access easement to the adjacent properties with state highway frontage with the County Assessor to facilitate shared access.~~
10. Any work to be completed within the highway right-of-way (Proctor Blvd./US HWY 26) requires an ODOT Miscellaneous Permit to be completed, including tree planting.
11. The applicant shall provide staff with an updated photometric plan limiting the foot candle illumination not to exceed City limitations. All proposed lighting shall be full-cutoff and not exceed 3,000 Kelvins.
12. Should ownership of either parcel be transferred each property will have to demonstrate it can independently conform to applicable parking standards at time of future development review.
13. All landscaping shall be continually maintained, including necessary watering, weeding, pruning, and replacing.
14. All parking, driveway and maneuvering areas shall be constructed of asphalt, concrete, or other approved material.
15. All stormwater runoff shall be treated, detained and discharged in conformance with Section 13.18 and 13.20 of the Sandy Municipal Code (SMC) and the latest edition of the City of Portland Stormwater Management Manual (including **sSection** 1.10 of the Source Control Manual).
- ~~16. The applicant shall submit a grading and erosion control permit and request an inspection of installed devices prior to any additional grading onsite. The grading and erosion control plan shall include a re-vegetation plan for all areas disturbed during construction of the subdivision. All erosion control and grading shall comply with Section 15.44 of the Municipal Code and as detailed below.~~
17. The applicant shall submit a plan identifying the locations of street lights along with specifications of proposed lighting fixtures to be reviewed in detail with construction plans. Full cut-off lighting shall be required and proposed street lighting on Proctor Blvd./US HWY 26 shall conform to the Streetlight Detail identified within Appendix F of the Development Code.
18. All on-site earthwork activities including any retaining wall construction should follow the requirements of the current edition of the Oregon Structural Specialty Code (OSSC). If the proposal includes a retaining wall, the applicant shall submit additional details on the proposed retaining wall for staff review and approval.
19. Site grading should not in any way impede, impound or inundate the adjoining properties. All the work within the public right-of-way and within the paved area should comply with American Public Works Association (APWA) and City requirements as amended. The applicant shall submit a grading and erosion control permit and request an inspection of installed devices prior to any additional grading onsite. All erosion control and grading shall comply with Section 15.44 of the Municipal Code.
20. If the applicant chooses to postpone street tree and/or landscaping installation, the applicant shall post a performance bond equal to 120 percent of the cost of the landscaping, assuring installation within 6 months. The cost of street trees shall be based on the street tree plan and at least \$400 per

tree. The cost of landscaping shall be based on the average of three estimates from three landscaping contractors; the estimates shall include as separate items all materials, labor, and other costs of the required action, including a two-year maintenance and warranty period.

21. The applicant will be required to pay for the cost of all street signs and the City will install the street signs. Signs to be determined during the construction review period.
22. The applicant shall submit the proposed trex rail color for staff's final approval. The proposed color shall be compatible with Appendix C, Building Color Palette or be of a natural stain/finish. Once approved the applicant shall update the plans to reflect the approved color.
23. The applicant shall include a 5-foot in depth landscape buffer to include evergreen plantings at a minimum of 6-feet tall to be located the length of the north property line in order to create an additional buffer for the neighboring, residential development to the north. Updated plans required to be submitted to Planning for review and approval prior to any grade work in the site.
24. The applicant shall choose an alternative tree species from the proposed Red Barron Crabapple that is not a fruit bearing tree and update the plans to include the change.

EXHIBIT A



CITY OF SANDY, OREGON

LAND USE APPLICATION FORM

(Please print or type the information below)

Planning Department
39250 Pioneer Blvd.
Sandy OR 97055
503-668-4886

Name of Project FUN TIME RV
 Location or Address 38477/38415 PROCTOR BLVD, SANDY OR 97055
 Map & Tax Lot Number T 25, R 4E, Section 138C; Tax Lot(s) 3900
 Plan Designation Retail/Commercial Zoning Designation C-1 Acres 0.33

Request:

Type II Design Review to allow for installing asphalt surface to allow for additional storage and parking of recreational vehicles.

I am the (check one) owner lessee of the property listed above and the statements and information contained herein are in all respects true, complete and correct to the best of my knowledge and belief.

Applicant <u>Firwood Design Group</u>	Owner <u>Jenny Fisk</u>
Address <u>359 E. Historic Col. River Hwy</u>	Address <u>18605 SW Pacific Dr.</u>
City/State/Zip <u>TROUTDALE, OR 97060</u>	City/State/Zip <u>TUALATIN, OR 97062</u>
Phone <u>503-668-3737</u>	Phone <u>503-925-9629, 503-827-1900</u>
Email <u>Kg@firwooddesign.com</u>	Email <u>JD@funtimervinc.com</u>
Signature	Signature

If signed by Agent, owner's written authorization must be attached.

File No.	Date <u>7/10/18</u>	Rec. No.	Fee \$ <u>1500⁰⁰</u>
Type of Review (circle one): Type I <u>Type II</u> Type III Type IV			

24E13BC039AA



**SUPPLEMENTAL APPLICATION
DESIGN REVIEW / CONDITIONAL USE PERMIT**

(Please print or type the information below)

Planning Department
39250 Pioneer Blvd.
Sandy OR 97055
503-668-4886

Name of Project FUN TIME RV

Location or Address 38477/38415 PROCTOR BLVD., SANDY OR 97055

Type of Business RV SALES

Products Manufactured NONE.

Hours of Operation MON-SAT. 8:30 A.M. - 5 P.M. SUN. 10 A.M. - 5 P.M.

No. of Employees per Shift N/A.

Total Site Size 0.33 ACRES Total Bldg. Square Footage N/A

Project Summary

No building is proposed, only paving the currently vacant parcel to allow for additional parking AND STORAGE OF RECREATIONAL VEHICLES.

USES WITHIN BUILDING (SQUARE FOOTAGE)

Offices: _____ Shop: _____ Storage: 14,000 sq. ft. +/-
OUTDOOR VEHICLE STORAGE

Kitchen: _____ Laundry: _____ Rest Rooms: _____

Other: _____

OCCUPANCY & CONSTRUCTION TYPE

(List all occupancies by square footage)

UBC Occupancy Rating: N/A

UBC Type of Construction: N/A

Will fire sprinklers be installed in the building? Yes No

SITE ANALYSIS DATA		
Type	Lot Coverage (Square Feet)	Lot Coverage (Percent of Site)
Buildings		
Parking Lots and Driveways		
Private Walks & Pedestrian Ways		
Landscaping – Improved Area		
Landscaping – Natural areas		
Storm Water Detention, Retention & Bioswale Areas		
Other (describe)		
Other (describe)		

CONSTRUCTION MATERIAL DETAILS

Color & Type of Siding Materials:

Color & Type of Trim Materials:

Color & Type of Roof Materials:

Color & Type of Exterior Doors:

Color & Type of Exterior Stairs, Balconies & Railings:

Trash & Recycling Enclosure (describe type, colors, height):

Type of Lighting Fixtures (describe):

Pole: _____

Wall-Mounted: _____

Fencing (height and type):

Mailboxes (location and type):

Private Pedestrian Walkways (type of surfacing):

Recreational Amenities (describe type and location):

Other Site Elements (describe):

RETAINING WALLS LESS THAN 4 FT IN HEIGHT.

STORM MANAGEMENT SYSTEM - TREATMENT AND

UNDERGROUND DETENTION

TYPE OF IRRIGATION SYSTEM

Describe type and brand of irrigation system to be installed. Formal irrigation plans must be submitted with construction plans.

N/A

PROPOSED SOIL AMENDMENTS

Describe soil conditions and proposed plans for soil treatment & amendments:

N/A

ADDITIONAL LANDSCAPING INFORMATION

Street Trees.

LANDSCAPE MATERIALS

Quantity	Type – Include botanical and common names. Plants must be keyed to landscape plan.	Size	Height	Spacing
1	CHANTICLEER PEAR TREE	2"		Per Plan

EXHIBIT B



First American

Date of Production: Tuesday, January 15, 2019

The ownership information enclosed is time sensitive and should be utilized as soon as possible.

This mailing list was produced with taxlot data from the Portland Metro regional government.

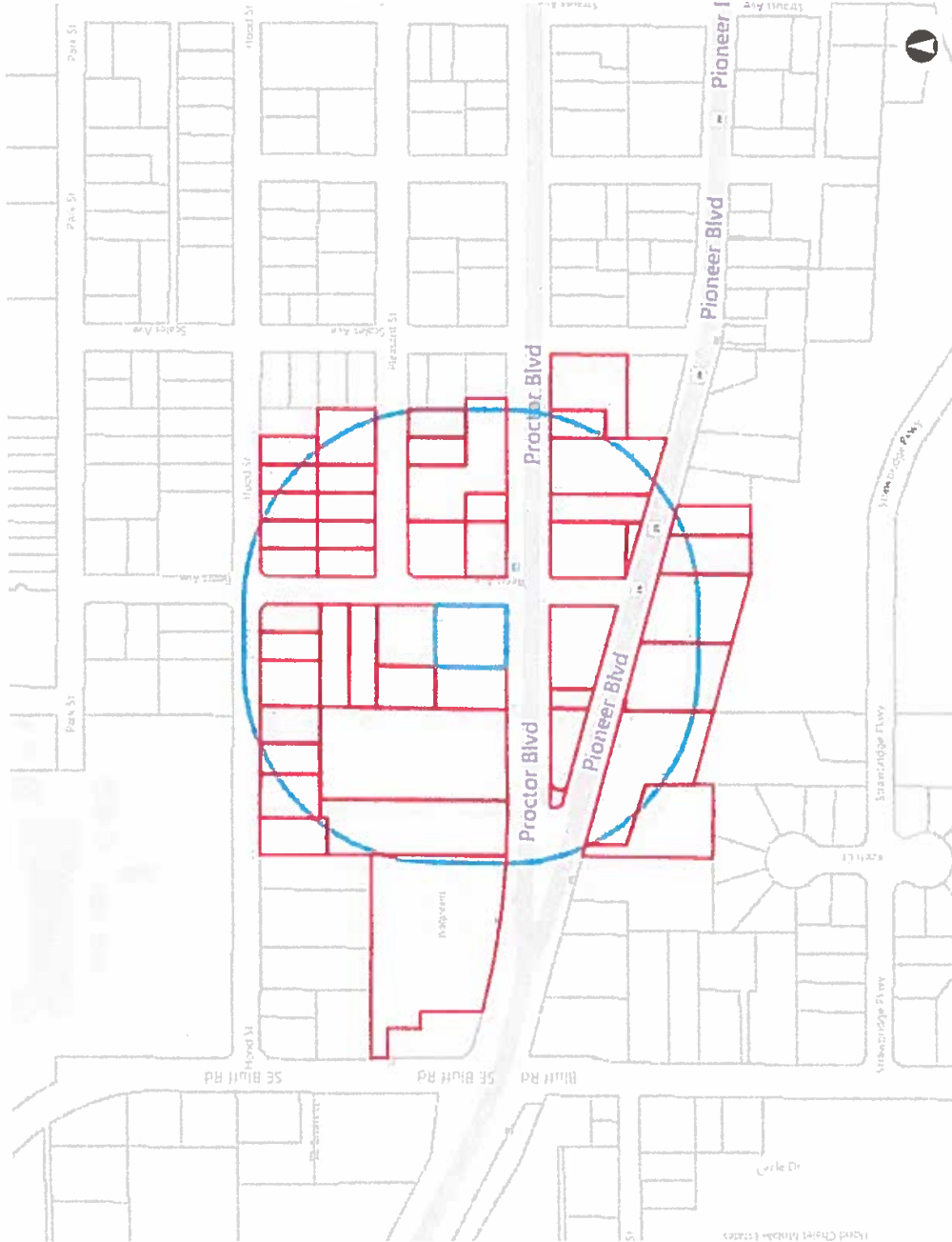
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350 ft buffer - 38477 Proctor Blvd



- Subject
- Radius
- Radius Properties



1/15/2019

Notes
Sandy, OR

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Customer Service Department
Phone: 503.219.8746(TRIO)
Email: cs.oregon@firstam.com
Report Generated: 1/15/2019

Ownership

Legal Owner(s): Red One Holdings Llc	Parcel #: 24E13BC03900
Site Address: 38477 Proctor Blvd Sandy, OR 97055	APN: 00657621
Mailing Address: 18605 SW Pacific Hwy Tualatin, OR 97062	County: Clackamas

Property Characteristics

Bedrooms: 0	Year Built: 0	Lot SqFt: 14425
Total Bathrooms: 0	Building SqFt: 0	Lot Acres: 0.33
Full Bathrooms: 0	First Floor SqFt: 0	Roof Type:
Half Bathrooms:	Basement Sqft: 0	Roof Shape:
Units: 0	Basement Type:	Porch Type:
Stories:		Building Style:
Fire Place: N		Garage:
Air Conditioning:		Garage SqFt: 0
Heating Type:		Parking Spots: 0
Electric Type:		Pool:

Property Information

Land Use: VACANT	Neighborhood:
Improvement Type: Commercial-	School District: Oregon Trail Sd 46
Legal Description: 182 SANDY PT BLK C	Zoning: C1

Assessor & Tax

Market Land: \$243,904	Taxes: \$1,805.68
Market Total: \$243,904	% Improved:
Market Structure: \$0	Levy Code: 046048
Assessed Total: \$104,283	Millage Rate: 17.4034

Sale History

Last Sale Date: 3/6/2017	Doc #: 2017-014977	Last Sale Price: \$1,400,000
Prior Sale Date:	Prior Doc #:	Prior Sale Price: \$0

Mortgage

1st Mortgage Date: 3/6/2017	Doc #: 2017-014978	
1st Mortgage Type:	1st Mortgage Lender: Carlson Proctor Property Llc	1st Mortgage: \$0
2nd Mortgage Type:		2nd Mortgage: \$0

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24E13BC03200	Walgreen Co	Po Box 1159	Deerfield	IL	60015	38241 Proctor Blvd	Sandy	OR	97055
24E13BC04900	Lowell & April Weare	38630 Hood St	Sandy	OR	97055	38634 Hood St	Sandy	OR	97055
24E13BC04500	Oscar Williams	Po Box 66831	Portland	OR	97290	17225 Beers Ave	Sandy	OR	97055
24E13BC02600	Jennifer Wood	38356 Hood St	Sandy	OR	97055	38356 Hood St	Sandy	OR	97055
24E13BC04700	Jon & Erin Woodard	38540 Hood St	Sandy	OR	97055	38540 Hood St	Sandy	OR	97055
24E13BC05600	Margie Workinger & Urleta Nestoza F	38525 Pleasant St	Sandy	OR	97055	38525 Pleasant St	Sandy	OR	97055
24E13BC05700	Anna Zell	28490 SE Curtin Rd	Estacada	OR	97023	17326 Beers Ave	Sandy	OR	97055

TAXID	Owner Name	Mailing Address	Mail City	Mail State	Mail Zip	Site Address	Site City	Site State	Site Zip
24E13CB01700	Adventure Holdings Llc	426 SE Grand Ave	Portland	OR	97214	38454 Pioneer Blvd	Sandy	OR	97055
24E13CB00400	Andys Ice Cream Lic	26814 SE Stark St	Troutdale	OR	97060	17400 Beers Ave	Sandy	OR	97055
24E13CB00401	Andys Ice Cream Lic	26814 SE Stark St	Troutdale	OR	97060	No Site Address	Sandy	OR	97055
24E13CB00500	Andys Ice Cream Lic	26814 SE Stark St	Troutdale	OR	97060	38560 Proctor Blvd	Sandy	OR	97055
24E13CB06700	William Baker	7122 SE Short Rd	Gresham	OR	97080	38565 Proctor Blvd	Sandy	OR	97055
24E13BC04300	Oregoner Lic	16510 SE Bluff Rd	Sandy	OR	97055	38462 Hood St	Sandy	OR	97055
24E13BC03500	Carlson Chevrolet Co	Po Box 636	Sandy	OR	97055	38361 Proctor Blvd	Sandy	OR	97055
24E13CB02100	Carlson Pioneer Property Llc	Po Box 820	Sandy	OR	97055	38400 Pioneer Blvd	Sandy	OR	97055
24E13CB02000	City Of Sandy	39250 Pioneer Blvd	Sandy	OR	97055	38362 Pioneer Blvd	Sandy	OR	97055
24E13CB00300	Elaine Kitchen Family Llc	37071 SE Kelso Rd	Boring	OR	97009	17475 Beers Ave	Sandy	OR	97055
24E13BC02500	Aranca Ferraria	38376 Hood St	Sandy	OR	97055	38376 Hood St	Sandy	OR	97055
24E13BC04100	Alan & Therese Fleischman	38720 Proctor Blvd STE 101	Sandy	OR	97055	17275 Beers Ave APT A	Sandy	OR	97055
24E13BC04200	Quentin Karstetter	Po Box 994	Sandy	OR	97055	17235 Beers Ave	Sandy	OR	97055
24E13BC04701	Grant Fretz	38556 Hood St	Sandy	OR	97055	38556 Hood St	Sandy	OR	97055
24E13BC05400	Anthony & Myrna Gonzales	Po Box 21	Sandy	OR	97055	38635 Pleasant St	Sandy	OR	97055
24E13BC05601	John Gough	38535 Pleasant St	Sandy	OR	97055	38535 Pleasant St	Sandy	OR	97055
24E13BC04000	Granstrom William L Of	2617 Jolie Pointe Rd	West Linn	OR	97068	17321 Beers Ave	Sandy	OR	97055
24E13BC04800	Doris Johnson	33991 SE Lusted Rd	Gresham	OR	97080	38574 Hood St	Sandy	OR	97055
24E13BC02300	Christy & Jason Kinsey	415 NE 4th St	Gresham	OR	97030	38432 Hood St	Sandy	OR	97055
24E13CB00100	Kathryn Leathers	255 Depot St	Fairview	OR	97024	38422 Proctor Blvd	Sandy	OR	97055
24E13CB00700	Lila Leathers	255 Depot St	Fairview	OR	97024	38600 Proctor Blvd	Sandy	OR	97055
24E13CB00901	Leathers Limited Ptnrship	255 Depot St	Fairview	OR	97024	No Site Address	Sandy	OR	97055
24E13CB04500	Leathers Limited Ptnrship	255 Depot St	Fairview	OR	97024	No Site Address	Sandy	OR	97055
24E13CB00800	Yong & Young Lim	740 SE 25th St	Gresham	OR	97080	38636 Proctor Blvd	Sandy	OR	97055
24E13BC05501	Jane McCarrel	21575 E Lodge Ln	Rhododendron	OR	97049	38565 Pleasant St	Sandy	OR	97055
24E13BC05500	Debra Mills	Po Box 456	Boring	OR	97009	38565 Pleasant St	Sandy	OR	97055
24E13BC02400	Richard Minor	12255 SE Wise Rd	Damascus	OR	97089	38422 Hood St	Sandy	OR	97055
24E13BC06000	Greg & Linda Osburn	15633 SE 329th Ave	Boring	OR	97009	38620 Pleasant St	Sandy	OR	97055
24E13BC06001	Greg & Linda Osburn	15633 SE 329th Ave	Boring	OR	97009	38640 Pleasant St	Sandy	OR	97055
24E13BC03600	Red One Holdings Lic	18605 SW Pacific Hwy	Tualatin	OR	97062	38415 Proctor Blvd	Sandy	OR	97055
24E13BC03800	Red One Holdings Lic	18605 SW Pacific Hwy	Tualatin	OR	97062	No Site Address	Sandy	OR	97055
24E13BC03900	Red One Holdings Lic	18605 SW Pacific Hwy	Tualatin	OR	97062	38477 Proctor Blvd	Sandy	OR	97055
24E13BC04001	Red One Holdings Lic	18605 SW Pacific Hwy	Tualatin	OR	97062	No Site Address	Sandy	OR	97055
24E13BC04400	Ruth Clark	38472 Hood St	Sandy	OR	97055	38472 Hood St	Sandy	OR	97055
24E13BC04600	Donna Ryan	38526 Hood St	Sandy	OR	97055	38526 Hood St	Sandy	OR	97055
24E13CB00200	Sandy Post #4273 Vfw	Po Box 404	Sandy	OR	97055	38452 Proctor Blvd	Sandy	OR	97055
24E13CB01400	Susan Tolle	1551 SW Maplecrest Dr	Portland	OR	97219	No Site Address	Sandy	OR	97055
24E13CB01500	Susan Tolle	1551 SW Maplecrest Dr	Portland	OR	97219	No Site Address	Sandy	OR	97055
24E13CB01600	United Pentecostal Ch Sandy	Po Box 1270	Sandy	OR	97055	38530 Pioneer Blvd	Sandy	OR	97055
24E13BC06500	Us Bank Properties	2800 E Lake St	Minneapolis	MN	55406	38561 Proctor Blvd	Sandy	OR	97055
24E13BC06800	Usmw #3 Lic	36370 Industrial Way	Sandy	OR	97055	38525 Proctor Blvd	Sandy	OR	97055

24E13CB01700
Adventure Holdings Llc
426 SE Grand Ave
Portland, OR 97214

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37071 SE Kelso Rd
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24E13BC05601
John Gough
38535 Pleasant St
Sandy, OR 97055

24E13BC02300
Christy & Jason Kinsey
415 NE 4th St
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24E13CB00901
Lila Leathers
255 Depot St
Fairview, OR 97024

24E13BC05501
Jane Mccarrel
21575 E Lodge Ln
Rhododendron, OR 97049

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Sandy, OR 97055

24E13BC06500
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2800 E Lake St
Minneapolis, MN 55406

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Usmw #3 Lic
36370 Industrial Way
Sandy, OR 97055

24E13BC03200
Walgreen Co
Po Box 1159
Deerfield, IL 60015

24E13BC04900
Lowell & April Weare
38630 Hood St
Sandy, OR 97055

24E13BC04500
Oscar Williams
Po Box 66831
Portland, OR 97290

24E13BC02600
Jennifer Wood
38356 Hood St
Sandy, OR 97055

24E13BC04700
Jon & Erin Woodard
38540 Hood St
Sandy, OR 97055

24E13BC05600
Margie Workinger & Urieta Nestoza F
38525 Pleasant St
Sandy, OR 97055

24E13BC05700
Anna Zell
28490 SE Currin Rd
Estacada, OR 97023

EXHIBIT C

Date: February 7, 2019

To: James Cramer, City of Sandy

From: Tracy Brown, Tracy Brown Planning Consultants, LLC

Re: Fun Time RV (File No. 18-030) Response to Incompleteness Letter 8/28/18

Please find supplemental items needed to allow you to deem the above referenced file complete and process the application. As you requested, six copies of revised materials have been provided. The revised plan set and narrative are a result of email correspondence you and I had regarding the project over the last two months. As you are aware, this application was originally submitted on July 10, 2018, and it was not deemed incomplete until August 28, 2018, 49 days later. Based on your December 20, 2018, email, you indicated the 181 day application submission deadline was extended to February 25, 2019.

We are requesting the following approvals with the revised application:

- Type II minor conditional use permit;
- Type II design review; and
- Type III special variance to Section 17.90.110(D)(1).

Previously I thought we would need a Type I Adjustment to the required 10 percent onsite landscaping requirement but the proposal is now designed to comply with this standard. The following items have been included with this submittal:

- Updated Notification List and Mailing Labels within 350 feet
- Reduced size Plan Set
- Updated Project Narrative
- Revised Plan Set
- Revised Stormwater Report
- Pre-application Conference Notes and Correspondence between applicant's representative and James Cramer, City of Sandy

The only item I am still waiting on is the lighting plan. We have been pursuing obtaining a plan from a qualified lighting contractor for over a month now and we are committed to providing this information to your office as soon as it is available. If we are unsuccessful in receiving a plan soon, we ask that the City deem the application complete without this information and include a condition requiring submittal of this plan with the building permit.

**Revised
Project Narrative
for**

File No. 18-030DR

**Funtime RV
Minor CUP, Design Review, and Special Variance**

38477 Proctor Blvd., Sandy, Oregon



**Prepared by
Tracy Brown Planning Consultants, LLC**

February 2019

I. General Project Description

This narrative is intended to replace the previously submitted narrative for this project submitted on July 10, 2018, (File No. 18-030 DR). A pre-application conference on the project was originally held with the former owner of the property to review the project on September 8, 2016. Based on the summary notes from this meeting, the current owner, FunTime RV submitted an application for the proposed project on July 10, 2018. Following a pre-application meeting with the current owner on August 8, 2018, the City deemed the application incomplete on August 28, 2018. The applicant then attended another pre-application conference with the City on November 26, 2018. Following this meeting additional correspondence was completed between the applicant's representative and the City in order to clarify items from the pre-application notes and determined required application submittals.

The project site is located at 38477 Proctor Blvd. (Township 2 South, Range 4 East, Section 13BC, tax lot 3900 of the Willamette Meridian). The project site contains approximately 13,985 square feet (0.32 acres) and has never been developed. The topography of the site is generally flat with about seven feet of elevation difference between the Southeast corner and the Northwest corner of the site. The entire lot is currently planted in grass and a split rail fence encloses the South and east boundaries of the property.

The site is zoned C-1, Central Business District and properties adjacent to the subject property on all sides are also zoned C-1. The project is proposed to expand the area of recreational vehicle parking and storage for the FunTime RV business located on the four lots directly west of the subject property (24E13BC tax lots 3500, 3600, 3800, and 4001). These properties are currently being used for the same purpose proposed with this application. Tax lot 3600 (38415 Proctor Blvd,) contains the office, repair shop, and parking for the business. The proposed application is an expansion of this use but these properties are not part of this application.

The primary FunTime RV business located at 38415 Proctor Blvd. to the west of the subject property received Conditional Use Permit approval (File No. 09-001 CUP) under the former business owner, Fred's RV. This approval allowed for the sales and service of automobiles, trucks, and recreational vehicles, etc., on all of the properties currently being occupied by Fun Time RV. No changes are proposed to these properties.

Site improvements include construction of a retaining wall in order to level the site. This wall extends for the majority of the common property between the existing business and the subject property (except for 27 feet). Other improvements include an underground stormwater detention facility, installation of a planter with curb, site paving, and landscaping. The applicant is also proposing to install a gated, limited-use access for the site onto Beers Avenue. This access is proposed only to facilitate periodic maneuvering of recreational vehicles on and off the site. The existing curb cut on Proctor Blvd. will be closed and landscape materials installed in this location and the Beers Ave. frontage will be improved with this proposal.

II. Application Approval Requests

The applicant requests the following approvals with this application:

- Type II minor conditional use permit;
- Type II design review to pave the site to be used for recreational vehicle display and sales; and a
- Type III special variance to Section 17.90.110(D)(1).

III. Items Submitted With This Application

- Land Use Application
- Notification List and Mailing Labels
- Exhibit A - Project Narrative
- Exhibit B - Civil Plans
 - Sheet 1 - Cover Sheet
 - Sheet 2 - Existing Conditions/Demo Plan
 - Sheet 3 - Site, Grading Plan & Landscaping Plan
 - Sheet 4 - Concept RV Display Layout
 - Sheet 5 - Beers Avenue Plan & Profile
 - Sheet 6 - Utility Plan
 - Sheet 7 - Details - City of Sandy
 - Sheet 8 - Details - City of Sandy
 - Sheet 9 - Details - ODOT
 - Sheet 10 - Details - Detention Facility
- Exhibit C - Preliminary Stormwater Report (previously submitted)
- Exhibit D - Pre-application Conference Notes and Correspondence between applicant's representative and James Cramer, City of Sandy

IV. Review of Applicable Approval Criteria

Development applications are required to meet standards set forth in the Sandy Development Code, codified as Title 17 of the Municipal Code. The following section addresses all applicable review criteria. Pertinent code provisions are cited below in plain text followed by a response identifying how the proposal complies with this standard in *italics*.

CHAPTER 17.42 - CENTRAL BUSINESS DISTRICT (C-1)

17.42.00 - INTENT

This district is intended to provide the community with a mix of retail, personal services, offices and residential needs of the community and its trade area in the city's traditional commercial core. This district is not intended for intensive automobile or industrial uses. This district is intended to provide the principal focus for civic and social functions within the community.

This commercial district is intended for civic uses and to provide all basic services and amenities required to keep the downtown the vital center of our community. While the district does not permit new low density building types, it is not intended to preclude dwelling units in buildings containing commercial activities. All development and uses

shall be consistent with the intent of the district, as well as compatible with the space, access and exposure constraints and opportunities of the central city.

Response: The applicant proposes a permitted outright use in the C-1 zone as listed in Section 17.42.10 (B)(1)(a) below. As specified in the intent language of this section, "The district is not intended for intensive automobile or industrial uses". The proposed use is not an "intensive automobile" or is it an "industrial use". "Intensive automobile" uses are those uses such as drive-through restaurants or coffee kiosks that generate high traffic volumes. The proposed use has a low trip generation rate in contrast to these uses. In addition, the proposed use is not an industrial use, but is commercial in nature.

17.42.10 - PERMITTED USES

B. Primary Uses Permitted Outright - Commercial in buildings with up to 30,000 square feet of gross floor area and without drive-through facilities:

1. Retail uses, including but not limited to:

a. Automotive trailer, recreational vehicle, motorcycle sales and rental;

Response: The proposed use is for the storage and display of recreational vehicles as permitted outright in this zone. The city determined at the pre-application that the proposed use is a permitted use in the C-1 zoning district.

17.42.30 - DEVELOPMENT STANDARDS

A.

Commercial	
Lot Area	No minimum
Lot Dimension	No minimum
Setbacks	No minimum ⁴ ; maximum 10 ft.
Lot Coverage	No maximum
Landscaping	10% minimum (includes required civic space in Section 17.90.110.)
Structure Height	45 ft. maximum
Off-Street Parking	See Chapter 17.98
Design Review Standards	See Section 17.90.110

Response: No structures are proposed with this application. On-site improvements include paving, landscaping, lighting, and construction of an underground stormwater treatment and detention facility only. Ten percent of the site will be landscaped including a minimum five foot perimeter landscape strip. As shown on submitted plans, the proposal complies with all development standards in this section.

B. Special Setbacks - Side or Rear Yard Abutting a More Restrictive District.

Response: The subject property is bordered on all sides by property that is also zoned C-1, Central Business District and does not require any special setbacks.

17.42.20 MINOR CONDITIONAL USES AND CONDITIONAL USES

A. Minor Conditional Uses:

3. Outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area;

Response: The applicant proposes using the site to display recreational vehicles over the majority of the site and will exceed the 10 percent outdoor display standard permitted outright. As such, the criteria for a Minor Conditional Use Permit are reviewed in Chapter 7.68 below.

Chapter 17.68 - CONDITIONAL USES

17.68.00 - INTENT

Certain uses listed in each zoning district require special review to determine what their effects may be to the surrounding properties, neighborhood, and community as a whole. The Minor Conditional Use Permit (Type II) and Conditional Use Permit (Type III) processes provide an opportunity to allow a use when potential adverse effects can be mitigated or deny a use if concerns cannot be resolved.

Response: The applicant requests a Minor Conditional Use Permit to exceed the 10 percent maximum outdoor display standard as provided in Section 17.42,20(A)(3).

17.68.20 - REVIEW CRITERIA

The Planning Director (Minor Conditional Use Permit) through a Type II process or the Planning Commission (Conditional Use Permit) through a Type III process may approve an application, approve with modifications, approve with conditions, or deny an application for a conditional use permit after a public hearing. The applicant must submit evidence substantiating that all requirements of this code relative to the proposed use are satisfied and consistent with the purposes of this chapter, policies of the Comprehensive Plan, and any other applicable policies and standards adopted by the City Council. The following criteria and compatibility factors shall be considered:

- A. The use is listed as either a minor conditional use or conditional use in the underlying zoning district or has been interpreted to be similar in use to other listed conditional uses.

Response: The applicant's request to provide "Outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area" is identified as a Minor Conditional Use in Section 17.74.20(A)(3).

- B. The characteristics of the site are suitable for the proposed use considering the size, shape, location, topography, and natural features.

Response: The subject property is suitable for the proposed use because it is located adjacent to an existing RV sales business, is highly visible, generally flat, and does not contain any natural constraints. The proposal to site recreational vehicles on the subject property complies with this criteria.

- C. The proposed use is timely considering the adequacy of the transportation systems, public facilities and services existing or planned for the area affected by the use.

Response: The subject property is located within the downtown area and is adjacent to Proctor Blvd. and Beers Ave., both functioning roads. The applicant intends to close the existing curb cut on Proctor Blvd. in front of the site and to complete frontage improvements adjacent to the property on Beers Ave. The proposal does

not require any public utilities except necessary to received stormwater after it is detained and cleaned onsite. The proposal is timely given the adequacy of items in this criteria.

- D. The proposed use will not alter the character of the surrounding area in a manner which substantially limits, precludes, or impairs the use of surrounding properties for the primary uses listed in the underlying zoning district.

Response: The proposed use is essentially an expansion of a use that has existed on adjacent properties for a number of years. Expansion of this use on the subject property will not alter the character of the surrounding alter from what already exists or impair the use of surrounding properties in any way.

- E. The proposed use will not result in the use of land for any purpose which may create or cause to be created any public nuisance including, but not limited to, air, land, or water degradation, noise, glare, heat, vibration, or other considerations which may be injurious to the public health, safety, and welfare.

Response: The proposed use is for the storage and display of recreational vehicles only. By its very nature, the proposed use will not create or cause any public nuisance.

- F. The proposed use will be reasonably compatible with existing or planned neighboring uses based on review of the following:

1. Basic site design (organization of uses on the site)
2. Visual elements (scale, structural design and form, materials, and so forth)
3. Noise
4. Noxious odors
5. Lighting
6. Signage
7. Landscaping for buffering and screening
8. Traffic
9. Effects on off-street parking
10. Effects on air quality and water quality

Response: The proposal will be compatible with existing and planned neighboring uses in all ways. Development of the site will include construction of a short wall along a portion of the western property line in order to level the site, paving, installing a curb, landscape planter, short fence, landscaping, and frontage improvements. Site lighting will be installed in compliance with Dark Sky standards. The proposal will not generate noise and noxious odors and will only marginally generate additional traffic to the business located next door. The proposal will have no effect on off-street parking, or air and water quality. The proposal complies with this criteria.

CHAPTER 17.84 - IMPROVEMENTS REQUIRED WITH DEVELOPMENT

17.84.20 TIMING OF IMPROVEMENTS

- A. All improvements required by the standards in this chapter shall be installed concurrently with development.

Response: All improvements will be completed prior to occupancy.

17.84.30 PEDESTRIAN AND BICYCLIST REQUIREMENTS

A. Sidewalks shall be required along both sides of all arterial, collector, and local streets, as follows:

1. Sidewalks shall be a minimum of 5 ft. wide on local streets. The sidewalks shall be separated from curbs by a tree planting area that provides separation between sidewalk and curb, unless modified in accordance with Subsection 3 below.

Response: Beers Ave. is identified in the City's Transportation System Plan as a local street. Frontage improvements along this street will be constructed in accordance with city standards. The majority of the Proctor Blvd. frontage is already improved. The only improvement to this frontage will be closing the existing curb cut and constructing a new curb in this location and planting landscape materials in this location.

4. The timing of the installation of sidewalks shall be as follows:

Response: Sidewalk improvements are proposed to be completed prior to occupancy.

17.84.50 STREET REQUIREMENTS

A. Traffic evaluations may be required of all development proposals in accordance with the following:

1. A proposal establishing the scope of the traffic evaluation shall be submitted for review to the City Engineer. The evaluation requirements shall reflect the magnitude of the project in accordance with accepted traffic engineering practices. Large projects should assess all nearby key intersections. Once the scope of the traffic evaluation has been approved, the applicant shall present the results with and an overall site development proposal. If required by the City Engineer, such evaluations shall be signed by a Licensed Professional Civil Engineer or Licensed Professional Traffic Engineer licensed in the State of Oregon.
2. If the traffic evaluation identifies level-of-service conditions less than the minimum standard established in the Transportation System Plan, improvements and funding strategies mitigating the problem shall be considered concurrent with a development proposal.

Response: A traffic evaluation was not been requested by the City or has one been submitted with this application. The proposed use is for the display of recreational vehicles only. Parking for employees and customers associated with this use will continue to be accommodated at the primary Funtime RV property located west of the subject property.

17.84.60 PUBLIC FACILITY EXTENSIONS

A. All development sites shall be provided with public water, sanitary sewer, broadband (fiber), and storm drainage.

Response: The proposed use will not require installation of any public facilities except for the connection of the proposed stormwater system to the public line located near the corner of Beers Ave. and Pleasant St. This facility is shown on submitted plans. No water, sanitary sewer, or fiber service is proposed or required.

17.84.80 FRANCHISE UTILITY EXTENSIONS

These standards are intended to supplement, not replace or supersede, requirements contained within individual franchise agreements the City has with providers of electrical power, telephone, cable television, and natural gas services (hereinafter referred to as “franchise utilities”).

Response: No franchise utilities are required.

17.84.90 LAND FOR PUBLIC PURPOSES

- A. Easements for public sanitary sewer, water, storm drain, pedestrian and bicycle facilities shall be provided whenever these facilities are located outside a public right-of-way in accordance with the following:

Response: No easements for the purposes identified in this section are anticipated.

CHAPTER 17.90 DESIGN STANDARDS

17.90.10 APPLICABILITY

The provisions of this chapter apply to all zones and uses as follows except as specified in Sections 17.90.10(B), (C), (D), (E), and (F) below:

- A. All construction within a Commercial or Industrial Zoning District or a non-residential use in a Residential Zoning District including the following:

1. New construction;
2. Replacement of a building that is destroyed as specified in Section 17.08.30;
3. Addition to an existing building;
4. Exterior alterations other than general maintenance on an existing building;
5. Site improvements including changes to landscaping, parking, civic spaces, etc.

Response: The proposal includes paving the site in order to display and sell recreational vehicles. Because the proposal is limited in scope and does not include construction or renovation of a building, the majority of code standards in this Chapter are not applicable and have not been addressed in this narrative.

17.90.110 DOWNTOWN AND VILLAGE COMMERCIAL (C-1 AND C-3) DESIGN STANDARDS

A. Site Layout and Vehicle Access

Intent: To provide for compact, walkable development, and to design and manage vehicle access and circulation in a manner that supports pedestrian safety, comfort and convenience. (Figures 17.90.110-C and 17.90.110-D)

1. All lots shall abut or have cross access to a dedicated public street.

Response: The subject property abuts both Proctor Blvd. and Beers Ave.

2. All lots that have access to a public alley shall provide for an additional vehicle access from that alley.
3. Off-street parking shall be located to the rear or side of buildings with no portion of the parking lot located within required setbacks or within 10-feet of the public right-of-way, as shown in Figure 17.90.110(C). When access must be provided directly from a public right-of-way, driveways for ingress or egress shall be limited to one per 150 ft. For lots with frontage of less than 150 ft. or less, shared access may be required.

4. Adjacent parking lots shall be connected to one another when the City determines it is practicable to do so. Developments shall avoid creating barriers to inter-parcel circulation.
5. Urban design details, such as raised or painted pedestrian crossings and similar devices incorporating changes in paving materials, textures or color, shall be used to calm traffic and protect pedestrians in parking areas.
6. Where openings occur between buildings facing Proctor Boulevard or Pioneer Boulevard, pedestrian ways shall connect the street sidewalk to any internal parking areas. Development shall avoid creating barriers to pedestrian circulation.
7. Parking lots may include public alley accessed garages at the rear property line, except where a setback is required for vision clearance or to conform to other city standards.
8. Raised walkways or painted crossings from the public street sidewalk to the building entrance(s) are required. Crosswalks through parking lots and drive aisles shall be constructed of a material contrasting with the road surface or be painted (e.g., colored concrete inlay in asphalt).

Response: These sections are not applicable.

9. Joint use of access points and interconnections and cross-over easements between parcels shall be required, where the City determines it is practicable and necessary. A development approval may be conditioned to require a joint use access easement and interconnecting driveways or alleys to comply with access spacing and other applicable code requirements.

Response: The applicant owns both the subject property and the property abutting this property to the West. As such, a cross-over easement is not necessary to allow cross-over access between these properties. During the pre-application conference ODOT recommended (August 15, 2018, letter) the applicant record a cross-over access easement between the subject property and the adjacent property to facilitate share access between the two properties. As shown on the submitted plan set, due to the grade difference on the subject property a retaining wall will be constructed along the western line of the subject property leaving only 27 feet at the southern portion of this common line without a wall. The applicant does not believe a cross-over easement makes sense in this location.

10. Connection to Adjacent Properties: The location of any real improvements to the property must provide for a future street and pedestrian connection to adjacent properties where the City determines this is practicable and necessary.
 11. Through lots and corner lots may be permitted with two access points, one onto each abutting street, where necessary to serve a centralized, shared parking facility. Such access points must conform to the above access spacing requirements and parking must be internalized to the property.
 12. Free-standing buildings shall be connected to one another with a seamless pedestrian network that provides access to building entrances and civic spaces.
- Response: The proposed project is intended to be used to display and sell recreational vehicles and does not require vehicle access except for limited-use access on Beers Avenue. The limited-use access is proposed only to facilitate the*

maneuvering of large recreational vehicles on and off the site only as needed. The submitted plan set indicates that this access will be gated.

D. Building Orientation and Entrances

Intent: To maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style. (Figures 17.90.110-A, 17.90.110-B, 17.90.110-C, 17.90.110-D, 17.90.110-E, 17.90.110-F, 17.90.110-G, 17.90.110-H, 17.90.110-I and representative photos in Appendix E)

1. Buildings shall be oriented to a public street or civic space. This standard is met when at least 50 percent of the subject site's street frontage is comprised of building(s) placed within 10 feet of a sidewalk or an approved civic space and not more than 20 percent of the off-street parking on a parcel as required by SDC 17.98, tract or area of land is located between a building's front façade and the adjacent street(s).

Response: Although a building is not proposed with this application, the City has determined that a special variance to this section is required. The special variance criteria for this request are reviewed in Chapter 17.66 below.

H. Lighting

Intent: To promote business vitality, public safety and aesthetics through effective outdoor lighting, consistent with the Sandy Style.

1. Streetscape lighting shall conform to the Downtown Sandy Streetscape Design and the requirements of Chapter 15.30, Dark Sky Ordinance.
2. Exterior lighting must be an integral part of the architectural design and must complement any ornamental street lighting and remain in context with the overall architectural character of the district. On-site light fixtures conforming to the Sandy Style are encouraged.
3. Lighting must be adequate for safety purposes. Walkways, parking lots, and building entrances should be illuminated at 1.5 - 2.0 foot candles.

Response: The applicant proposes installing new lighting on the site for safety and security purposes.

CHAPTER 17.92 LANDSCAPING AND SCREENING GENERAL STANDARDS - ALL ZONES

Response: As noted above, because of site constraints with the proposed stormwater detention facility and the nature of the proposed use, no onsite landscaping is proposed with this application. The proposal includes landscaping within the public right-of-way in the location of the removed approach on Proctor Blvd.

17.92.10 GENERAL PROVISIONS

D. Planter and boundary areas used for required plantings shall have a minimum diameter of 5-ft. (2-1/2 ft. radius, inside dimensions). Where the curb or the edge of these areas

are used as a tire stop for parking, the planter or boundary plantings shall be a minimum width of 7-1/2 ft.

Response: All planter areas have a minimum depth of five feet. The proposed use is to display recreational vehicles for sale. These RV's will be located in the same place for several days rather and the project is not considered a parking lot to park automobiles on the short term basis. With this use the applicant is committed to taking extra care in placing the RV's to ensure landscaping will not be impacted. Given these circumstances, wheel stops are not needed to protect landscaping.

17.92.30 REQUIRED TREE PLANTINGS

Planting of trees is required for all parking lots with 4 or more parking spaces, public street frontages, and along private drives more than 150 feet long. Trees shall be planted outside the street right-of-way except where there is a designated planting strip or City adopted street tree plan.

The City maintains a list of appropriate trees for street tree and parking lot planting situations. Selection of species should be made from the city-approved list. Alternate selections may be approved by the Director following written request. The type of tree used shall determine frequency of trees in planting areas. Trees in parking areas shall be dispersed throughout the lot to provide a canopy for shade and visual relief.

Response: A Landscape Plan is included with the submitted plan set. This plan identifies both onsite landscaping and landscape materials along both the Proctor Blvd. and Beers Ave. frontages. As shown on these plans, 10 percent of the site as required is proposed to be landscaped.

17.92.40 IRRIGATION

Landscaping shall be irrigated, either with a manual or automatic system, to sustain viable plant life.

Response: All landscape areas will be irrigated using either a manual or automatic system. The details of this system will be determined with building plans.

17.92.50 TYPES AND SIZES OF PLANT MATERIALS

A. At least 75% of the required landscaping area shall be planted with a suitable combination of trees, shrubs, or evergreen ground cover except as otherwise authorized by Chapter 17.92.10 F.

D. Deciduous trees shall be balled and burlapped, be a minimum of 7 feet in overall height or 1 1/2 inches in caliper measured 6 inches above the ground, immediately after planting. Bare root trees will be acceptable to plant during their dormant season.

F. Shrubs shall be a minimum of 1 gallon in size or 2 feet in height when measured immediately after planting.

G. Hedges, where required to screen and buffer off-street parking from adjoining properties shall be planted with an evergreen species maintained so as to form a continuous, solid visual screen within 2 years after planting.

H. Vines for screening purposes shall be a minimum of 1 gallon in size or 30 inches in height immediate after planting and may be used in conjunction with fences, screens, or walls to meet physical barrier requirements as specified.

I. Groundcovers shall be fully rooted and shall be well branched or leafed. If used in lieu of turf in whole or in part, ground covers shall be planted in such a manner as to provide complete coverage in one year.

J. Turf areas shall be planted in species normally grown as permanent lawns in western Oregon. Either sod or seed are acceptable. Acceptable varieties include improved perennial ryegrasses and fescues used within the local landscape industry.

K. Landscaped areas may include architectural features or artificial ground covers such as sculptures, benches, masonry or stone walls, fences, rock groupings, bark dust, decorative hard paving and gravel areas, interspersed with planted areas. The exposed area developed with such features shall not exceed 25% of the required landscaped area. Artificial plants are prohibited in any required landscape area.

Response: The submitted Landscape Plan has been designed in accordance with the standards of this section.

17.92.70 LANDSCAPING BETWEEN PUBLIC RIGHT-OF-WAY AND PROPERTY LINES

Except for portions allowed for parking, loading, or traffic maneuvering, a required setback area abutting a public street and open area between the property line and the roadway in the public street shall be landscaped. That portion of the landscaping within the street right-of-way shall not count as part of the lot area percentage to be landscaped.

Response: A five foot landscaped planter is proposed around the perimeter of the majority of the site adjacent to both public rights-of-way.

17.92.80 BUFFER PLANTING - PARKING, LOADING AND MANEUVERING AREAS

Buffer plantings are used to reduce building scale, provide transition between contrasting architectural styles, and generally mitigate incompatible or undesirable views. They are used to soften rather than block viewing. Where required, a mix of plant materials shall be used to achieve the desired buffering effect. Buffering is required in conjunction with issuance of construction permits for parking areas containing 4 or more spaces, loading areas, and vehicle maneuvering areas.

Boundary plantings shall be used to buffer these uses from adjacent properties and the public right-of-way. On-site plantings shall be used between parking bays, as well as between parking bays and vehicle maneuvering areas. A balance of low-lying ground cover and shrubs, and vertical shrubs and trees shall be used to buffer the view of these facilities. Decorative walls and fences may be used in conjunction with plantings, but may not be used by themselves to comply with buffering requirements. Exception: truck parking lots are exempt from parking bay buffer planting requirements.

Response: A five foot landscape planter is proposed onsite along the perimeter of the site. This planter has been widened at the corner of Proctor Blvd. and Beers Ave. to provide additional aesthetic benefit.

CHAPTER 17.98 PARKING, LOADING, AND ACCESS REQUIREMENTS

17.98.20 OFF-STREET PARKING REQUIREMENTS

Response: All business parking is adequately provided on the adjacent property also under the control of the applicant. The proposal does not necessitate constructing any new parking.

17.98.60 DESIGN, SIZE AND ACCESS

All off-street parking facilities, vehicular maneuvering areas, driveways, loading facilities, accessways, and private streets shall conform to the standards set forth in this section.

A. Parking Lot Design. All areas for required parking and maneuvering of vehicles shall have a durable hard surface such as concrete or asphalt.

Response: The proposal is not considered a parking lot and the requirements of this section are not applicable.

B. Size of Space.

1. A standard parking space shall be 9 feet by 18 feet.
2. A compact parking space shall be 8 feet by 16 feet.
3. Handicapped parking spaces shall be 13 feet by 18 feet. Accessible parking shall be provided for all uses in compliance with the requirements of the State of Oregon (ORS 447.233) and the Americans with Disabilities Act.
4. Parallel parking spaces shall be a length of 22 feet.
5. No more than 35 percent of the parking stalls shall be compact spaces.

Response: No vehicle parking is proposed and the requirements of this section are not applicable.

17.98.120 LANDSCAPING AND SCREENING

- A. Screening of all parking areas containing 4 or more spaces and all parking areas in conjunction with an off-street loading facility shall be required in accordance with zoning district requirements and Chapter 17.98. Where not otherwise specified by district requirement, screening along a public right-of-way shall include a minimum 5-ft. depth of buffer plantings adjacent to the right-of-way.
- B. When parking in a commercial or industrial district adjoins a residential zoning district, a sight-obscuring screen that is at least 80% opaque when viewed horizontally from between 2 and 8 feet above the average ground level shall be required. The screening shall be composed of materials that are an adequate size so as to achieve the required degree of screening within 3 years after installation.
- C. Except for a residential development which has landscaped yards, parking facilities shall include landscaping to cover not less than 10% of the area devoted to parking facilities. The landscaping shall be uniformly distributed throughout the parking area and may consist of trees, shrubs, and ground covers.
- D. Parking areas shall be divided into bays of not more than 20 spaces in parking areas with 20 or more spaces. Between, and at the end of each parking bay, there shall be planters that have a minimum width of 5 feet and a minimum length of 17 feet for a single depth bay and 34 feet for a double bay. Each planter shall contain one major structural tree and ground cover. Truck parking and loading areas are exempt from this requirement.
- E. Parking area setbacks shall be landscaped with major trees, shrubs, and ground cover as specified in Chapter 17.92.
- F. Wheel stops, bumper guards, or other methods to protect landscaped areas shall be provided. No vehicle may project over a property line or a public right-of-way. Parking

may project over an internal sidewalk, but a minimum clearance of 5 feet for safe pedestrian circulation is required.

Response: The requirements in these sections are not applicable because no new vehicle parking is proposed or required.

17.98.130 PAVING

- A. Parking areas, driveways, aisles and turnarounds shall be paved with concrete, asphalt or comparable surfacing, constructed to city standards for off-street vehicle areas.
- B. Gravel surfacing shall be permitted only for areas designated for non-motorized trailer or equipment storage, propane or electrically powered vehicles, or storage of tracked vehicles.

Response: As shown on submitted plans the entire site will be paved with asphalt. A proposed typical paving section is shown on the plan set.

17.98.140 DRAINAGE

Parking areas, aisles and turnarounds shall have adequate provisions made for the on-site collection of drainage waters to eliminate sheet flow of such waters onto sidewalks, public rights-of-way and abutting private property.

Response: A preliminary stormwater management plan is provided as part of the application package. This plan has been designed in accordance with the City of Sandy Stormwater Management requirements. As shown on the submitted Utility Plan the proposed stormwater system includes a water quality catch basin and underground stormwater detention chambers. The system is designed to connect to the existing stormwater pipe located near the corner of Beers Avenue and Pleasant Street.

17.98.150 LIGHTING

Artificial lighting shall be provided in all required off-street parking areas. Lighting shall be directed into the site and shall be arranged to not produce direct glare on adjacent properties. Light elements shall be shielded and shall not be visible from abutting residential properties. Lighting shall be provided in all bicycle parking areas so that all facilities are thoroughly illuminated and visible from adjacent sidewalks or vehicle parking lots during all hours of use.

Response: As noted above, the applicant proposes installing new lighting to illuminate the site. All site lighting will be designed and installed in accordance with Chapter 15.30, Dark Sky Ordinance standards.

CHAPTER 15.30 - DARK SKY ORDINANCE

15.30.000 Purpose.

The purpose of the Sandy Dark Sky Ordinance is to regulate outdoor lighting in order to reduce or prevent light pollution. This means to the extent reasonably possible the reduction or prevention of glare and light trespass, the conservation of energy, and promotion of safety and security. (Ord. 2002-11)

Response: All new light fixtures will be designed and installed in accordance with these regulations. A photometric analysis and lighting fixture cut-sheets are currently being designed for the site by a qualified lighting contractor. This information is not ready to be included with the application package. This information will be submitted as soon as it is available. If this information is not obtained prior to issuance of the completeness

check letter, the applicant requests the application be deemed complete and the approval be conditioned to submit information in compliance with this Chapter as part of the building permit submittal process.

CHAPTER 17.66 - ADJUSTMENTS & VARIANCES

17.66.80 TYPE III SPECIAL VARIANCES

The Planning Commission may grant a special variance waiving a specified provision for under the Type III procedure if it finds that the provision is unreasonable and unwarranted due to the specific nature of the proposed development. In submitting an application for a Type III Special Variance, the proposed development explanation shall provide facts and evidence sufficient to enable the Planning Commission to make findings in compliance with the criteria set forth in this section while avoiding conflict with the Comprehensive Plan.

Response: *As advised by the City, the applicant has submitted a request for a Special Variance to Section 17.90.110(D)(1). This section states,*

“Buildings shall be oriented to a public street or civic space. This standard is met when at least 50 percent of the subject site’s street frontage is comprised of building(s) placed within 10 feet of a sidewalk or an approved civic space and not more than 20 percent of the off-street parking on a parcel as required by SDC 17.98, tract or area of land is located between a building’s front façade and the adjacent street(s).”

The applicant’s proposal is to develop the subject property for the display and sales of recreational vehicles. This use is an extension of the existing use currently operating on the four properties directly west of the subject property. Because a building already exists for the business, no building is needed or proposed with this application. The City has determined that a Special Variance is needed to this section.

One of the following sets of criteria shall be applied as appropriate.

A. The unique nature of the proposed development is such that:

1. The intent and purpose of the regulations and of the provisions to be waived will not be violated; and

Response: *As noted above, use of the subject property is an extension of the business currently operating on the four tax lots located to the west of the subject property. As such, no building is proposed or needed in order to expand.*

The intent of Section 17.90.110(D) - Building Orientation states, “To maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style.”

Because a building is not needed or proposed, compliance with Section 17.90.110(D)(1) is not possible. The applicant’s proposal to attractively landscape

the site and install perimeter fencing will define the streetscape and enhance the visual appearance of the site. In addition, the proposed use of the property for storage and display of recreational vehicles will provide additional variety, visual interest, and an appearance of economic vitality to this area. The proposal complies with criteria.

2. Authorization of the special variance will not be materially detrimental to the public welfare and will not be injurious to other property in the area when compared with the effects of development otherwise permitted.

Response: The proposed variance to this standard will have no effect on the public welfare or other properties in the area. The location of the site is such that it separated from other properties that are not under the owner's control. The proposal complies with this criteria.

- B. The variance approved is the minimum variance needed to permit practical compliance with a requirement of another law or regulation.

Response: Only a single variance is necessary to allow the proposal to comply with code standards. The requested special variance is the minimum variance needed to allow compliance with this standard. The proposal complies with this criteria.

- C. When restoration or replacement of a nonconforming development is necessary due to damage by fire, flood, or other casual or natural disaster, the restoration or replacement will decrease the degree of the previous noncompliance to the greatest extent possible.

Response: The proposal does not involve nonconforming development.

V. Conclusion

The applicant requests a minor conditional use permit and design review approval to improve the subject property for the display and sales of recreational vehicles. The subject property is an expansion of the Funtime RV business located on the four tax lots directly west of the subject property. The property is zoned C-1, Central Business District and the proposed is a permitted use in this zone. The applicant intends closing the existing curb cut on Proctor Blvd. and installing a planter strip in this location. The applicant will also complete frontage improvements on Beers Ave. and has requested approval for a limited-use access on this street. Onsite improvements include paving and the installation of stormwater detention and water quality facilities, landscaping, and lighting. The applicant has also applied for a Special Variance to Section 17.90.110(D)(1) regarding building orientation even though a building is not needed or proposed with this application. Based on the information provided in this submittal, the application complies with or exceeds relevant code standards and can be approved or conditions imposed to be bring the proposal into compliance with this code.

EXHIBIT D

FUNTIME RV, INC. SITE DEVELOPMENT NE BEERS AVE AND PROCTOR BLVD, SANDY, OREGON

GENERAL NOTES:

- ALL WORK AND MATERIALS SHALL CONFORM TO THESE PLANS AND THE APPLICABLE PROVISIONS OF THE CITY OF SANDY PUBLIC WORKS STANDARDS.
- IN ORDER TO PROTECT UNDERGROUND FACILITIES, EXCAVATORS PERFORMING THE WORK SET FORTH ON THESE PLANS MUST COMPLY WITH THE PROVISIONS OF ORS 757.541 TO 757.571 (REQUIRES CONTRACTOR TO NOTIFY UTILITIES AT LEAST 48 HOURS, BUT NOT MORE THAN 10 BUSINESS DAYS, PRIOR TO ANY EXCAVATION).
- THE LOCATION OF EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND SHOWN FOR INFORMATION PURPOSES ONLY. THE CONTRACTOR SHALL HAVE ALL UTILITIES LOCATED PRIOR TO COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION. ADDITIONAL UNDERGROUND UTILITIES MAY EXIST.
- VERTICAL DATUM: ELEVATION ESTABLISHED USING GPS, NAVD 88 DATUM AND HOLDING OPUS SOLUTION COMPUTED USING GEOD12B.
- TRENCHES WITHIN THE RIGHTS OF WAY SHALL BE BACKFILLED WITH AN APPROVED GRANULAR MATERIAL CONFORMING TO APWA CLASS B SPECIFICATIONS.
- TRENCHES OUTSIDE OF RIGHTS OF WAY MAY BE BACKFILLED IN ACCORDANCE WITH NATIVE MATERIAL AND COMPACTION SPECIFICATIONS FOR APWA CLASS A BACKFILL.
- VEGETATION AND TOPSOIL ARE TO BE STRIPPED TO MINERAL EARTH (AND INSPECTED BY THE PROJECT ENGINEER OR GEOTECHNICAL ENGINEER) PRIOR TO PLACEMENT OF FILL OR BASE MATERIALS.
- IN ADDITION TO ANY REQUIRED COMPACTION TESTING, THE CITY MAY REQUIRE A PROOF ROLL WITH A FULLY LOADED 10-YARD DUMP TRUCK TO CHECK SUBGRADE COMPACTNESS PRIOR TO PLACEMENT OF ROCK SUBBASE AND AGAIN AT THE COMPLETION OF THE PLACEMENT OF THE BASE ROCK PRIOR TO PAVING THE FIRST LIFT OF ASPHALT.
- ASPHALTIC CONCRETE MIX IS TO BE BATCHED FROM A MIX FORMULA APPROVED BY OSDH FOR MATERIAL USED. PAVING CONTRACTOR SHALL PROVIDE A CERTIFICATE OF COMPLIANCE FROM ASPHALT PAVEMENT PLANT.
- SUBSEQUENT SETTLEMENT OR CRACKING OF FINISHED SURFACE WITHIN THE WARRANTY PERIOD SHALL BE CONSIDERED TO BE A FAILURE OF THE SUBGRADE AND REPAIRED AT NO COST TO THE CITY AND IN A MANNER ACCEPTABLE TO THE CITY. A PERFORMANCE BOND IS REQUIRED TO GUARANTEE REPAIRS UNDER THE WARRANTY PERIOD.
- THE CONTRACTOR SHALL CONTROL TRAFFIC THROUGH THE PROJECT SITE IN CONFORMANCE WITH THE LATEST EDITION OF "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", "OREGON SUPPLEMENTS". THE CONTRACTOR SHALL AT ALL TIMES MAINTAIN LOCAL ACCESS FOR HOMEOWNERS ALONG THE PROJECT SITE.
- THE CONTRACTOR AND/OR SUB-CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) SET OF APPROVED CONSTRUCTION PLANS ON THE JOB SITE AT ALL TIMES DURING THE CONSTRUCTION PHASES.
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, TOPSOIL AND OTHER MATERIAL ENCOUNTERED DURING THE CONSTRUCTION OF THE ROADWAY AND WHERE INDICATED ON THE PLANS. MATERIAL SHALL BE DISPOSED OF IN ACCORDANCE WITH LOCAL, REGIONAL AND STATE REGULATIONS AT FACILITIES AUTHORIZED TO ACCEPT SUCH MATERIAL.
- CONTRACTOR SHALL CAREFULLY MAINTAIN BENCHMARKS, PROPERTY CORNERS, MONUMENTS AND OTHER REFERENCE POINTS. IF SUCH POINTS ARE DISTURBED OR DESTROYED BY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND PAY FOR THEIR REPLACEMENT BY EMPLOYING A PROFESSIONAL LAND SURVEYOR TO RESET PROPERTY CORNERS AND OTHER SUCH MONUMENTS.
- EXCESS EXCAVATED MATERIAL SHALL BE HAULED AND DISPOSED OF AT SITES PROVIDED BY THE OWNER AND APPROVED PURSUANT TO AN APPROPRIATE GRADING PERMIT. FILL SITES SHALL BE LEVELED AND GRADED TO DRAIN. THE CONTRACTOR SHALL CORRECT ANY FILL RELATED CONDITIONS.
- FINAL CLEANUP - PRIOR TO FINAL ACCEPTANCE AND PAYMENT, THE CONTRACTOR SHALL CLEAN THE WORK SITE AND ADJACENT AREAS OF ANY DEBRIS, DISCARDED ASPHALTIC CONCRETE MATERIAL OR OTHER ITEMS DEPOSITED BY THE CONTRACTORS PERSONNEL DURING THE PERFORMANCE OF THIS CONTRACT.
- A PERMIT IS REQUIRED FOR ANY WORK IN THE PUBLIC RIGHT-OF-WAY, CONTACT THE CITY OF SANDY PUBLIC WORKS DEPARTMENT OR VISIT www.ci.sandy.or.us FOR PERMIT APPLICATION FORMS.
- ALL WORK MUST COMPLY WITH CITY OF SANDY GRADING AND EROSION CONTROL PERMIT #xxxx GR/EC ISSUED XX/XX/XX.
- ALL WORK MUST COMPLY WITH THE CITY OF SANDY'S EROSION CONTROL STANDARDS. CONTRACTOR TO SCHEDULE INSPECTIONS 48 NORMAL BUSINESS HOURS IN ADVANCE.
- IF, AT ANYTIME DURING THE ON-SITE CONSTRUCTION, THE CONTRACTOR OBSERVES AN ENVIRONMENTAL IMPACT ON-SITE SUCH AS OIL TANKS, CONTAMINATION RESIDUE OR HAZARDOUS MATERIALS, SPILLAGE, ETC., IT MUST BE REPORTED, INSPECTED, AND TREATED IN CONFORMANCE WITH THE APPROPRIATE AGENCY'S REQUIREMENTS.

UTILITY NOTES:

- THE CONTRACTOR IS REQUIRED TO OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF SANDY AND OREGON DEPARTMENT OF TRANSPORTATION PRIOR TO BEGINNING CONSTRUCTION.

NOTIFICATION:

GENERAL - THE CONTRACTOR SHALL, AS A MINIMUM, COORDINATE THE PROPOSED CONSTRUCTION ACTIVITIES WITH THE OWNER AND LOCAL PUBLIC AGENCIES, UTILITIES AND COMPANIES DURING CONSTRUCTION TO AVOID DAMAGE AND TO PREVENT THE INTERRUPTION OF SERVICES AND UTILITIES TO RESIDENTS AND BUSINESSES:

OREGON UTILITY NOTIFICATION CENTER
1-503-246-6899

CLACKAMAS COUNTY
(503) 353-4400

CITY OF SANDY ENGINEERING DEPT. (503) 668-5533
BUILDING DIVISION INSPECTIONS (503) 668-6941

PORTLAND GENERAL ELECTRIC
(503) 226-8111

VERIZON NORTHWEST (TELEPHONE)
1-800-483-4100

NORTHWEST NATURAL GAS
(503) 226-4211

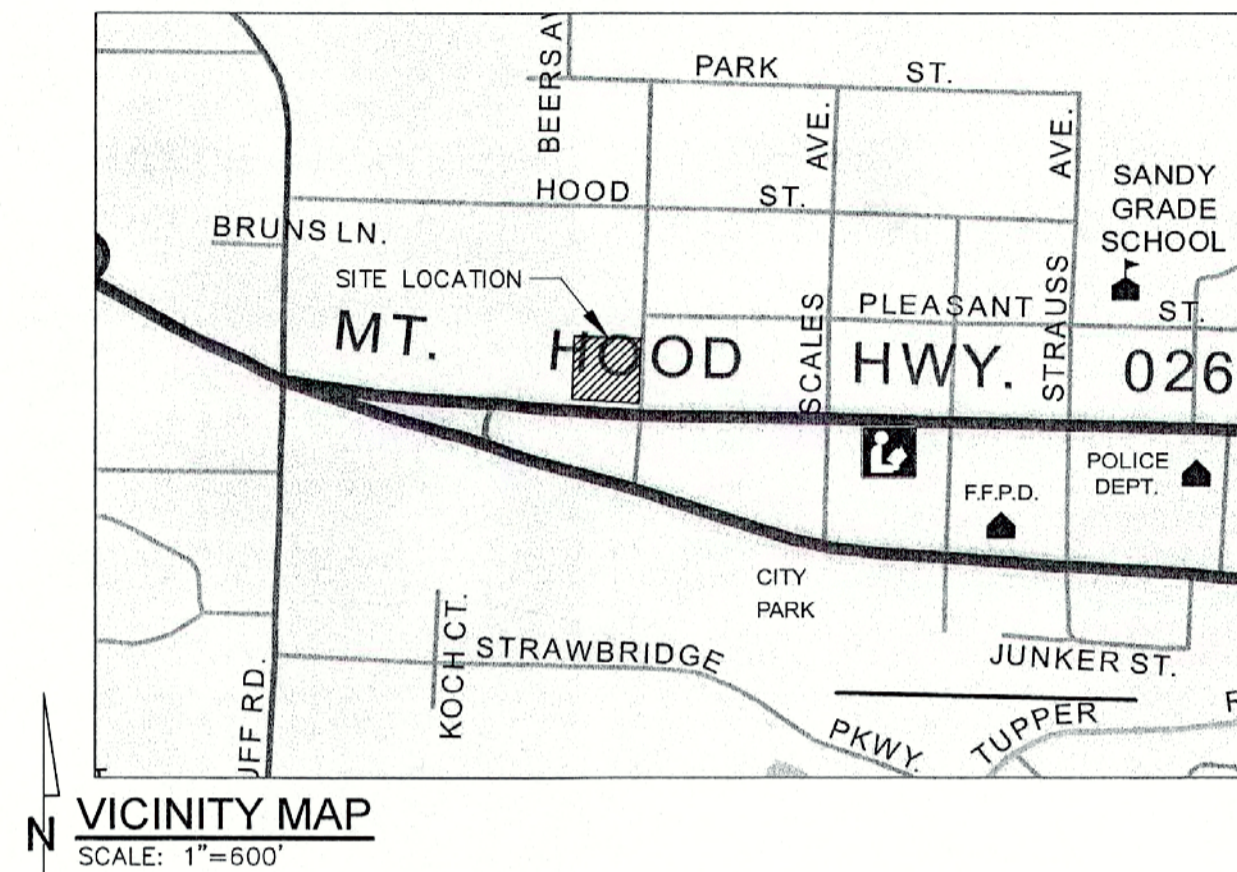
CHARTER COMMUNICATIONS (CABLE TV)
1-866-731-5420

STREET AND STORM DRAINAGE NOTES:

- STREET AND STORM DRAIN IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF SANDY STANDARDS AND REQUIREMENTS.
- ALL TRENCH EXCAVATION SHALL CONFORM TO STANDARD STORM SEWER SPECIFICATIONS AND SHALL BE UNCLASSIFIED.
- PIPE BEDDING AND PIPE ZONE SHALL CONFORM TO THE EXCAVATION AND BACKFILL DETAILS, AND SHALL BE 3/4"-0" CRUSHED ROCK.
- THE CITY REQUIRES COMPACTION WITHIN THE RIGHT-OF-WAY TO BE 95% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO T-180. CONTRACTOR TO DETERMINE TYPE OF EQUIPMENT AND METHOD USED TO ACHIEVE REQUIRED COMPACTION.
- TRENCH BACKFILL OUTSIDE OF RIGHTS OF WAY OR PAVED AREAS MAY BE EXCAVATED TRENCH MATERIAL. TRENCH BACKFILL IN PAVED AREAS SHALL BE AN APPROVED GRANULAR MATERIAL.
- MATERIAL IN SOFT SPOTS WITHIN THE ROADWAY SHALL BE REMOVED TO THE DEPTH REQUIRED TO PROVIDE A FIRM FOUNDATION AND SHALL BE REPLACED WITH 1-1/2"-0" CRUSHED ROCK. THE ENTIRE SUBGRADE SHALL BE THOROUGHLY COMPACTED TO 95% AASHTO T-180.
- CONTRACTOR SHALL NOTIFY THE ENGINEER AND CITY OF SANDY WHEN SUBGRADE IS COMPLETE AND 24 HOURS PRIOR TO PLACEMENT OF ROCK BASE MATERIAL AND 24 HOURS PRIOR TO FINAL PAVING FOR AN INSPECTION OF THE WORK. FAILURE TO DO SO WILL MAKE ANY SUBGRADE FAILURE PROBLEMS THE RESPONSIBILITY OF THE CONTRACTOR. A PROOF ROLL WITH A FULLY LOADED 10-YARD DUMP TRUCK MAY BE REQUIRED TO CHECK SUBGRADE COMPACTION PRIOR TO PLACEMENT OF ROCK SUBBASE AND AGAIN AT THE COMPLETION OF THE PLACEMENT OF THE BASE ROCK PRIOR TO PAVING THE FIRST LIFT OF ASPHALT.
- ALL SAWCUT JOINTS SHALL BE STRAIGHT, TACKED AND SAND SEALED UPON PAVING.
- THE CITY REQUIRES A SUCCESSFUL MANDREL PULL ON ONE SECTION OF EACH DIAMETER OF STORM PIPE USED.
- ASPHALT COMPACTION SHALL BE PERFORMED USING NUCLEAR GAUGE. THE RICE DENSITY TESTS SHALL BE 91% FOR THE BASE LIFT AND 92% FOR THE TOP LIFT IN ACCORDANCE WITH ODOT TM305 OR AASHTO T-209. SUBMIT TESTING REPORTS TO THE CITY.

GRADING NOTES:

- FILLS SHALL BE COMPACTED TO 95 PERCENT OF MAXIMUM DENSITY IN THE BUILDING ENVELOPE AND 92 PERCENT OF MAXIMUM DENSITY ON THE REMAINDER OF THE LOT AS DETERMINED BY ASTM TEST SD 1557-91, METHOD A, OR AN EQUIVALENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING THE NECESSARY ARRANGEMENTS FOR SUCH TESTING AND FOR SUPPLYING THE RESULTS TO THE CITY OF SANDY.
- ALL CUTS SHALL BE MADE CONSISTENT WITH THE DETAILS NOTED IN THE PLANS. NO CUT SHALL EXCEED A GRADE OF 2 HORIZONTAL TO 1 VERTICAL UNLESS APPROVED BEFOREHAND BY THE ENGINEER AND THE CITY OF SANDY.
- APPROPRIATE BENCHING OF FILLS IS REQUIRED FOR FILLS OVER 5 FEET IN HEIGHT ON SLOPES IN EXCESS OF 5 HORIZONTAL TO 1 VERTICAL. BENCHING MUST BE DONE AS PER THE APPROVED PLANS. THE CITY OF SANDY SHALL INSPECT BENCHES PRIOR TO FILL PLACEMENT.
- CUT AND FILL SLOPES SHALL BE PROTECTED FROM EROSION. SUCH CONTROL MAY CONSIST OF APPROPRIATE REVEGETATION OR OTHER ACCEPTABLE MEANS AND METHODS. EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO EARTHWORK OR SITE STRIPPING.
- THE CONTRACTOR SHALL COORDINATE WITH CITY PERSONNEL BY CALLING (503) 668-6941 OR (503) 668-5533 ANYTIME FOR REQUIRED INSPECTIONS AT THE FOLLOWING STAGES OF CONSTRUCTION:
- THE CONTRACTOR SHALL NOT COMPACT SOIL/SUB-GRADE UNDER STORMTECH CHAMBERS TO PRESERVE INFILTRATION CAPACITY.



SHEET INDEX:

- C1 - COVER SHEET
- C2 - EXISTING CONDITIONS & DEMO PLAN
- C3 - SITE, GRADING, & LANDSCAPING PLAN
- C4 - CONCEPT RV DISPLAY LAYOUT
- C5 - BEERS AVENUE PLAN & PROFILE
- C6 - UTILITY PLAN
- C7 - DETAILS SHEET - CITY OF SANDY
- C8 - DETAILS SHEET - CITY OF SANDY
- C9 - DETAILS SHEET - ODOT
- C10 - DETAILS SHEET - DETENTION FACILITY



EXPIRES: 06/30/19
SIGNATURE DATE: _____
PRELIMINARY

DATE:	NO.	REVISION

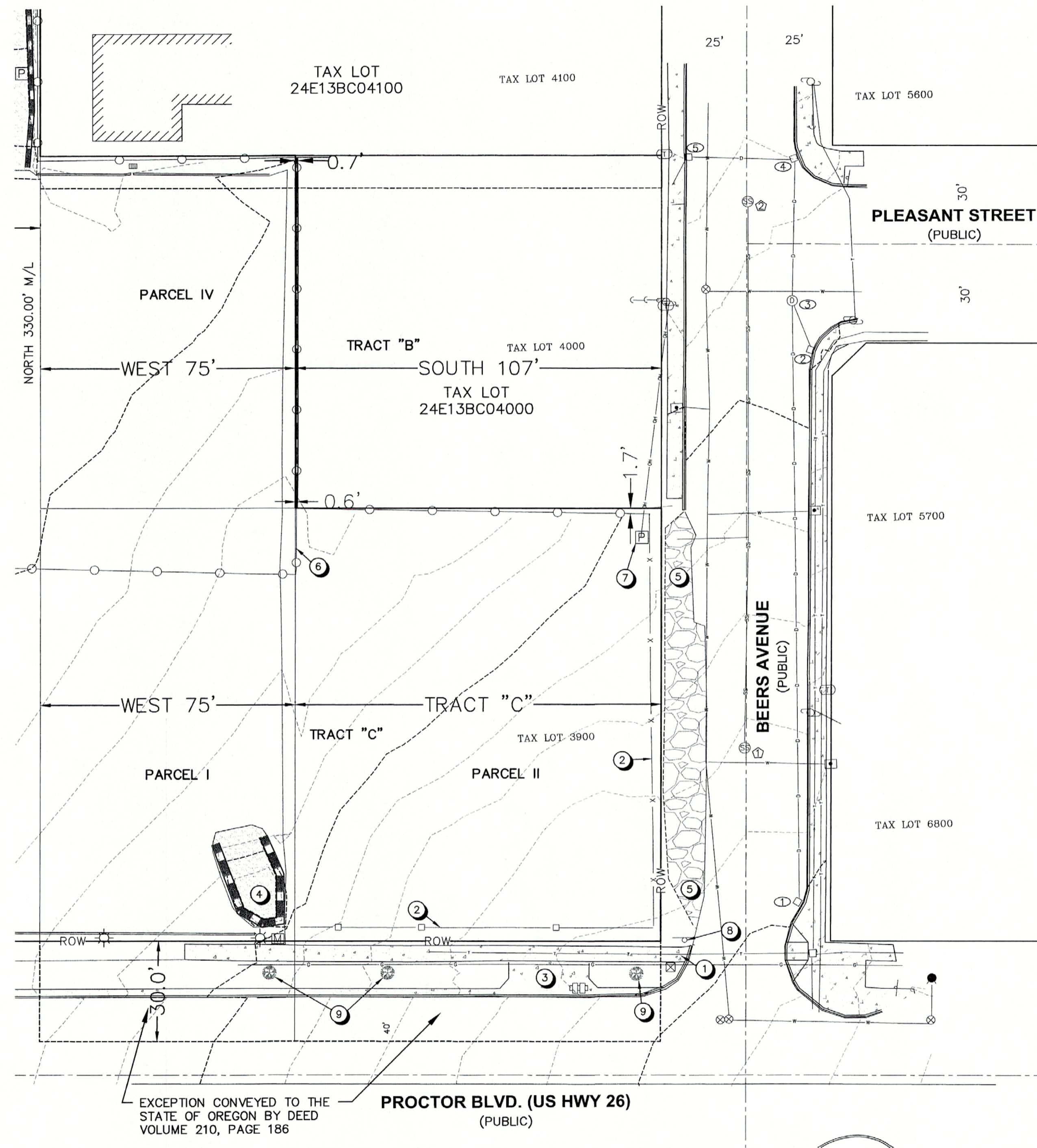
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Firwood Design Group, LLC
SURVEYING • ENGINEERING • PLANNING

359 E. HISTORIC COLUMBIA RIVER HWY
TROUTDALE, OREGON 97060
BUS: (503) 668-3737 • FAX: (503) 668-3788

FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

COVER SHEET
SITE DEVELOPMENT
SANDY, OREGON 97055

C1
10



- ⊕ SANITARY MANHOLE
- ⊙ STORM MANHOLE
- ⊕ CLEANOUT
- CATCH BASIN
- FIRE HYDRANT
- WATER VALVE
- ⊠ WATER METER
- ⊕ COMMUNICATION RISER
- ⊕ UTILITY POLE
- ▲ SIGN
- MAILBOX
- ⊕ GUY ANCHOR
- ⊕ POWER POLE
- ⊕ DECIDUOUS TREE

- LEGEND**
- S— SEWER LINE
 - S— STORM LINE
 - W— WATERLINE
 - G— GAS LINE
 - U— UNDERGROUND COMMUNICATION
 - X— WOOD FENCE
 - X— WIRE FENCE
 - - - - - 935' - - - - - MAJOR CONTOUR LINE
 - - - - - MINOR CONTOUR LINE
 - ○ ○ ○ CHAINLINK FENCE
 - — — — — PROPERTY LINE
 - — — — — EDGE OF PAVEMENT
 - — — — — ROW
 - ▨ GRAVEL SURFACE
 - ▨ CONCRETE SURFACE
 - ▨ BUILDING

- KEY NOTES**
- 1 EXISTING RAMP AND CURB TO BE REMOVED AS NECESSARY FOR ADA RAMP CONSTRUCTION AND INTERSECTION IMPROVEMENTS
 - 2 EXISTING FENCE TO BE REMOVED
 - 3 EXISTING DRIVE ACCESS TO BE REMOVED FOR LANDSCAPING, ODOT MISC. PERMIT REQUIRED
 - 4 EXISTING CONCRETE VEHICLE RAMP TO BE REMOVED FOR VEHICLE ACCESS
 - 5 EXISTING PAVEMENT AND GRAVEL TO BE REMOVED FOR FRONTAGE IMPROVEMENTS
 - 6 EXIST CHAINLINK FENCE TO BE RELOCATED/REMOVED
 - 7 EXIST POWER POLE TO BE REMOVED
 - 8 RELOCATE STOP SIGN AS DIRECTED BY CITY
 - 9 PROTECT EXISTING TREES

- ⊕ RIM = 939.60'
IE 12" OUT NW. = 936.31'
- ⊕ RIM = 938.22'
IE 8" OUT N. = 932.88'
- ⊕ RIM = 934.37'
IE 3" IN N. = 933.79'
IE 6" IN SE. = 941.09'
IE 3" IN SE. = 933.85'
IE 10" OUT W. = 931.91'
- ⊕ RIM = 933.61'
IE 8" IN E. = 928.41'
IE 8" IN N. = 928.37'
IE 8" IN S. = 928.41'
IE 8" OUT W. = 928.29'
- ⊕ RIM = 934.45'
IE 10" IN SE. = 931.35'
IE 6" IN E. = 931.31'
IE 12" IN S. = 931.90'
IE 12" OUT N. = 930.80'
- ⊕ RIM = 933.00'
IE 12" IN S. = 930.32'
IE 12" OUT W. = 930.12'
- ⊕ RIM = 932.35'
IE 6" IN W. = 930.51'
IE 12" IN E. = 929.97'
IE 12" OUT N. = 929.52'

EXCEPTION CONVEYED TO THE STATE OF OREGON BY DEED VOLUME 210, PAGE 186

PROCTOR BLVD. (US HWY 26) (PUBLIC)

DATE:	NO.	REVISION

DRAWN:	DESIGNED:	CHECKED:
SCALE: AS SHOWN	DATE: FEB 5, 2019	
PROJECT NO. E17-060		

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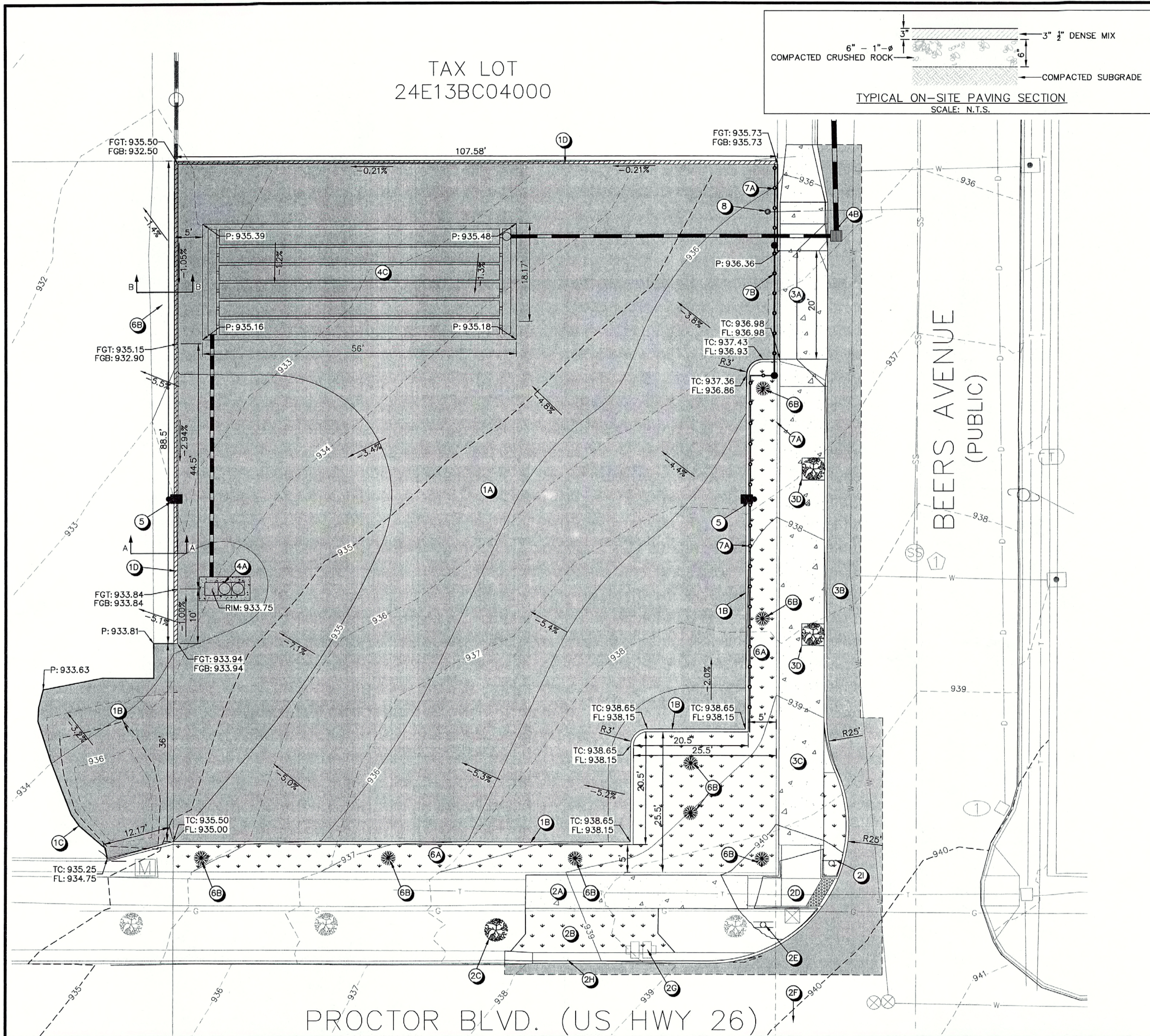
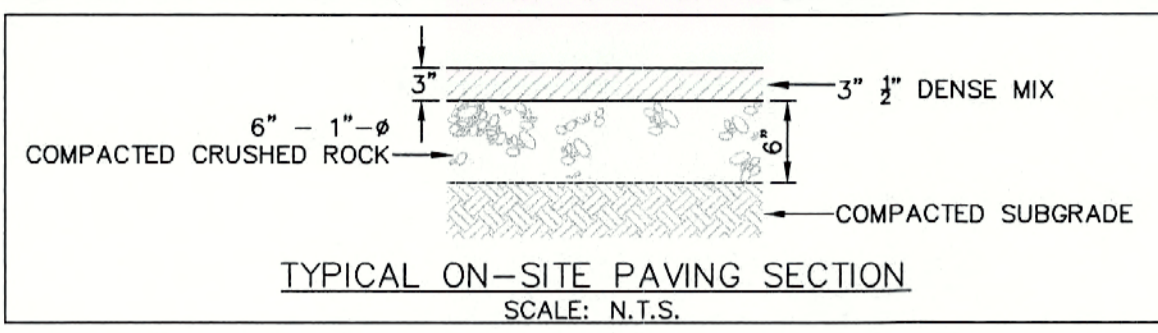
EXISTING CONDITIONS & DEMO PLAN
SITE DEVELOPMENT
SANDY, OREGON 97055

C2
10



EXPIRES: 06/30/19
SIGNATURE DATE: _____
PRELIMINARY

TAX LOT
24E13BC04000



KEY NOTES

- 1A) CONSTRUCT NEW ASPHALT PAVING SURFACE PER SECTION THIS SHEET
- 1B) CONSTRUCT STANDARD TYPE "C" CURB, SEE SHEET C7 FOR DETAIL
- 1C) MATCH LIMITS OF EXIST AC ON ADJACENT PROPERTY FOR REMOVAL OF CONCRETE VEHICLE RAMP
- 1D) CONSTRUCT 196 L.F. RETAINING WALL FROM EXISTING GROUND ELEVATION TO FINISHED GRADE ELEVATION
- 2A) CONSTRUCT NEW 6" CONCRETE SIDEWALK. MATCH EXIST CONCRETE SIDEWALK AT LIMITS OF REMOVAL OF EXISTING DRIVECUT. SEE DETAIL ON SHEET C9
- 2B) INSTALL PLANTER IN-LINE WITH EXISTING PLANTER, ADJACENT TO SIDEWALK
- 2C) PLANT ONE CHANTICLEER PEAR TREE, 7 FEET TALL AND/OR 1 1/2" CALIPER
- 2D) CONSTRUCT SINGLE ADA ACCESS FROM SIDEWALKS ON NORTH SIDE OF PROCTOR BLVD AND WEST SIDE OF BEERS AVE TO CROSSWALK ACROSS BEERS AVENUE. SEE DETAIL VIEW ON THIS SHEET
- 2E) INSTALL ODOT NO CROSSWALK SIGN, SEE SHEET C9 FOR DETAIL
- 2F) INSTALL ODOT NO CROSSWALK SIGN ON SOUTH SIDE OF PROCTOR BLVD., SEE SHEET C9 FOR DETAIL
- 2G) ADJUST EXISTING MAILBOX TO PROPOSED FINISH GRADE IN SAME LOCATION AFTER CONSTRUCTION OF NEW CURB
- 2H) INSTALL STANDARD CURB WITH CONCRETE APRON TO MATCH EXISTING, SEE SHEET C9 FOR DETAIL
- 2I) RE-INSTALL EXISTING STOP SIGN
- 3A) CONSTRUCT COMMERCIAL DRIVEWAY APPROACH. SEE DETAIL SHEET C7
- 3B) WIDEN EXISTING AC SURFACE OF BEERS AVE. TO MEET EDGE OF CURB. SEE TYPICAL SECTION SHEET C5
- 3C) CONSTRUCT NEW 8" CONCRETE SIDEWALK AND STANDARD 6" CURB. MATCH EXIST CONCRETE SIDEWALK AND CURB PAST PROPERTY LINE ON WEST SIDE OF BEERS AVE. SEE TYPICAL SECTION ON SHEET C5
- 3D) INSTALL 48" X 48" TREE WELL WITH RED BARRON CRABAPPLE TREE, 7 FEET TALL AND/OR 1 1/2" CALIPER
- 4A) INSTALL CONCRETE CATCHBASIN STORMFILTER. FOR DESIGN INFORMATION SEE UTILITIES PLAN ON SHEET C6 AND CATCHBASIN DETAIL ON SHEET C10
- 4B) INSTALL NEW STORMWATER CATCHBASIN WITH CONNECTIONS FROM PROPOSED STORMWATER DETENTION CHAMBERS AND TO DOWNSTREAM MANHOLE. SEE UTILITY PLAN ON SHEET C6
- 4C) INSTALL 42 STORMTECH SC-310 CHAMBERS (300 L.F.) IN A SERIES OF 50'-FT LENGTHS. SEE TECHNICAL SPECIFICATIONS AND INSTALLATION DETAILS ON SHEET C10
- 5) INSTALL AREA LIGHT ON POST
- 6A) 5' LANDSCAPING BUFFER - PLANT NATIVE GRASS MIX AT 1 LB PER 1000 SQ. FT.
- 6B) PLANT RED TWIG DOGWOOD, 5 GAL
- 7A) INSTALL 3' TALL BLACK CHAIN LINK FENCE
- 7B) INSTALL 3' TALL BLACK CHAIN LINK SLIDING GATE TO REMAIN CLOSED AND LOCKED EXCEPT FOR INGRESS & EGRESS OF PRODUCT TO BE DISPLAYED IN THE PROPOSED DISPLAY AREA
- 8) RELOCATE SANITARY CLEANOUT TO PROPERTY LINE

LEGEND

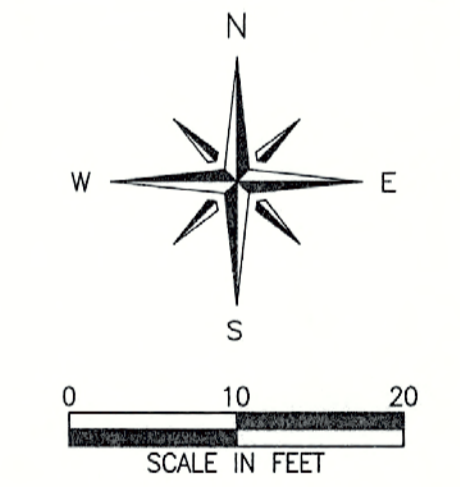
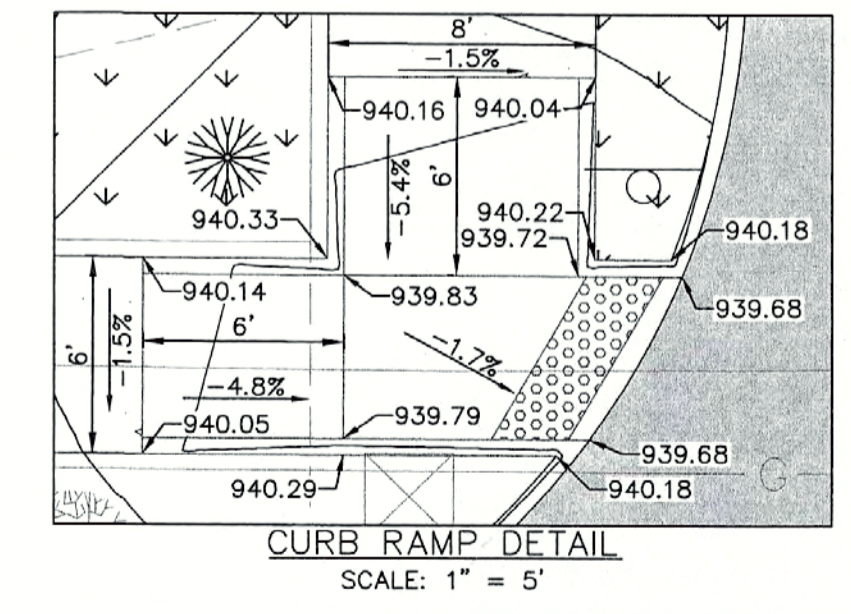
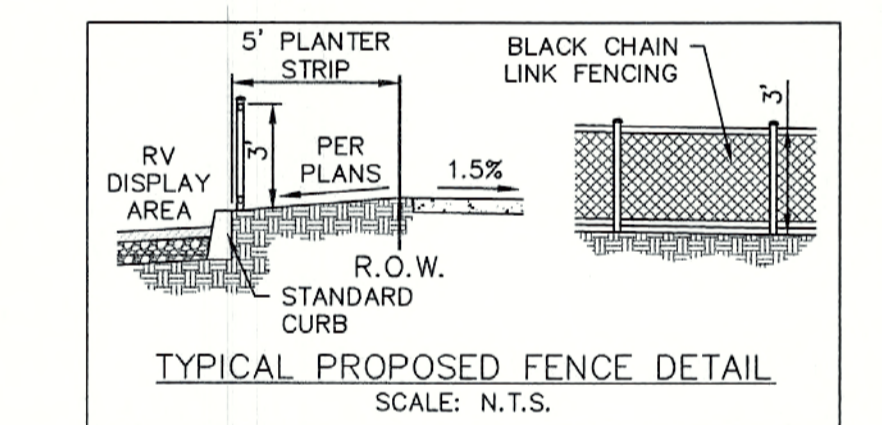
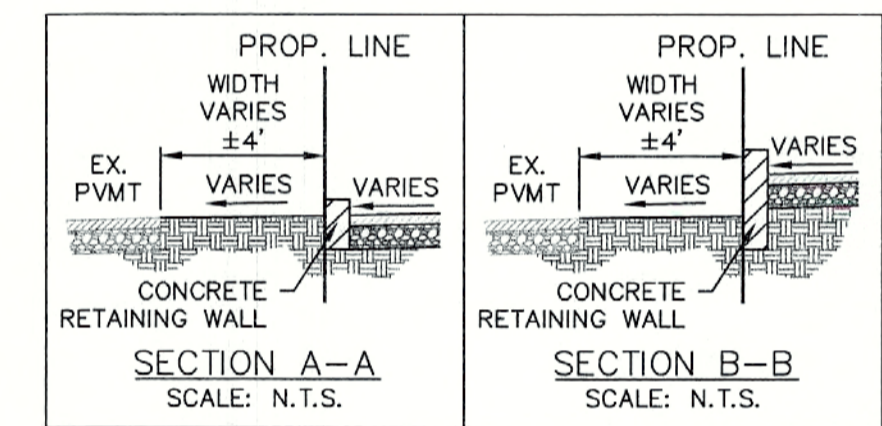
- PLANTER
- PROPOSED CONCRETE SIDEWALK
- PROPOSED AC
- SAW CUT LINE (TYP)

GRADING QUANTITIES (ESTIMATED):

CUT = 140 CY
 FILL = 400 CY
 PAVEMENT SECTION VOLUME = 445 CY
 DETENTION SECTION VOLUME = 105 CY
 NET = 290 CY EXPORT

LANDSCAPING AREAS:

SITE AREA: 13,985 SF
 REQUIRED LANDSCAPED AREA
 (10% OF SITE AREA): 1,399 SF
 6A) PROPOSED LANDSCAPED AREA: 1,399 SF



REGISTERED PROFESSIONAL
 ENGINEER
FOR REVIEW
 OREGON
 APR 14 2009
 MELLI A. GROVER
 EXPIRES: 06/30/19
 SIGNATURE DATE: _____
PRELIMINARY

DATE:	NO.	REVISION

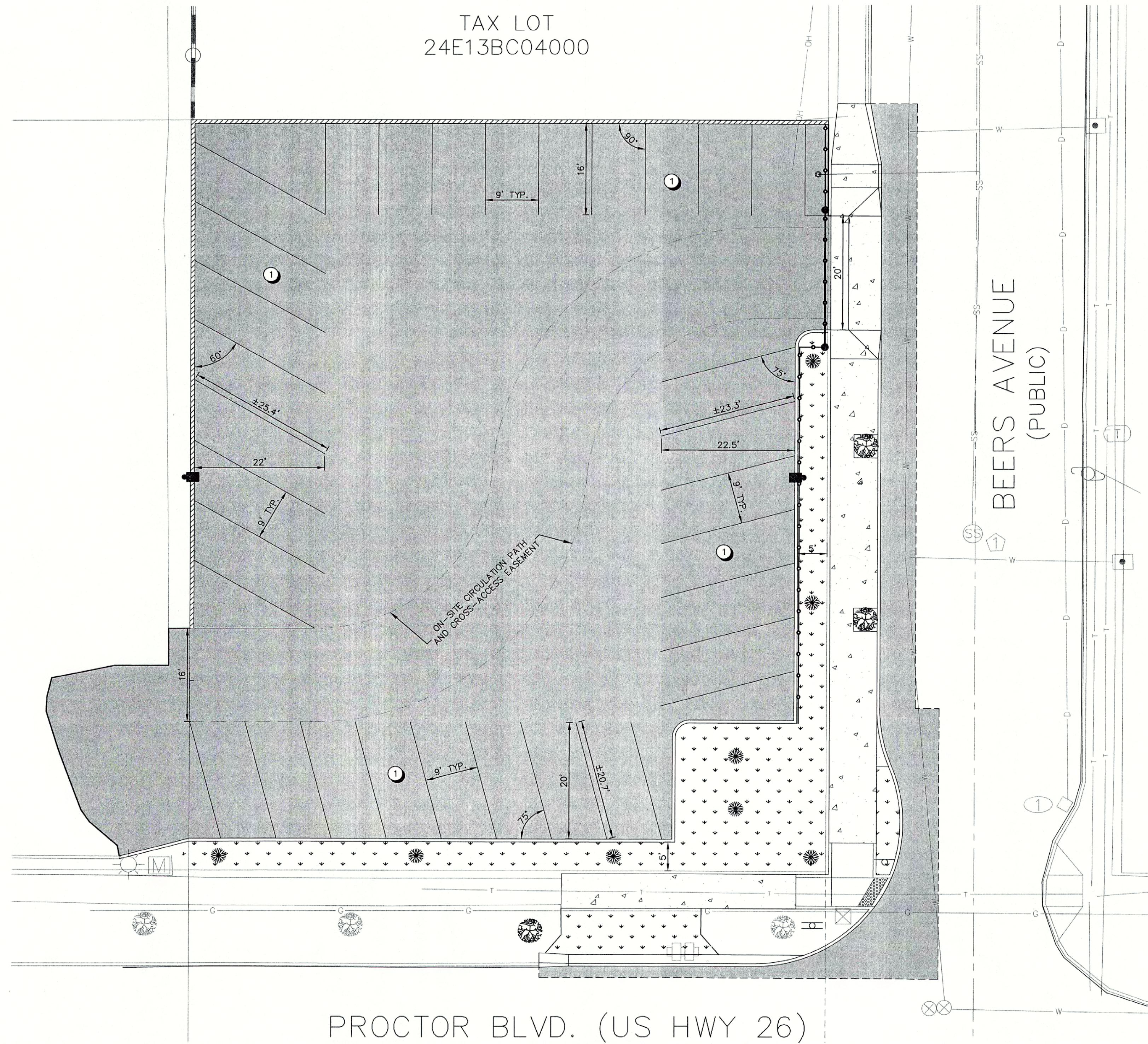
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 TROUTDALE, OREGON 97060
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FUNTIME RV, INC
 NE BEERS AVE & PROCTOR BLVD
 SANDY, OREGON 97055

SITE, GRADING, & LANDSCAPING PLAN
 SITE DEVELOPMENT
 SANDY, OREGON 97055

C3
 10

TAX LOT
24E13BC04000

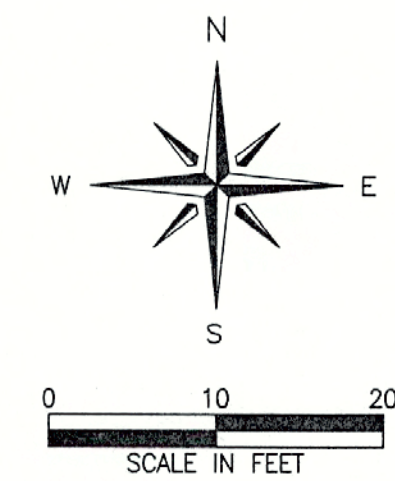


KEY NOTES

- 1. CONCEPT RV DISPLAY SPACE (TYPICAL)
DISPLAY SPACES WILL NOT BE STRIPED

RV DISPLAY NOTES

1. THE LAYOUT SHOWN ON THIS PLAN IS CONCEPTUAL IN NATURE AND NOT INTENDED TO LIMIT THE SITE OWNER'S DISCRETION IN RECREATIONAL VEHICLE DISPLAY CONFIGURATION.
2. RECREATIONAL VEHICLES ON DISPLAY SHALL BE PLACED IN SUCH A MANNER AS TO NOT ENCRUCH ON OR DAMAGE SITE LANDSCAPING OR LIGHTING FIXTURES.



EXPIRES: 06/30/19
SIGNATURE DATE:
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PROCTOR BLVD. (US HWY 26)

FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

CONCEPT RV DISPLAY LAYOUT
SITE DEVELOPMENT
SANDY, OREGON 97055

C4
10

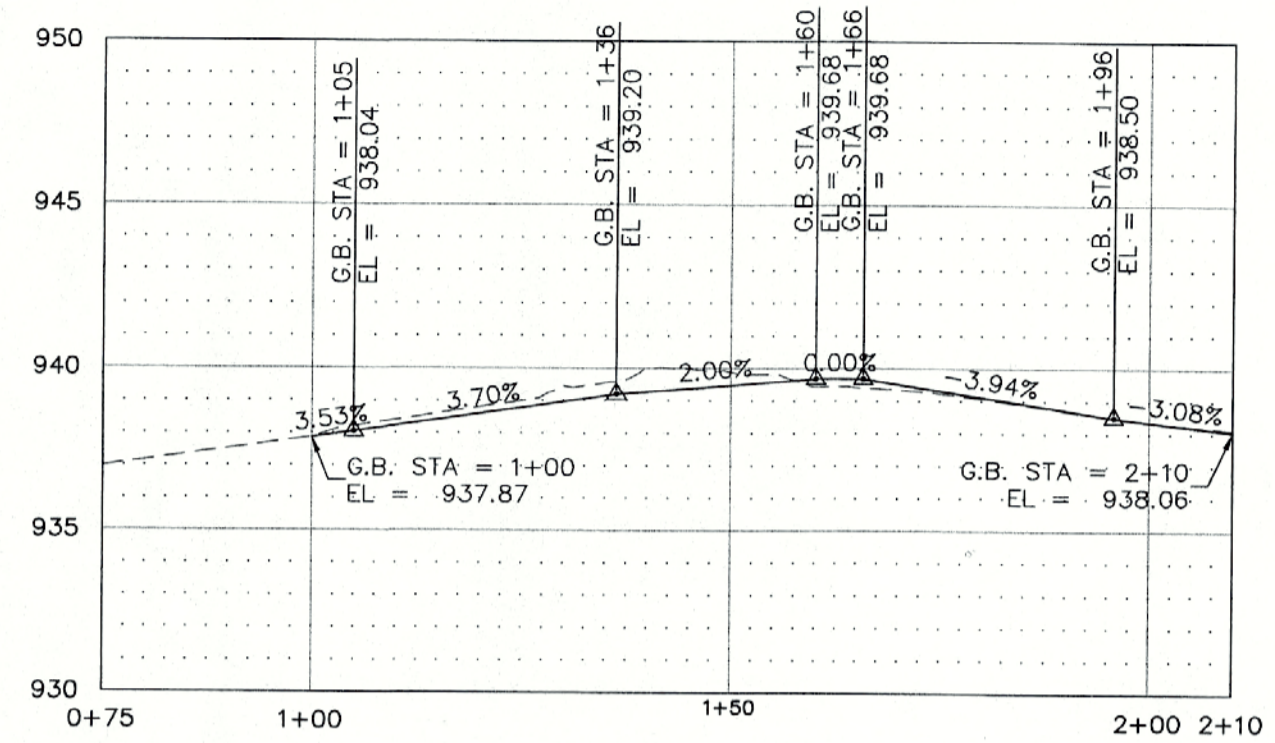
DATE:	NO.	REVISION

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SURVEYING + ENGINEERING + PLANNING

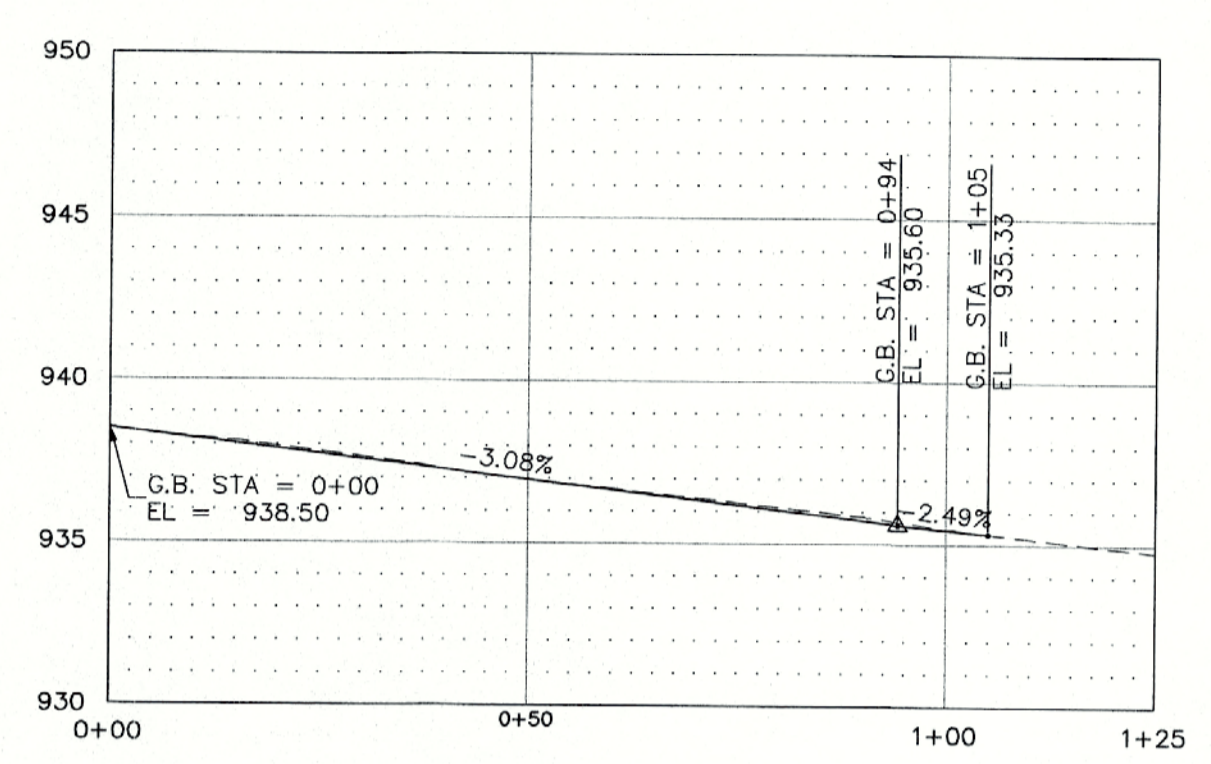
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TROUTDALE, OREGON 97060
BUS: (503) 668-3737 • FAX: (503) 668-3788

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SCALE: AS SHOWN DATE: FEB 5, 2019
PROJECT NO. E17-060

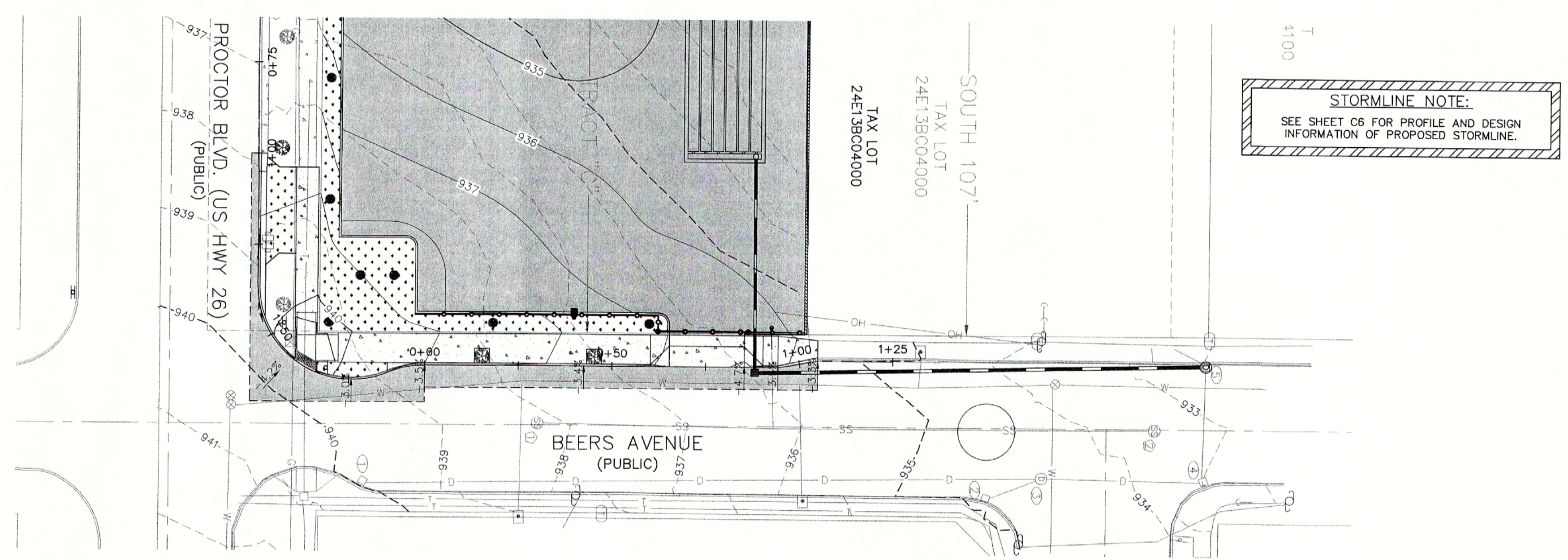
FOR REVIEW
OREGON
Jan 14, 2019
KELLY A. GROVER



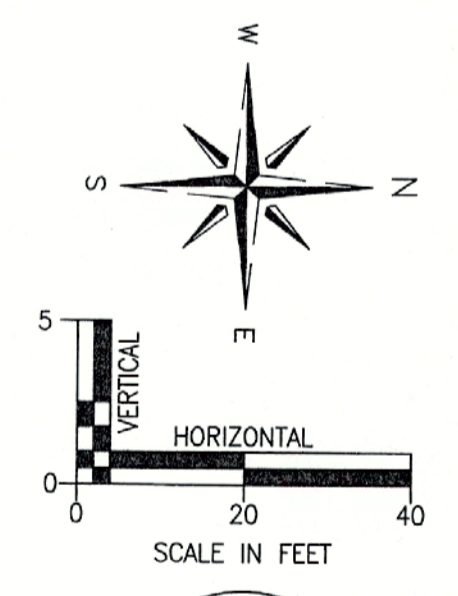
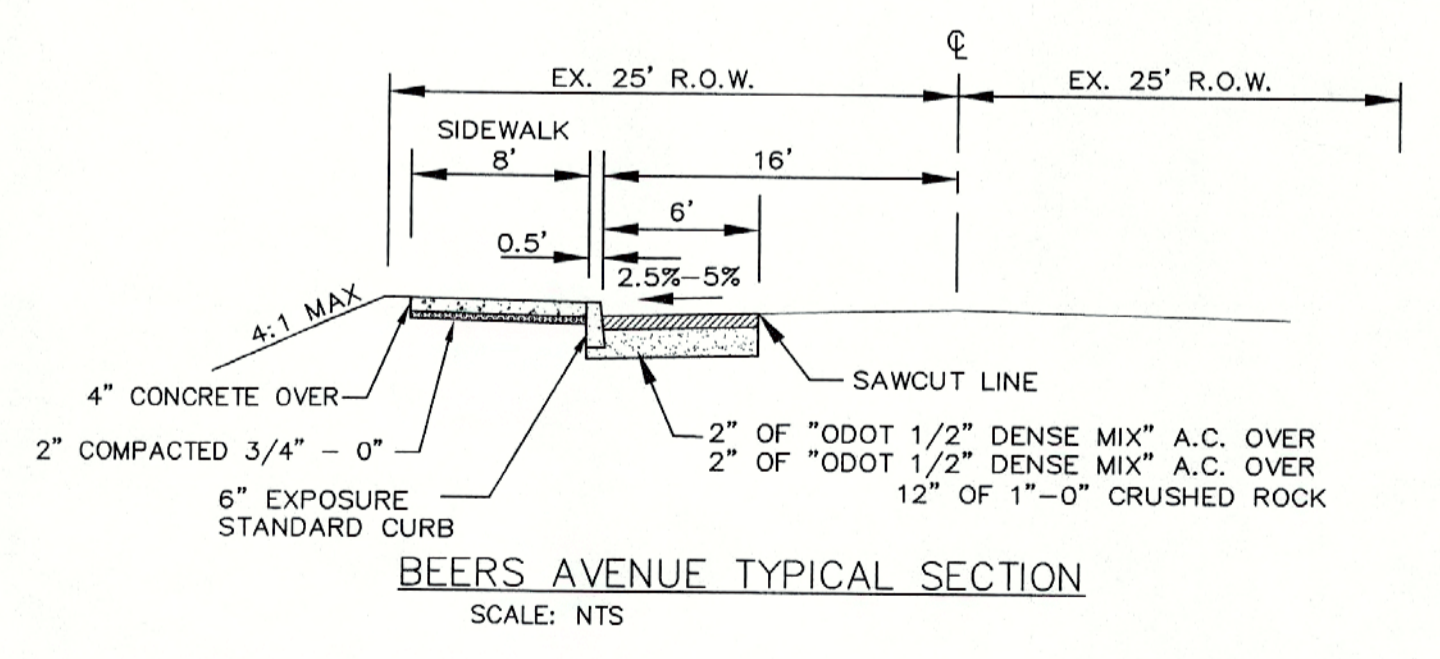
CURB RETURN PROFILE
SCALE: SEE SCALE BAR



BEERS AVENUE FLOWLINE PROFILE
SCALE: SEE SCALE BAR



STORMLINE NOTE:
SEE SHEET C6 FOR PROFILE AND DESIGN INFORMATION OF PROPOSED STORMLINE.



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DATE:	NO.	REVISION

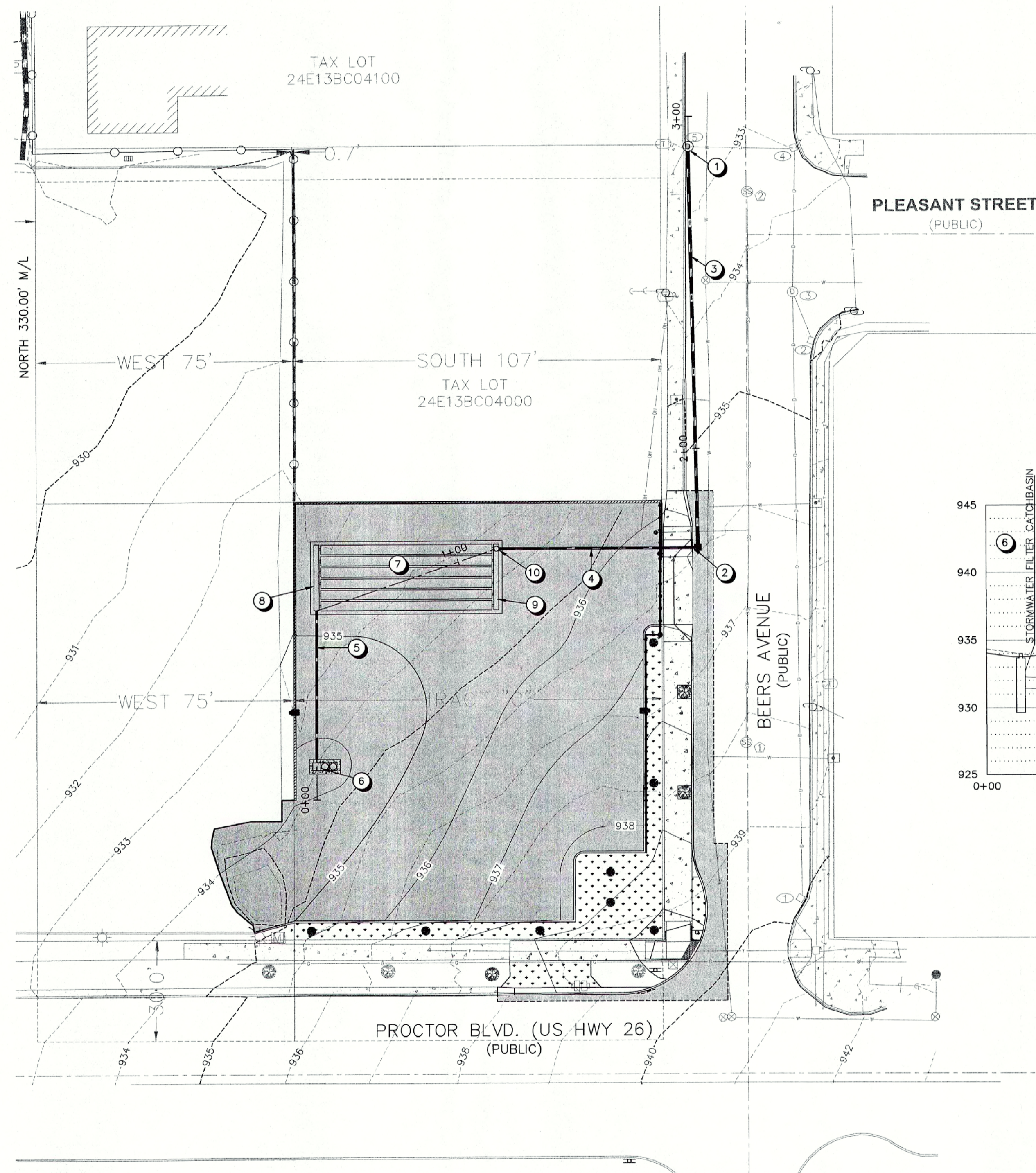
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PROJECT NO. E17-060		

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FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

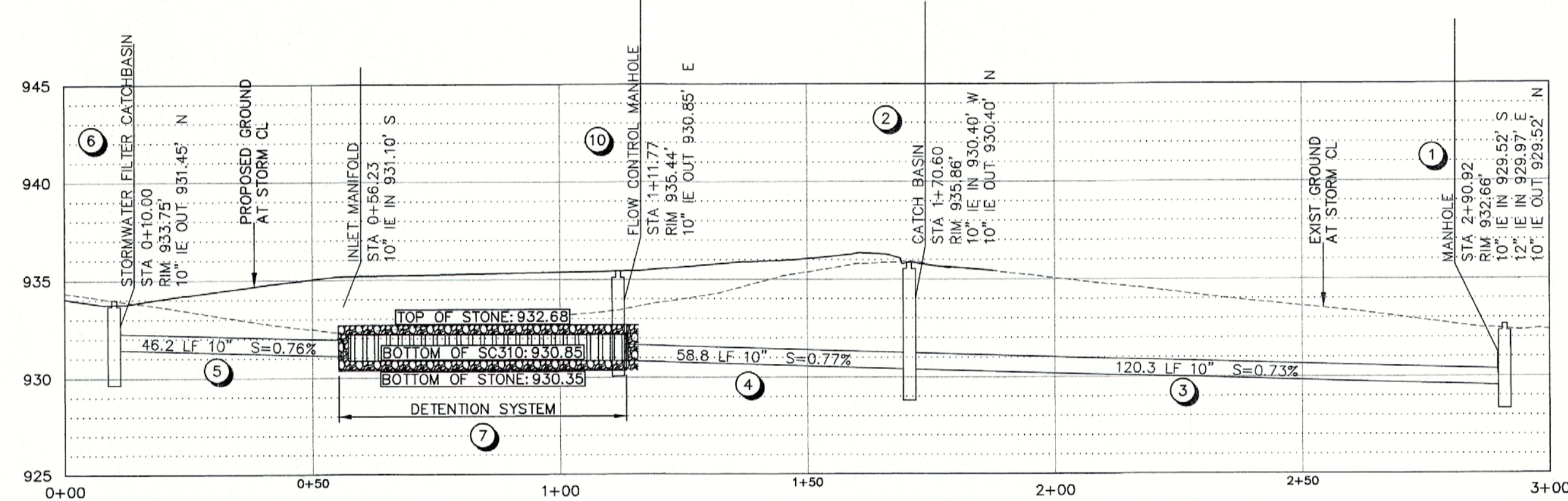
BEERS AVENUE PLAN & PROFILE
SITE DEVELOPMENT
SANDY, OREGON 97055

C5
10

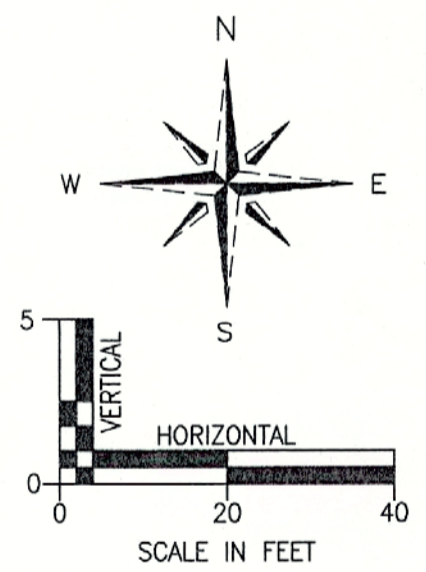


KEY NOTES

- 1 REMOVE EXISTING CATCH BASIN AND INSTALL MANHOLE SEE DETAIL SHEET C8
RIM = 932.35
IE 6" IN W. (EXIST) = 930.51
IE 12" IN E. (EXIST) = 929.97
IE 12" OUT N. (EXIST) = 929.52
IE 10" IN S. = 929.52
- 2 CATCH BASIN SEE DETAIL SHEET C8
RIM = 935.86
IE 10" IN W. = 930.95
IE 10" OUT N. = 930.95
- 3 10" HDPE STORM LINE
120.3 L.F., S = 0.0073
- 4 10" HDPE STORM LINE
58.8 L.F., S = 0.0077
- 5 10" HDPE STORM LINE
46.2 L.F., S = 0.0076
- 6 STORMWATER FILTER CATCHBASIN SEE DETAIL SHEET C10
RIM ELE = 933.75
IE 10" OUT N. = 932.03
- 7 STORMWATER DETENTION, 300 LF STORMTECH CHAMBERS SEE DETAIL SHEET C10
- 8 18" ECCENTRIC HEADER 16.2 LF.
IE 10" IN = 931.10
CONNECT STORMTECH CHAMBERS TO HEADER PER MANUFACTURER REQUIREMENTS
- 9 18" ECCENTRIC HEADER 16.2 LF.
IE 10" OUT = 930.85
CONNECT STORMTECH CHAMBERS TO HEADER PER MANUFACTURER REQUIREMENTS
- 10 INSTALL FLOW CONTROL MANHOLE SEE DETAIL SHEET C8
RIM ELE = 935.44
IE 10" OUT = 931.54



STORMLINE PROFILE
SCALE: SEE SCALE BAR



REGISTERED PROFESSIONAL
ENGINEER
67149PE
FOR REVIEW
OREGON
14, 2008
KELLI A. GROVER
EXPIRES: 06/30/19
SIGNATURE DATE:
PRELIMINARY

DATE:	NO.	REVISION

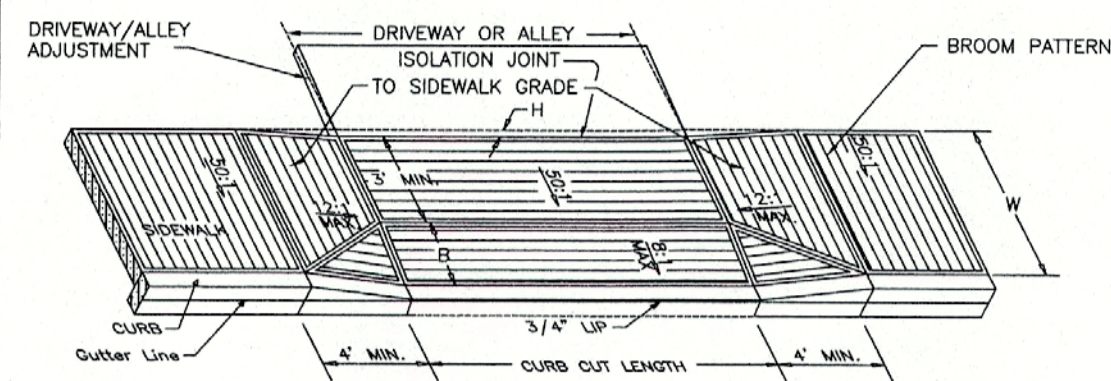
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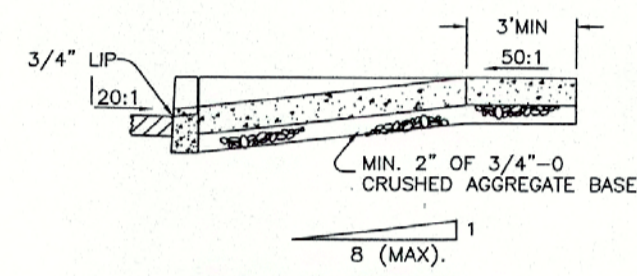
FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

UTILITY PLAN
SITE DEVELOPMENT
SANDY, OREGON 97055

C6
10



W	B	H
5'	2'	0.08' (1")
6'	2'	0.08' (1")
8'	4'	0.08' (1")



TYPICAL SECTION THROUGH DRIVEWAY/ALLEY RAMP

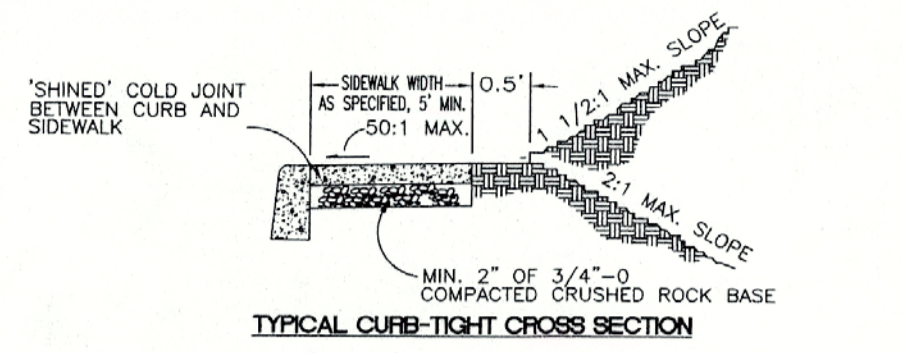
NOTES:

- RESIDENTIAL DRIVEWAYS AND SIDEWALK SECTIONS THROUGH DRIVEWAYS SHALL HAVE A MINIMUM NOMINAL THICKNESS OF 4". CONCRETE STRENGTH SHALL BE 3300 PSI.
- CONCRETE FOR COMMERCIAL USE AND ALLEY APPROACHES SHALL HAVE A MINIMUM NOMINAL THICKNESS OF 6". CONCRETE STRENGTH SHALL BE 3300 PSI.
- CURB TRANSITIONS FOR COMMERCIAL USE AND ALLEY APPROACHES SHALL BE 5 FEET.
- CONSTRUCT CONTRACTION JOINT IN CENTER OF DRIVEWAY WHEN DRIVEWAY WIDTH EXCEEDS 15'.
- NO LIP AT GUTTER LINE IF USED AS SIDEWALK ACCESS RAMP.
- SEE SIDEWALK DETAILS FOR RESTRICTIONS AND SPECIFICATIONS NOT SHOWN.
- THE 50:1 CROSS-SLOPE OF SIDEWALK IS MEASURED FROM HORIZONTAL. THE 12:1 SLOPE OF SIDEWALK TRANSITION TO DRIVEWAY/ALLEY IS RELATIVE TO THE RUNNING SLOPE OF THE SIDEWALK. THE SLOPE OF THE APRON IS MEASURED RELATIVE TO HORIZONTAL.

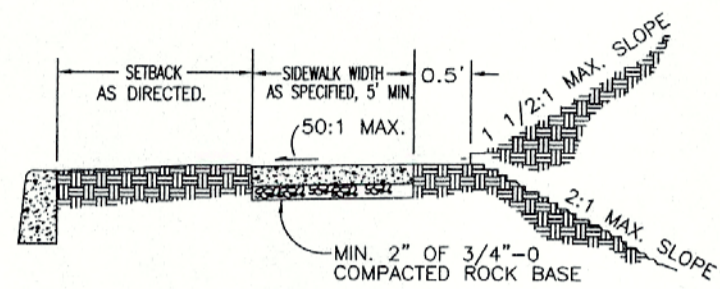
CITY OF SANDY

DRIVEWAY AND ALLEY APPROACHES WITH DEPRESSED CURBLINE SIDEWALK

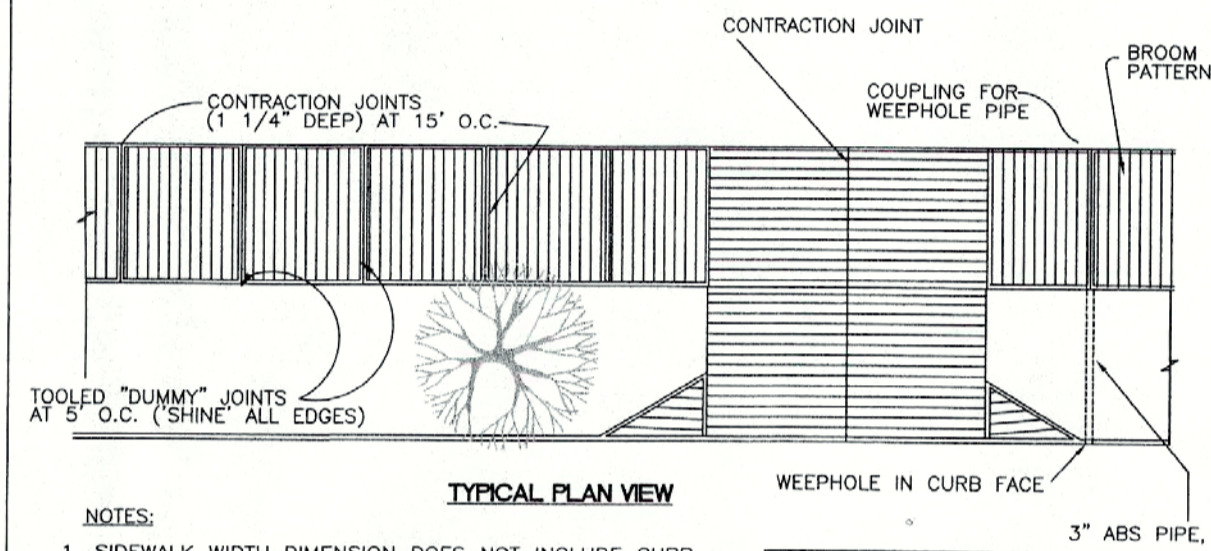
DATE: SEPT 1997 DRAWING NO. 208



TYPICAL CURB-TIGHT CROSS SECTION



TYPICAL SETBACK TYPE CROSS SECTION



TYPICAL PLAN VIEW

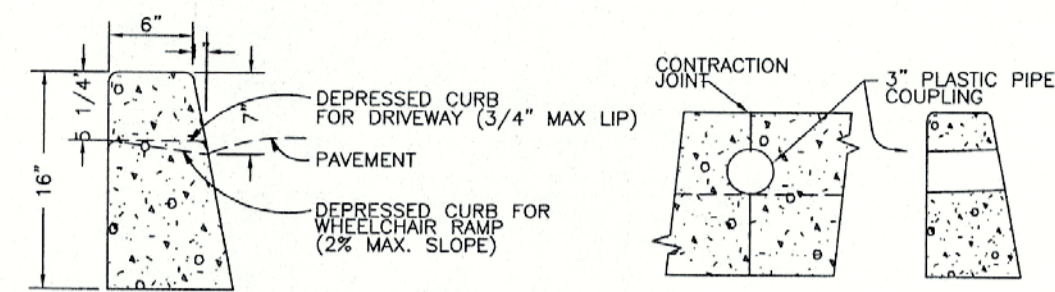
TOOLED "DUMMY" JOINTS AT 5' O.C. ("SHINE" ALL EDGES)

- NOTES:
- SIDEWALK WIDTH DIMENSION DOES NOT INCLUDE CURB FOR CURB-TIGHT SIDEWALKS.
 - SIDEWALKS 8 FEET AND WIDER SHALL HAVE A LONGITUDINAL CONTRACTION JOINT AT THE MIDPOINT.
 - CONCRETE THICKNESS FOR STANDARD SIDEWALKS SHALL BE NOMINAL 4" MIN.; THICKNESS IN DRIVEWAY SHALL BE 6"
 - INSTALL 3" ABS WEEPHOLE PIPES IN SIDEWALKS IN LOCATIONS DIRECTED BY THE ENGINEER. PLACE CONTRACTION JOINT OVER THE TOP OF THE PIPE.

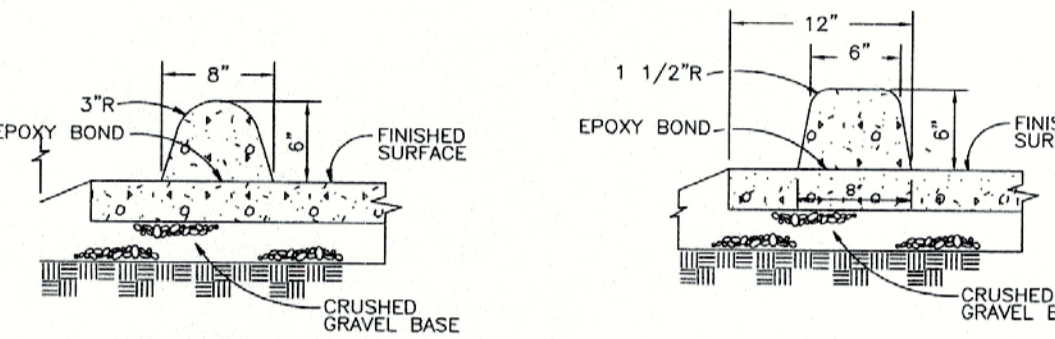
CITY OF SANDY

SIDEWALK

DATE: SEPT 2004 DRAWING NO. 205



TYPE "C" CURB



EXTRUDED AC BONDED CURB

EXTRUDED CONCRETE BONDED CURB

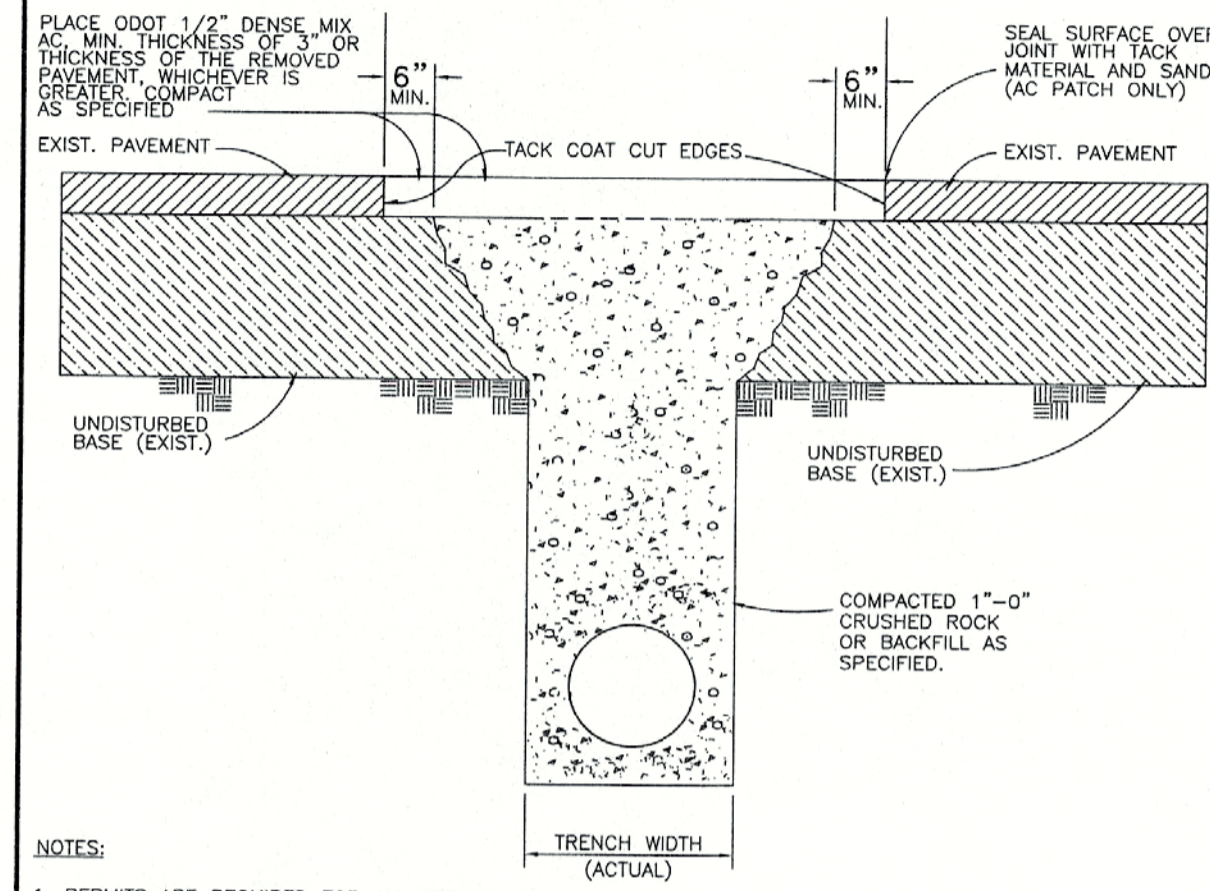
NOTES:

- ISOLATION JOINTS SHALL BE PLACED ONLY AS SPECIFIED.
- CONTRACTION JOINTS SHALL BE PLACED AT 15' INTERVALS AND SHALL EXTEND AT LEAST 50% THROUGH THE CURB.
- A CONTRACTION JOINT SHALL BE PLACED ALONG AND OVER WEEP HOLE THROUGH THE CURB AND THROUGH THE SIDEWALK.
- WHEN SIDEWALKS ARE CONSTRUCTED, EXTEND 3" PIPE TO BACK OF SIDEWALK AND INSTALL COUPLING.

CITY OF SANDY

CURB AND WEEPHOLE

DATE: SEPT 1997 DRAWING NO. 203



NOTES:

- PERMITS ARE REQUIRED FOR ANY STREET CUT. CALL 503-668-5533 OR VISIT WWW.CI.SANDY.ORG FOR AN APPLICATION.
- ALL EXISTING AC OR PCC PAVEMENT SHALL BE SAWCUT PRIOR TO SURFACE RESTORATION.
- PCC PAVEMENT SHALL BE REPLACED WITH CONCRETE TO A MINIMUM THICKNESS OF 8" OR TO THE THICKNESS OF THE REMOVED PAVEMENT, WHICHEVER IS GREATER.
- TRENCH BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM RELATIVE DENSITY (ASTM T-99). THE CITY MAY REQUIRE TESTING TO VERIFY COMPACTION.
- IF EXISTING BASE MATERIAL IS CTB OR ATB, THEN REPLACEMENT MATERIAL SHALL MATCH EXISTING. CDF MAY BE SUBSTITUTED WITH APPROVAL OF CITY ENGINEER.
- STREETS WITH RECENT OVERLAYS (≤ 3 YEARS) MAY HAVE MORE EXTENSIVE SURFACE RESTORATION REQUIREMENTS.

CITY OF SANDY

STREET OPENING

DATE: AUG 2004 DRAWING NO. 213



EXPIRES: 06/30/19
SIGNATURE DATE: _____
PRELIMINARY

DATE	NO.	REVISION

DRAWN: DESIGNED: CHECKED:
SCALE: AS SHOWN DATE: FEB 5, 2019
PROJECT NO. E17-060

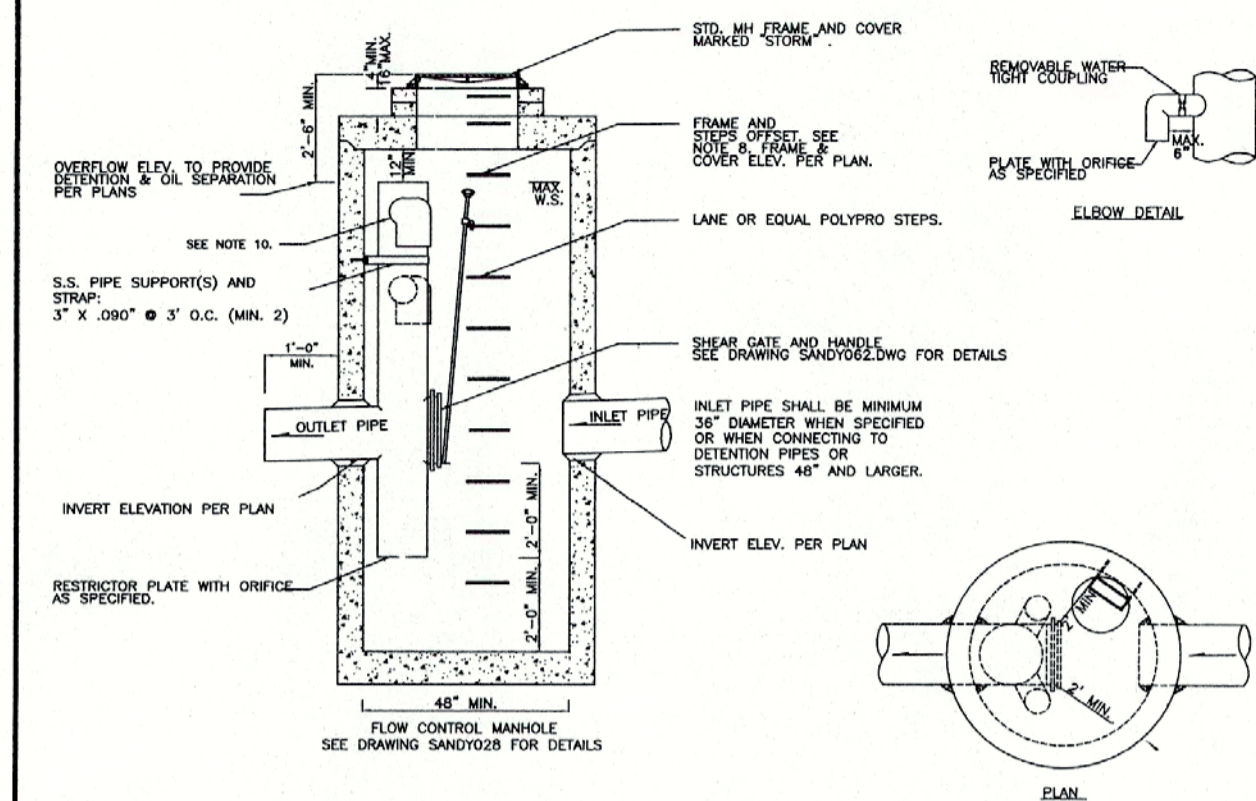
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Firwood Design Group, LLC
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BUS: (503) 668-3737 FAX: (503) 668-3788

FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

DETAILS SHEET - CITY OF SANDY
SITE DEVELOPMENT
SANDY, OREGON 97055

C7
10



NOTES:

- PIPE SIZES AND SLOPES PER PLANS.
- OUTLET CAPACITY: NOT LESS THAN COMBINED INLETS.
- EXCEPT AS SHOWN OR NOTED, UNITS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS FOR STORM OR SANITARY MANHOLES 48" MIN. DIAM.
- PIPE SUPPORTS AND RESTRICTOR/SEPARATOR SHALL BE OF SAME MATERIAL AND BE ANCHORED AT 2' MAX SPACING OR EMBEDDED 2" IN WALL.
- THE RESTRICTOR/SEPARATOR SHALL BE FABRICATED FROM 300# ALUMINUM OR 10#4 ALUMINIZED STEEL OR 10#4 GALVANIZED STEEL PIPE IN ACCORDANCE WITH ASHTR 8.30.11.15.1.1.1 AND 11.17.1. GALVANIZED STEEL SHALL HAVE TREATMENT 'T'.
- OUTLET SHALL BE CONNECTED TO CURB OR STORM PIPE WITH A STANDARD COUPLING BAND FOR CMP OR PE PIPE OR FIBRO COUPLER FOR CONCRETE PIPE.
- THE VERTICAL RISER STEM OF THE RESTRICTOR/SEPARATOR SHALL BE THE SAME DIA. AS THE HORIZONTAL OUTLET PIPE WITH AN 8" MIN. DIA.
- FRAME AND LADDER OR STEPS OFFSET SO THAT: A. CLEARANCE GATE IS VISIBLE FROM 15'; B. CLEAR DOWN SPACE IS CLEAR OF RISER AND CLEARANCE GATE; C. FRAME IS CLEAR OF CURB.
- IF CMP OUTLET PIPE CONNECTS TO CONCRETE PIPE, CMP OUTLET PIPE SHALL HAVE SMOOTH O.D. EQUAL TO CONCRETE PIPE (D. LESS 1/4").
- MULTI-ORIFICE ELBOWS MAY BE LOCATED AS SHOWN OR ALL ON ONE SIDE OF RISER TO ASSURE LARGER CLEARANCE.

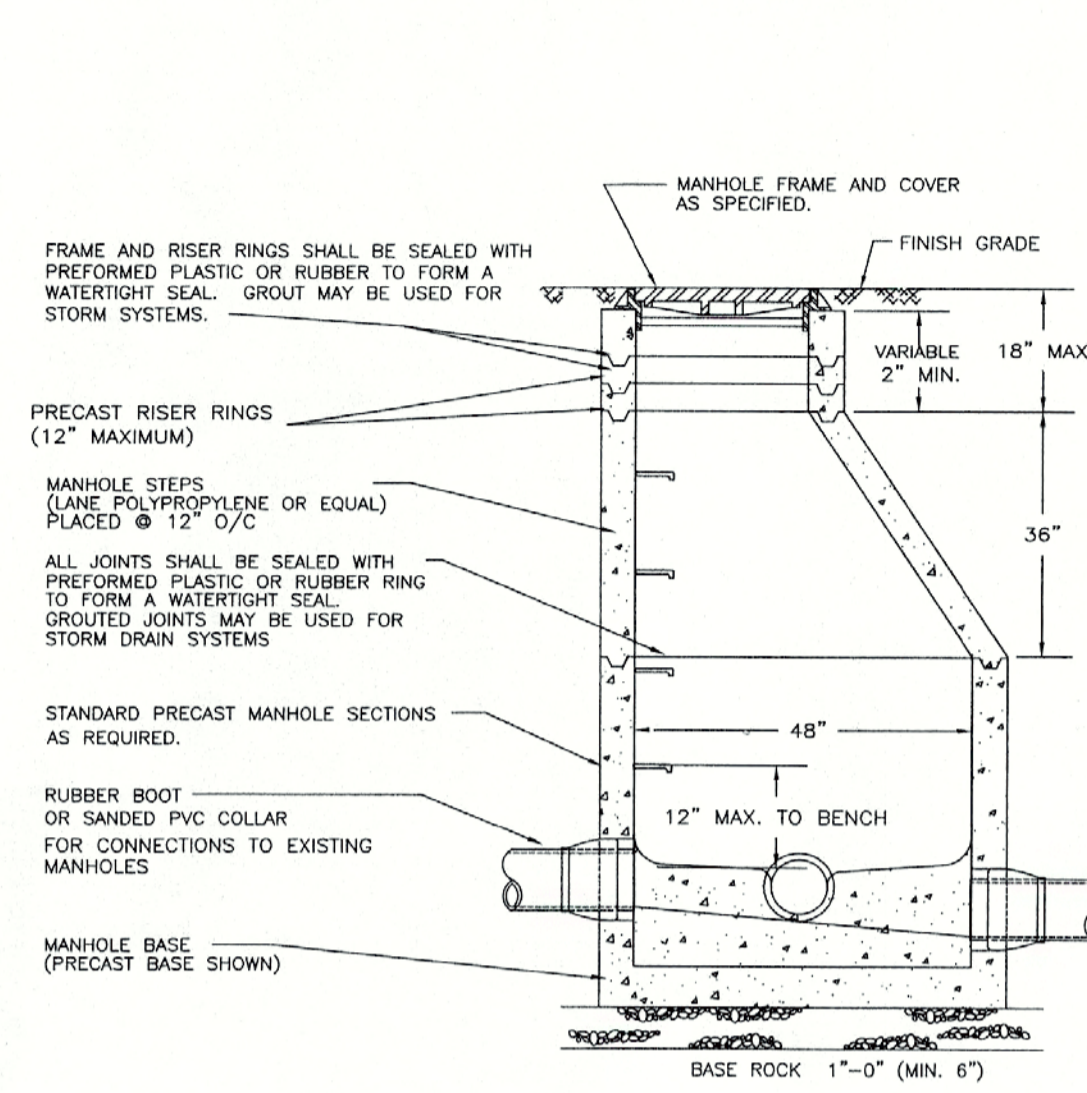
ORIFICE AND INVERT ELEVATION TABLE

ORIFICE NO.	ORIFICE NO.	ORIFICE NO.	ORIFICE NO.
DIAMETER _____ IN.	DIAMETER _____ IN.	DIAMETER _____ IN.	DIAMETER _____ IN.
ELEVATION _____ FT.	ELEVATION _____ FT.	ELEVATION _____ FT.	ELEVATION _____ FT.
RIM ELEVATION _____ FT.			
INLET PIPE INVERT ELEVATION _____ FT.			
OVERFLOW ELEVATION _____ FT.			
OUTLET PIPE ELEVATION _____ FT.			
SUMP ELEVATION _____ FT.			

CITY OF SANDY

FLOW CONTROL MH

DATE: JUNE 2004 DRAWING NO: SANDY 065DWG



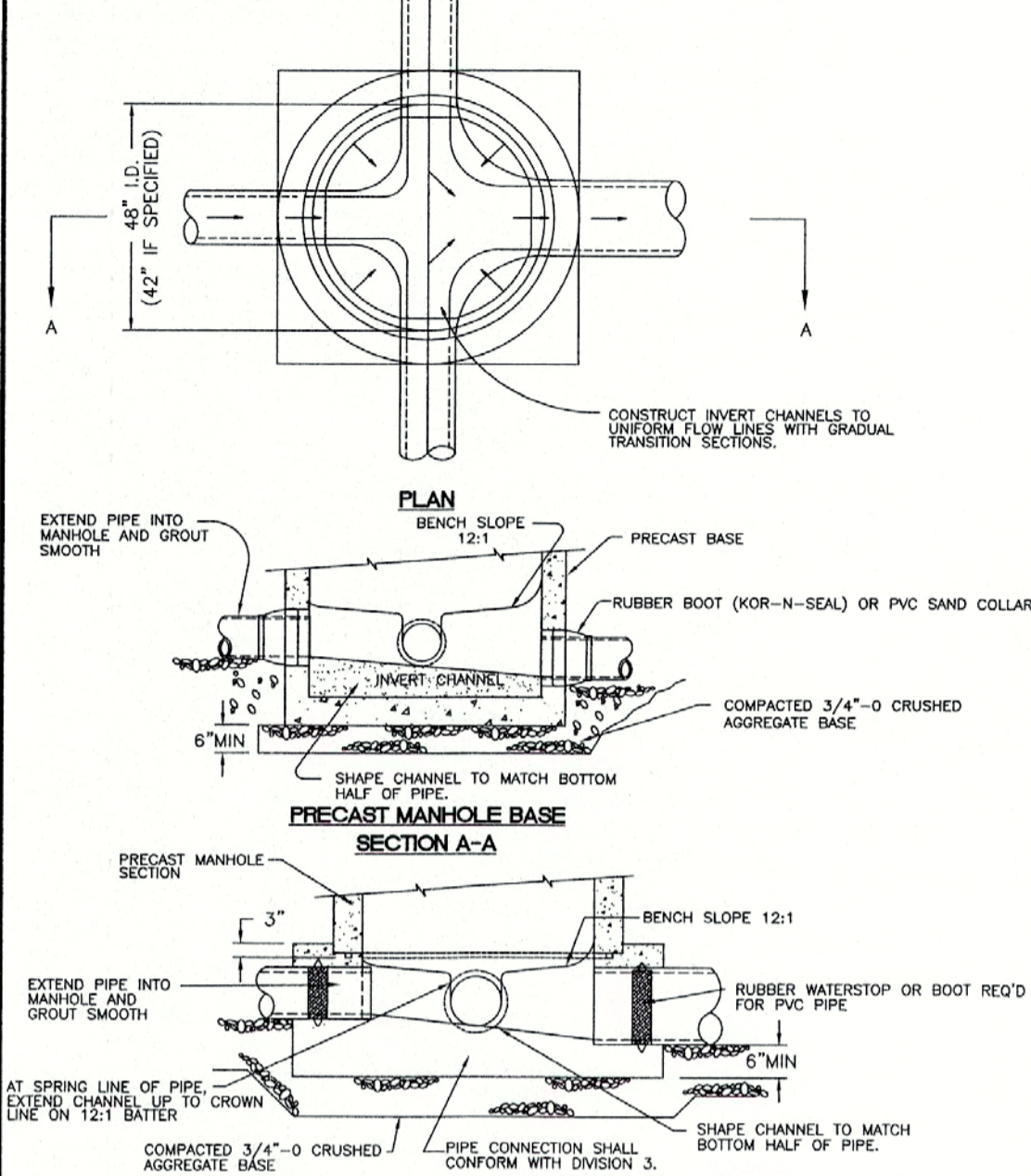
NOTES:

- STANDARD PRECAST MANHOLE SECTION DIAMETER SHALL BE 48". USE 42" IF SPECIFIED BY THE ENGINEER.
- ALL CONNECTING PIPES SHALL HAVE A FLEXIBLE JOINT WITHIN 18" OF MANHOLE WALL (IF BOOT IS NOT USED).
- SEE MANHOLE BASE SECTION DRAWING FOR BASE DETAILS.
- MAXIMUM DISTANCE FROM TOP STEP TO FINISH GRADE SHALL NOT EXCEED 24".

CITY OF SANDY

STORM AND SANITARY SEWER MANHOLE

DATE: NOV 1997 DRAWING NO: 315



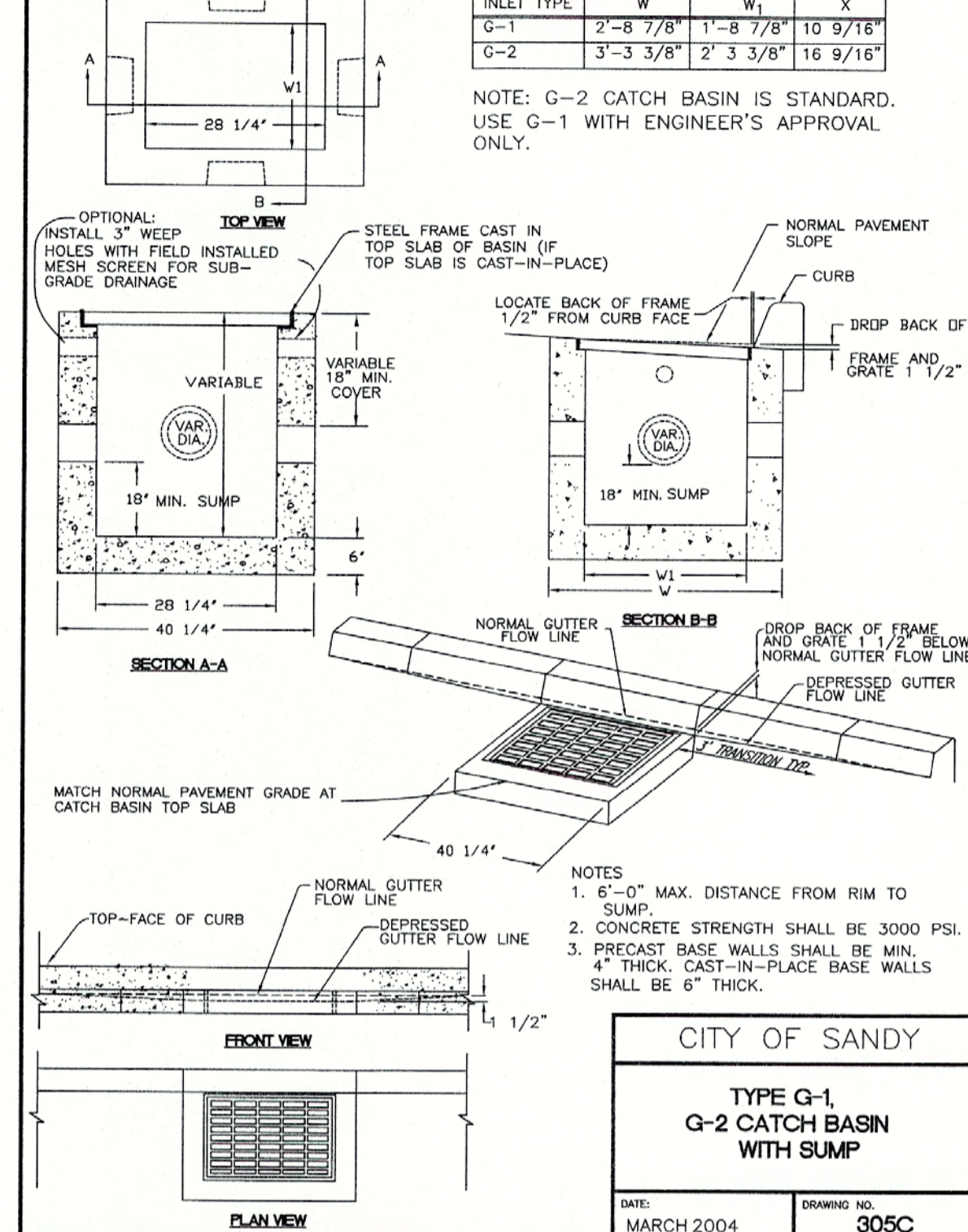
NOTES:

- CONCRETE SHALL BE CLASS 3000.
- CHANNELS SHALL BE CONSTRUCTED TO PROVIDE SMOOTH SLOPES AND RADII TO OUTLET PIPE.
- BASES MAY BE PRECAST OR POURED IN PLACE.
- THIS MANHOLE BASE SECTION SHALL BE USED FOR PIPE SIZES UP TO 24".
- USE RUBBER BOOTS FOR NEW MANHOLE INSTALLATION. USE PVC SAND COLLAR FOR EXISTING MANHOLE CONNECTION.

CITY OF SANDY

MANHOLE BASE SECTION

DATE: AUG 1996 DRAWING NO: 316



INLET TYPE	W	W1	X
G-1	2'-8 7/8"	1'-8 7/8"	10' 9/16"
G-2	3'-3 3/8"	2' 3 3/8"	16' 9/16"

NOTE: G-2 CATCH BASIN IS STANDARD. USE G-1 WITH ENGINEER'S APPROVAL ONLY.

- NOTES:
- 6"-0" MAX. DISTANCE FROM RIM TO SUMP.
 - CONCRETE STRENGTH SHALL BE 3000 PSI.
 - PRECAST BASE WALLS SHALL BE MIN. 4" THICK. CAST-IN-PLACE BASE WALLS SHALL BE 6" THICK.

CITY OF SANDY

TYPE G-1, G-2 CATCH BASIN WITH SUMP

DATE: MARCH 2004 DRAWING NO: 305C



EXPIRES: 06/30/19
SIGNATURE DATE: _____
PRELIMINARY

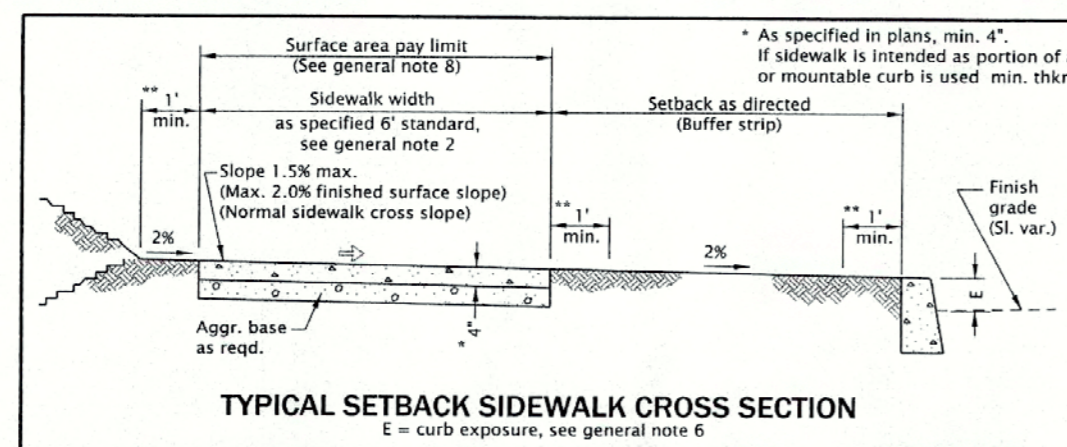
DATE:	NO.	REVISION
DRAWN: DESIGNED: CHECKED:		
SCALE: AS SHOWN DATE: FEB 5, 2019		
PROJECT NO. E17-060		

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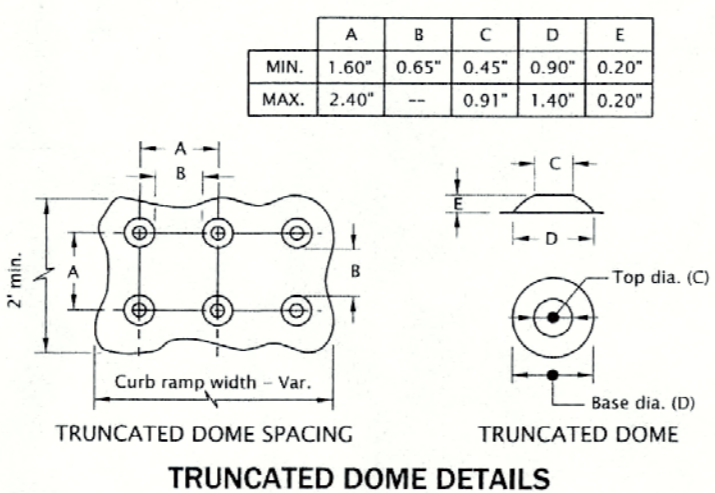
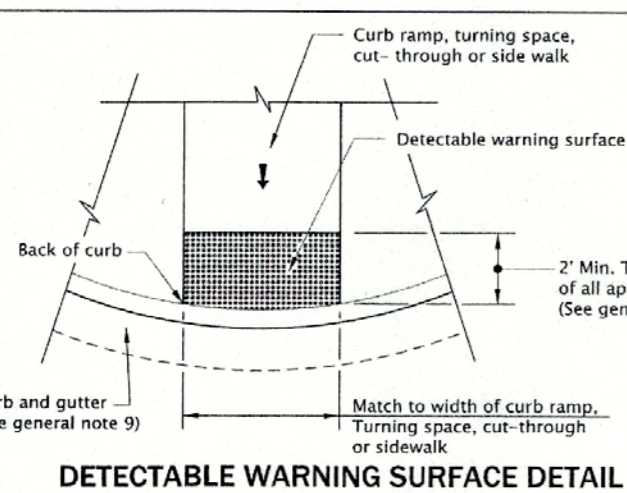
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- GENERAL NOTES FOR ALL DETAILS:**
1. Include additional paved or unpaved 2' clearance to vertical faces higher than 5' such as retaining walls, sound walls, fences and buildings.
 2. Curb type and sidewalk width as shown on plans or as directed.
 3. On sidewalks 8' and wider, provide a longitudinal joint at the midpoint.
 4. Install 3" PVC weep hole pipes in sidewalks where shown on plans, and allowed by jurisdiction. Place contraction joint over top of pipe. See Std. Dwg. RD700 for weep hole detail.
 5. Const. expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveway.
 6. For monolithic curb & sidewalk, const. expansion joints at 45' maximum spacing.
 7. Const. contraction joints at 15' maximum spacing, and at ends of each driveway and curb ramp.
 8. For curb details, see Std. Dws. RD700 & RD701.
 9. Sidewalk details are based on ODOT applicable standards.
 10. For driveway details not shown, see Std. Dws. RD725, RD730, RD735, RD740, RD745 & RD750.
 11. See project plans for details not shown.

CALC. BOOK NO.	N/A	BASELINE REPORT DATE	16-JUL-2018
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications			
OREGON STANDARD DRAWINGS			
SIDEWALKS			
2018			
DATE	REVISION	DESCRIPTION	
01-2018	REVISED	NOTE	
02-2018	REVISED	DETAILS & NOTES	

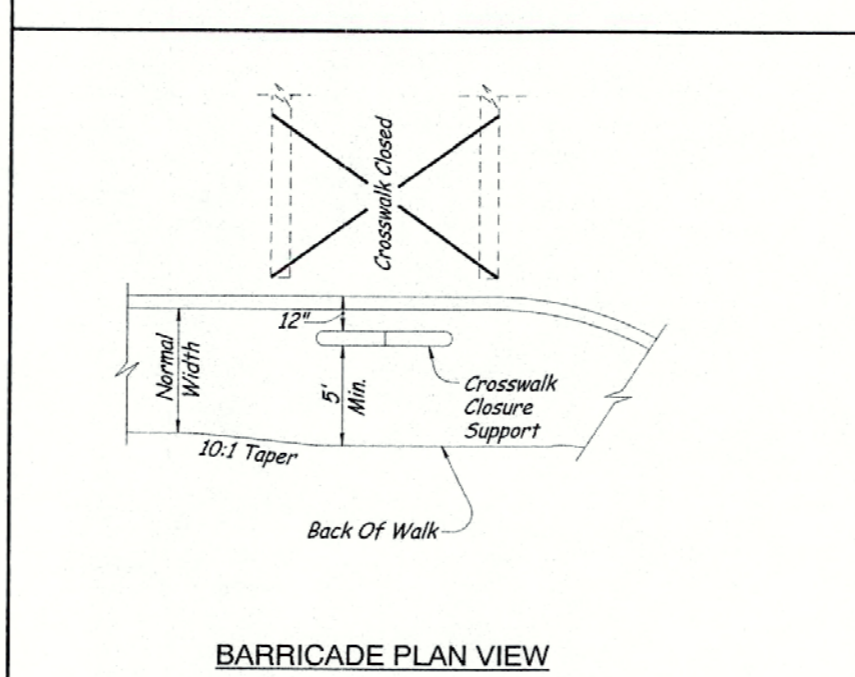
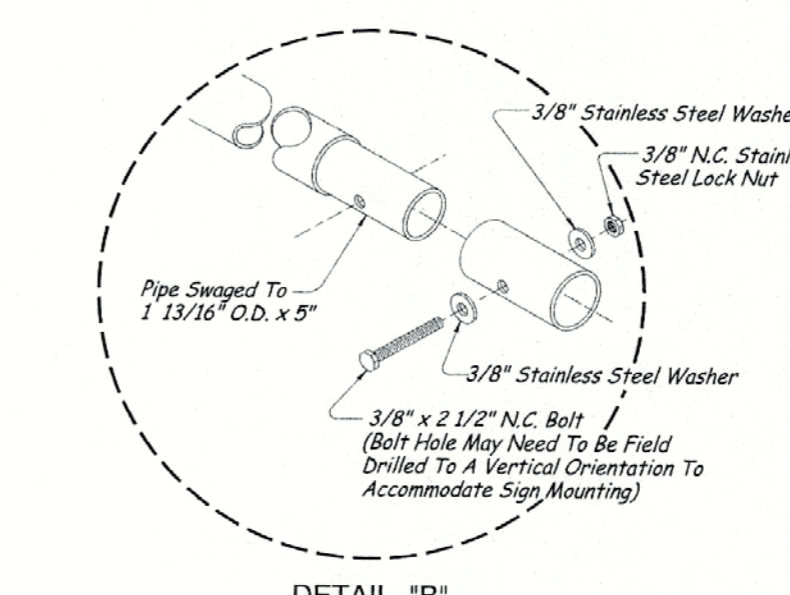
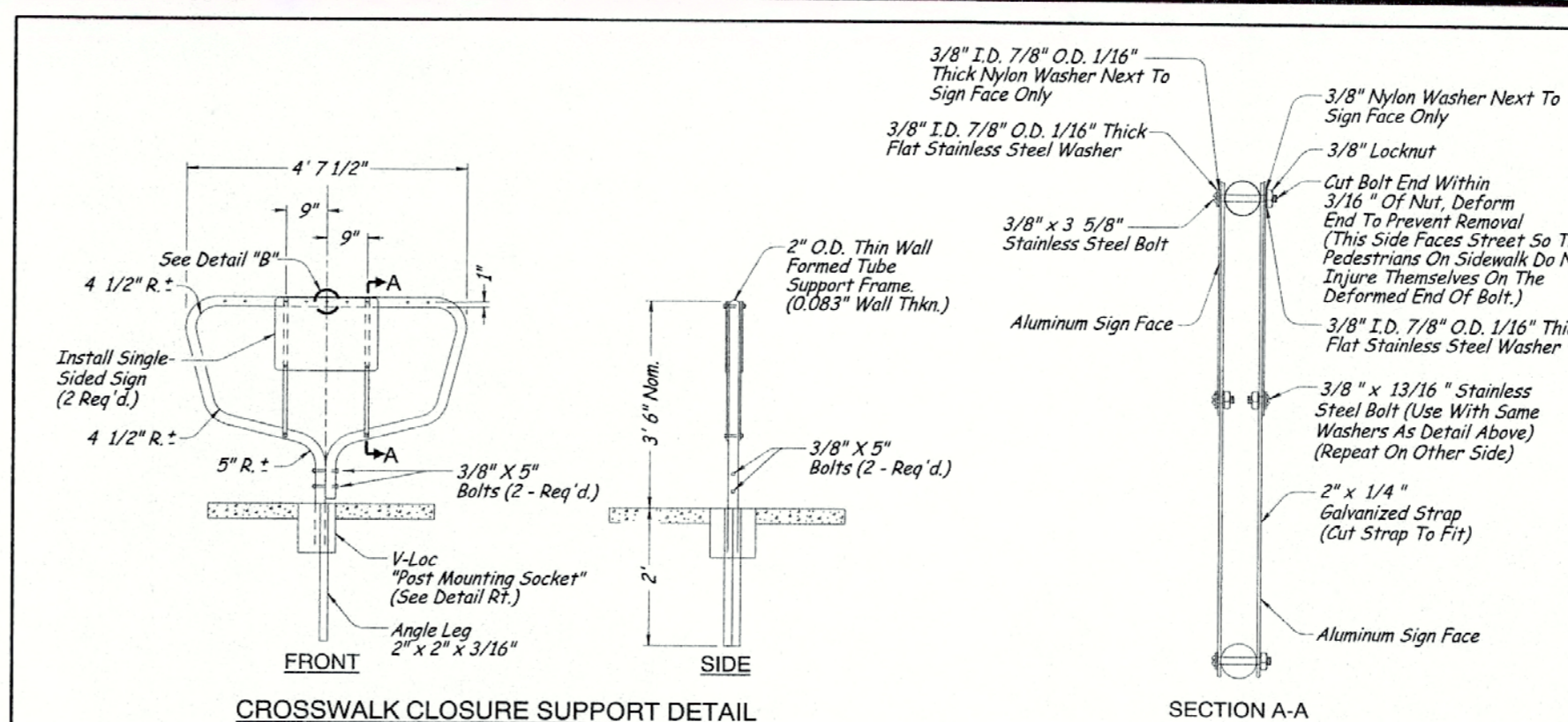
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



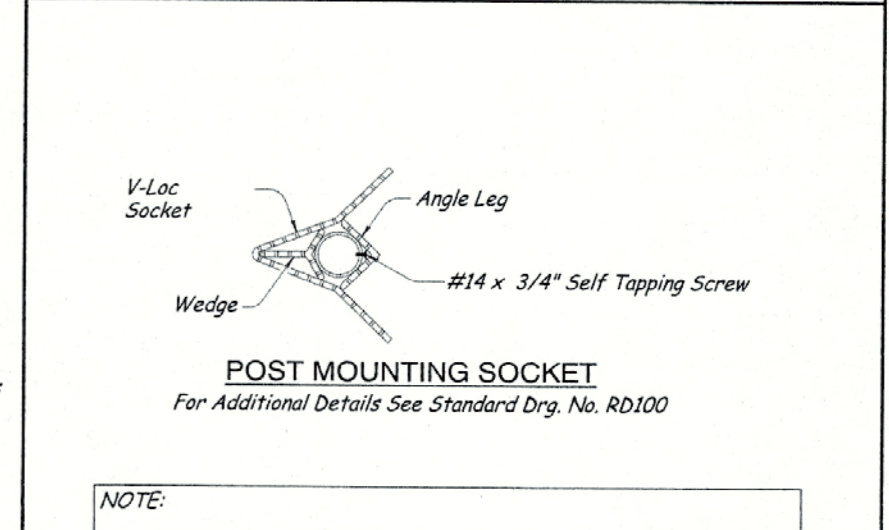
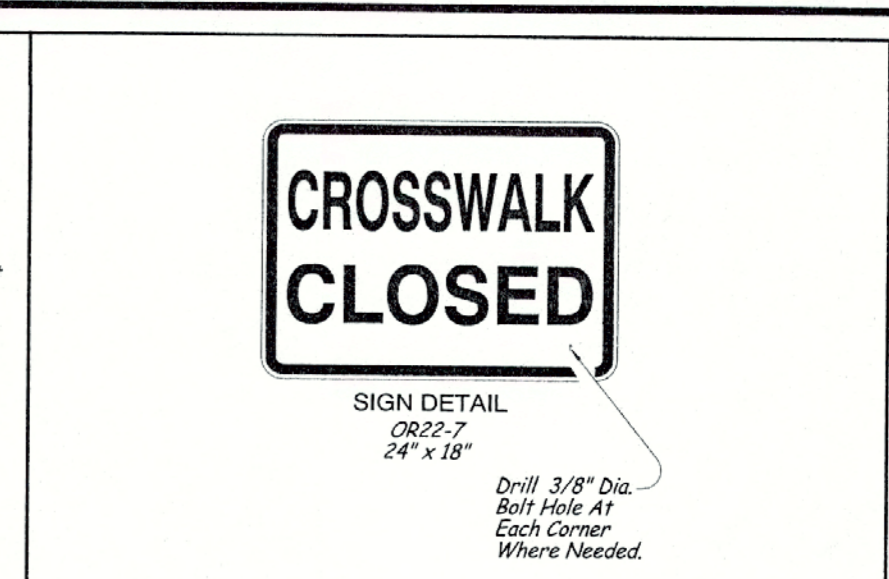
- GENERAL NOTES FOR ALL DETAILS:**
1. Detectable warning surface details & locations are based on ODOT applicable Standards.
 2. See project plans for details not shown.
 3. See Std. Dws. RD700 & RD701 for curbs.
 4. See Std. Dwg. RD720 for sidewalks.
 5. See Std. Dws. TMS05 & TMS30 for crosswalk markings, widths, etc.
 6. See Std. Dws. RD705 & RD710 for Islands.
 7. The detectable warning surface shall extend the full width of the curb ramp, or other roadway entrance as applicable. A gap of up to 2 inches on each side of the detectable warning surface is permitted.
 8. The detectable warning surface shall typically be placed at the back of curb across the roadway entrance. Detectable warning surface placement for perpendicular ramps vary as shown. Detectable warning surface may be radial or rectangular, but must comply with the truncated dome size and spacing standards. Detectable warning surface may be cut to meet necessary shape as shown in plans. Color to be safety yellow if no color specified in construction note.
 9. Detectable warning surface shall be used where the pedestrian access route meets the street, in the following locations:
 - a) Curb ramps (See Std. Dws. RD755, RD756, & RD757).
 - b) Crossing Islands (Accessible Route Islands), (See Std. Dwg. RD710).
 - c) Rail crossings (See detail).
 10. Where public transportation stations (rail, bus, etc.) use platform boarding, detectable warning surface shall be placed along the full edge length of the station, when not protected by platform screens or guards.
 11. Detectable warning surface shall not be used in the following locations:
 - a) End of sidewalk transitions that are not at the crosswalk (See Std. Dwg. RD754).
 - b) Uncontrolled standard concrete driveways (See Std. Dws. RD725, RD730, RD735, RD740, RD745, & RD750).
 - c) Parking lots.
 12. Only use details allowed by jurisdiction.
 13. On or along state highways, curb and gutter is required at curb ramps.
 14. Grade breaks at the top and bottom of curb ramp runs shall be perpendicular to the direction of the ramp run. Grade breaks shall not be permitted on the surface of ramp runs and turning spaces. Surface slopes that meet at grade breaks shall be flush.

Detectable warning surface Slope 7.5% max. (Max. 8.3% finished surface slope)	Slope 1.5% max. (Max. 2.0% finished surface slope) (Normal sidewalk cross slope)		
CALC. BOOK NO.	N/A	BASELINE REPORT DATE	16-JUL-2018
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications			
OREGON STANDARD DRAWINGS			
DETECTABLE WARNING SURFACE DETAILS & PLACEMENT LOCATIONS			
2018			
DATE	REVISION	DESCRIPTION	

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



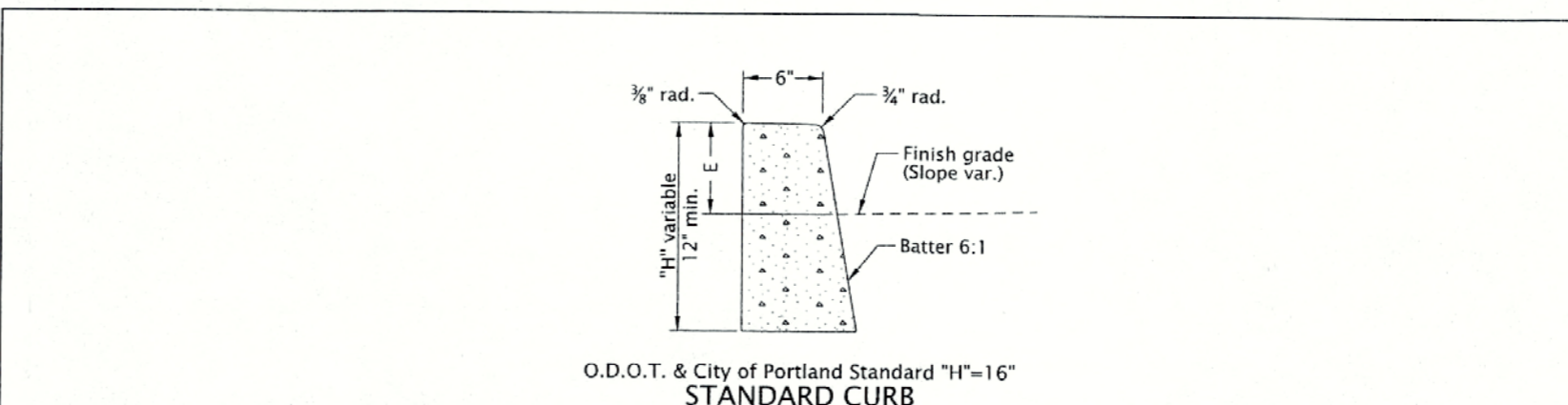
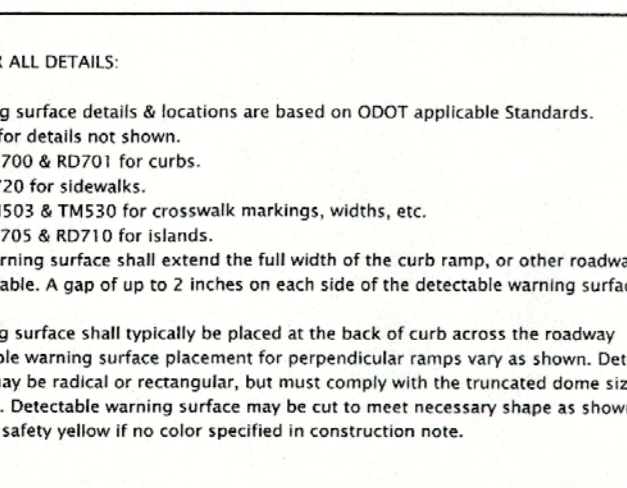
- GENERAL NOTE:**
1. All Holes In The Tube Support Frame To Be Pre-drilled By The Manufacturer. (1/32" Larger Than Mounting Bolt)
 2. Pipe Swaged To 1 13/16" O.D. x 5"



NOTE: Care Shall Be Taken That No Concrete Is Placed Within Mounting Socket.

CALC. BOOK NO.	N/A	BASELINE REPORT DATE	07-01-16
ACCOMPANIED BY BASELINE REPORT			
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications			
OREGON STANDARD DRAWINGS			
CROSSWALK CLOSURE DETAIL			
2015			
DATE	REVISION	DESCRIPTION	
07-01-16	REVISED	CHANGED PRACTICE (CHANGE FROM 4 TO 5)	

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



- GENERAL NOTES FOR ALL DETAILS:**
1. Curb exposure "E" = 6" to 9", as measured vertically from flowline to highest point on curb. Vary as shown on plans or as directed. O.D.O.T. standard "E" = 7".
 2. Const. expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveway.
 3. Const. contraction joints at 15' maximum spacing, and at ends of each inlet and ramp.
 4. Transitions shall be used to connect curbs of different exposures "E".
 5. "E" is the total vertical dimension of those curb surfaces having a slope of 1:1 or steeper. Minimum desirable transition length shall be 20' for each 1" difference in "E".
 6. Tops of all curbs shall slope toward the roadway at 1.5% max. (Max. 2.0% finished surface slope), unless otherwise shown, or as directed.
 7. Dimensions are nominal, vary to conform with curb machine approved by the engineer.
 8. Dimensions adjacent to radii are measured to the point of intersection of curb surfaces.
 9. For drainage curbs, see Std. Drg. RD701.
 10. For sidewalk ramp details, see Std. Drg. RD755.

NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications			
OREGON STANDARD DRAWINGS			
CURBS			
2015			
DATE	REVISION	DESCRIPTION	
01-2015	ADDED	NOTE	
02-2015	REVISED	NOTE	
03-2015	REVISED	NOTE	
07-2015	REVISED	DETAILS & ADDED NOTE	
07-2017	ADDED	NOTE	

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



EXPIRES: 06/30/19
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DATE	NO.	REVISION

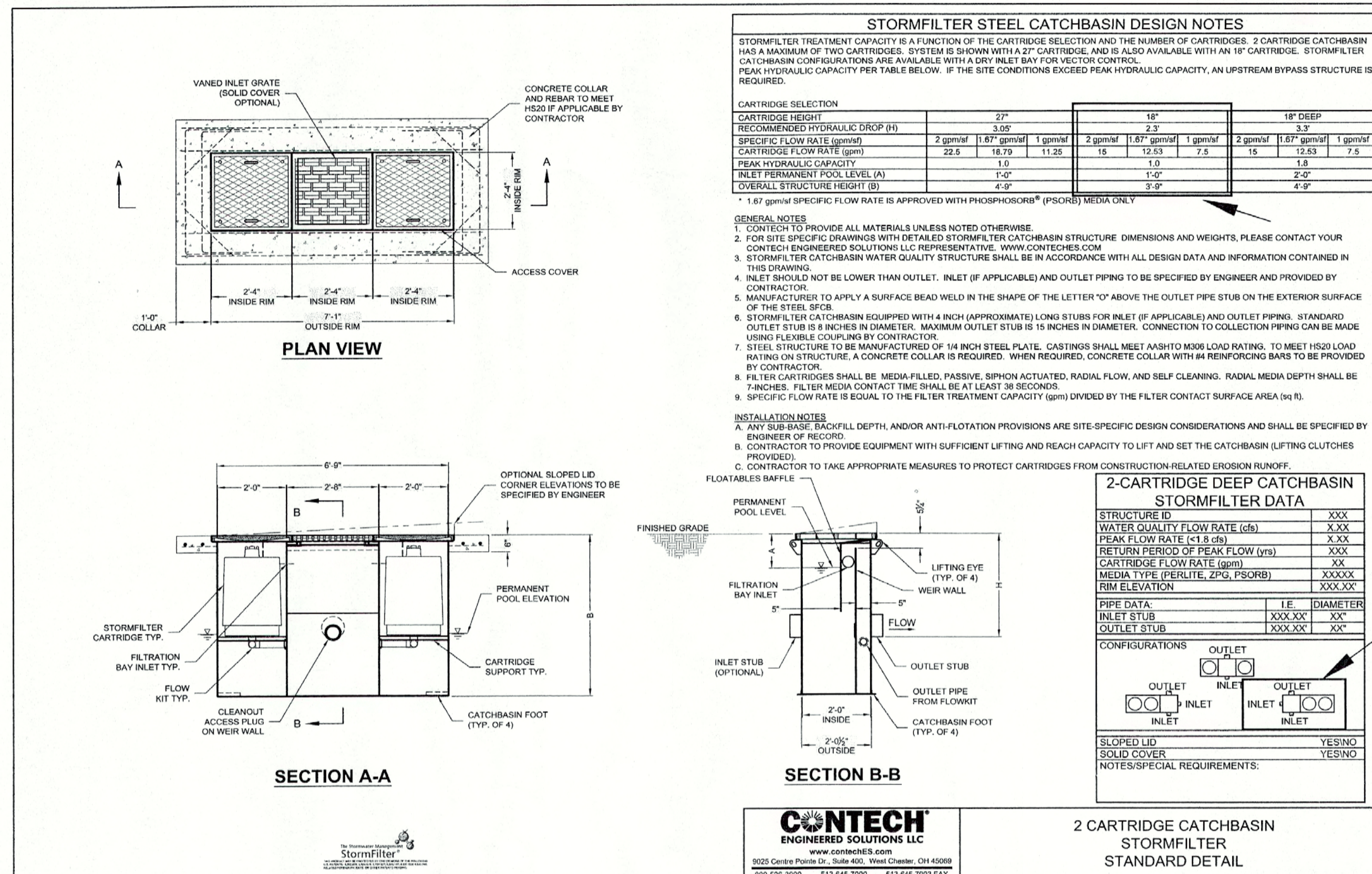
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SITE DEVELOPMENT
SANDY, OREGON 97055

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2-CARTRIDGE DEEP CATCHBASIN STORMFILTER DATA

STRUCTURE ID	XXX
WATER QUALITY FLOW RATE (cfs)	X.XX
PEAK FLOW RATE (cfs)	X.XX
RETURN PERIOD OF PEAK FLOW (yrs)	XXX
CARTRIDGE FLOW RATE (gpm)	XX
MEDIA TYPE (PERLITE, ZPG, PSORB)	XXXXX
RIM ELEVATION	XXX.XX

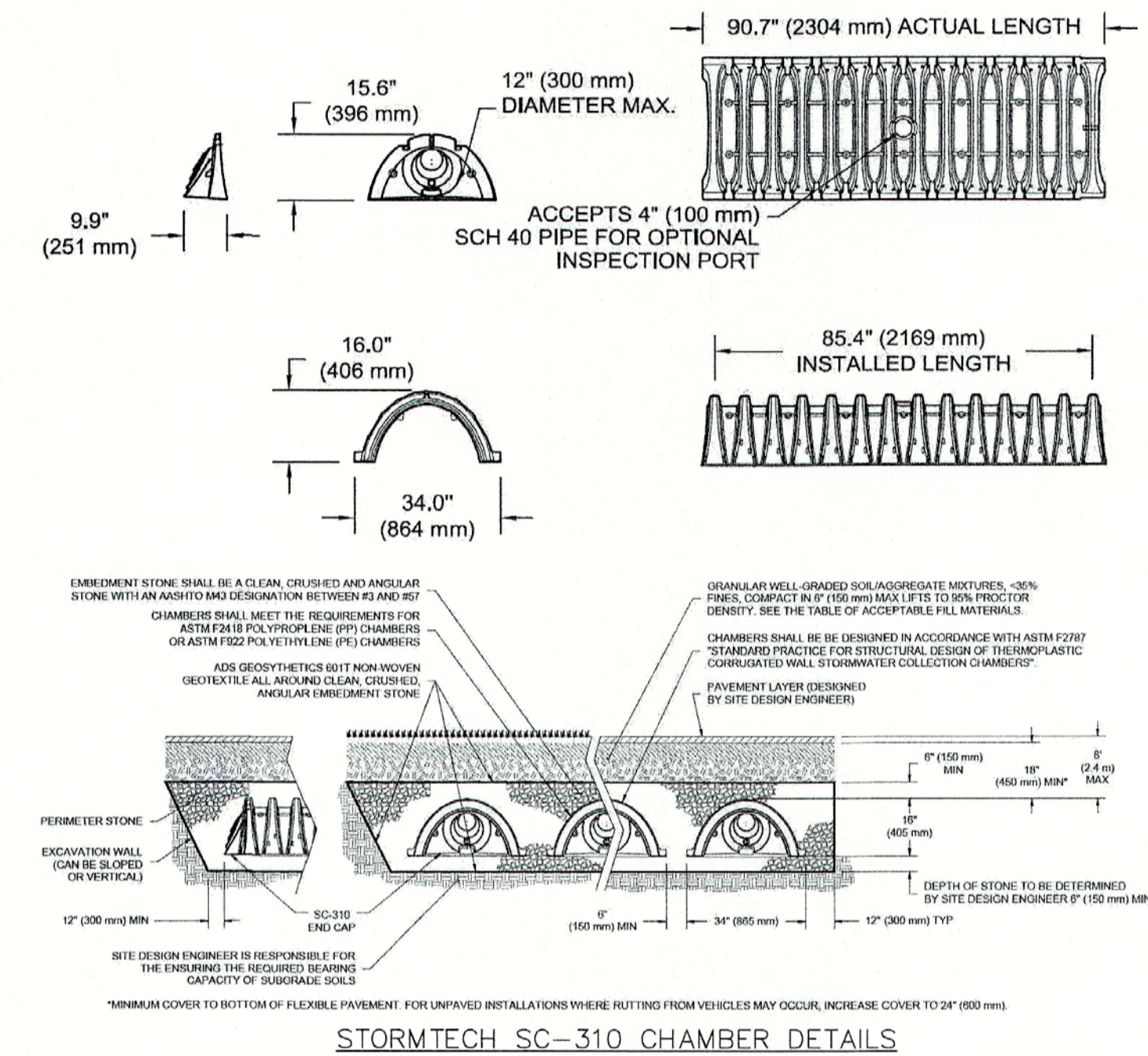
PIPE DATA: INLET STUB: XXX.XX' XX" OUTLET STUB: XXX.XX' XX"

CONFIGURATIONS:

SLOPED LID: YES/NO
SOLID COVER: YES/NO

NOTES/SPECIAL REQUIREMENTS:

2 CARTRIDGE CATCHBASIN STORMFILTER STANDARD DETAIL



STORMTECH SC-310 CHAMBER DETAILS
SCALE: NTS



EXPIRES: 06/30/19
SIGNATURE DATE: _____
PRELIMINARY

DATE	NO.	REVISION

DRAWN:	DESIGNED:	CHECKED:
SCALE: AS SHOWN	DATE: FEB 5, 2019	
PROJECT NO. E17-060		

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DETAILS SHEET - DETENTION FACILITY
SITE DEVELOPMENT
SANDY, OREGON 97055

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EXHIBIT E

PRELIMINARY STORMWATER REPORT

**Fun Time RV Site Improvements
38415 Proctor Blvd
Sandy, OR 97055**

Prepared By:

FDG 
Firwood Design Group, LLC
SURVEYING • ENGINEERING • PLANNING

359 E. Historic Columbia River Highway
Troutdale, OR 97060
503.668.3737- fax 503.668.3788

Preliminary Stormwater Report

FIRWOOD DESIGN GROUP, LLC

STORMWATER CALCULATIONS

**Proposed Site Improvements
City of Sandy, Oregon**

For

Fun Time RV

June 8, 2018

Revised February 6, 2019

Prepared by:

Firwood Design Group, LLC
359 E. Historic Columbia River Highway
Troutdale, OR 97060
(503) 668-3737

FDG # E17-060



Preliminary Stormwater Report

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- I. OBJECTIVE
- II. METHODOLOGY
- III. REFERENCES
- IV. SITE DESCRIPTION
- V. STORMWATER MANAGEMENT

APPENDICES

HydroCAD output

Soil Maps

Preliminary Stormwater Report

STORM DRAINAGE CALCULATIONS

I. OBJECTIVE

The objective is to provide stormwater treatment and detention for the new impervious parking area associated with the proposed site improvements.

Stormwater discharge from the proposed new impervious parking area will be collected, treated in a Contech Stormwater Filter catch basin. The water will be detained in a StormTech chamber system with a flow control manhole. The proposed detention and treatment will conform to the City of Sandy storm water requirements.

II. METHODOLOGY

As per the City of Sandy code, the City of Portland stormwater manual was applied in developing the proposed stormwater management for the impervious surface areas. HydroCAD is used to apply the Santa Barbara unit hydrograph for the respect storm intensities with a 24 hr duration.

For water quality a flow rate evaluation is applied for pollution reduction per Table1-2 in the City of Portland stormwater manual, with storm intensity of 0.2 inches/hr for a 24hr duration.

For detention the proposed underground detention system is designed to have a released rate not to exceed the following:

1. The post construction 24 hour 2 year recurrence interval storm event runoff will not exceed the 2 year pre development 2 year 24 hour runoff
2. The post construction 24 hour 5 year recurrence interval storm event runoff will not exceed the 5 year pre development 5 year 24 hour runoff
3. The post construction 24 hour 10 year recurrence interval storm event runoff will not exceed the 10 year pre development 10 year 24 hour runoff
4. The post construction 24 hour 25 year recurrence interval storm event runoff will not exceed the pre development 25 year runoff.

III. REFERENCES:

USGS Soil Maps for Multnomah County, Oregon
City of Portland, Stormwater Management Manual
City of Sandy Development Code

Preliminary Stormwater Report

IV. SITE DESCRIPTION:

The site is a corner lot situated at parcel located at the northwest intersection of Beers Ave and Proctor Blvd, more specifically located in T2 R4E Sect. 13BC, Lot 3900. The lot is approximately 13,985 sq. ft. in size, with existing topography gently sloping to a low point in the northwesterly corner. Currently, the site is a vacant parcel with grass vegetation. The Soils per the USDA Soils maps are predominately classified as Cazadero Silt Clay loam (15B) with 0 to 7 percent slopes, Hydrologic Group C. Group C soils are generally slow infiltrating when thoroughly wetted, with a slow rate of water transmission.

V. STORMWATER MANAGEMENT:

Water Quality Analysis

The impervious area relating to the new paved area is approximately 12,600 sq. ft. (0.289 acres) in total. The water quality storm event intensity, as required by the City of Sandy, is 0.20 inches / hour. Using the Rational Method $Q = CIA$:

Q = Peak discharge, cfs

C = Rational method runoff coefficient

I = Rainfall intensity, inch/hour

A = Drainage area, acre

$$Q_{\text{water quality}} = (0.98) (0.20) (.289) = 0.057 \text{ cfs, or } 25.6 \text{ gpm}$$

The new paved area will be graded to slope to a low point with a Contech stormfilter catchbasin. The basin will contain two cartridges, with each cartridge capable of treating 15gpm (30 gpm total), sufficient to provide water quality for the design storm event.

Quantity Control Analysis

The Santa Barbara Urban Hydrograph (HydroCAD) was used to create the basin hydrographs (see appendix for data and calculations) and to estimate the peak flows for the design storms. A curve number (CN) value of 98 was assigned to the impervious surfaces and a CN value of 79 (Pasture/Grassy type C soil) was used for pervious areas in both existing and proposed conditions. The time of concentration is 6 minutes, the minimum value.

Preliminary Stormwater Report

The design storms, as required by the City of Sandy design and construction standards, are as follows:

Recurrence Interval (years)	Total Precipitation Depth (In)
2	3.50
5	4.50
10	4.80
25	5.50

Each storm event was modeled for both pre-construction conditions and post-construction conditions. The model includes a StormTech 310 chamber detention system with 300 linear feet of detention chambers installed on top of 6-inches of drain rock. The detention system is unlined, allowing for partial infiltration of detained runoff. The modelled infiltration rate of 0.83 inches/hr is based on the USDA Soil Map published saturated conductivity for Cazedaro Silt Loam. The primary outlet of the detention system is a flow control manhole, designed to regulate flows to pre-development flow rates. This will be accomplished using two orifices, a 2.5" orifice at the pipe invert elevation and a 2.0" orifice at an elevation 0.40' above the pipe invert. For storms larger than the 25-year event the tee riser pipe will act as an overflow. The following table summarizes the calculated flow rates in all conditions:

Pre-Construction and Post Construction Stormwater flows:

Design Storm Event	Pre-Development (Existing) Peak Flow (cfs)	Post-Construction Peak Flow (cfs)	Allowed Post-Construction Runoff	Design Post-Construction Runoff
2 year	0.09	0.25	0.09	0.09
5 year	0.16	0.32	0.16	0.15
10 year	0.17	0.34	0.17	0.16
25 year	0.22	0.39	0.22	0.20

Conclusion

The proposed improvements consisting of paving the existing vacant lot will require stormwater treatment and detention to conform to the City of Sandy development code. The proposed treatment is via a Contech Stormfilter cartridge catch basin that will direct flow to the Stormtech chamber detention system. The unlined detention system will allow some infiltration (limited by site soils) but the majority of the water will be detained and released via a flow control manhole and conveyed to the public stormwater infrastructure on Beers Ave. The facilities have been sized in accordance with the City of Sandy design standards and the methods employed represent industry standard practices.

prelim storm runoff 310chamber

Type IA 24-hr 2yr Rainfall=3.50"

Prepared by HydroCAD SAMPLER 1-800-927-7246 www.hydrocad.net

Printed 2/6/2019

HydroCAD® 10.00-22 Sampler s/n S08125 © 2018 HydroCAD Software Solutions LLC

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Summary for Subcatchment 5S: Fun Time RV-Pre

Runoff = 0.09 cfs @ 8.03 hrs, Volume= 0.042 af, Depth> 1.55"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 2yr Rainfall=3.50"

Area (sf)	CN	Description
13,985	79	Pasture/grassland/range, Fair, HSG C
13,985		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.6	160	0.0400	0.17		Sheet Flow, Grass: Dense n= 0.240 P2= 3.25"

prelim storm runoff 310chamber

Type IA 24-hr 2yr Rainfall=3.50"

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Summary for Subcatchment 7S: Fun Time RV-Post

Runoff = 0.25 cfs @ 7.92 hrs, Volume= 0.083 af, Depth> 3.09"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 2yr Rainfall=3.50"

Area (sf)	CN	Description
12,586	98	Paved parking & roofs
1,399	79	50-75% Grass cover, Fair, HSG C
13,985	96	Weighted Average
1,399		10.00% Pervious Area
12,586		90.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
6.0					Direct Entry,

prelim storm runoff 310chamber

Type IA 24-hr 2yr Rainfall=3.50"

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Summary for Pond 9P: storm tech

Inflow Area = 0.321 ac, 90.00% Impervious, Inflow Depth > 3.09" for 2yr event
 Inflow = 0.25 cfs @ 7.92 hrs, Volume= 0.083 af
 Outflow = 0.12 cfs @ 8.35 hrs, Volume= 0.082 af, Atten= 51%, Lag= 26.0 min
 Discarded = 0.03 cfs @ 4.55 hrs, Volume= 0.055 af
 Primary = 0.09 cfs @ 8.35 hrs, Volume= 0.028 af

Routing by Stor-Ind method, Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Peak Elev= 0.40' @ 8.35 hrs Surf.Area= 1,611 sf Storage= 437 cf

Plug-Flow detention time= 33.6 min calculated for 0.082 af (99% of inflow)
Center-of-Mass det. time= 29.8 min (701.1 - 671.2)

Volume	Invert	Avail. Storage	Storage Description
#1	0.00'	621 cf	ADS_StormTech SC-310 @ 300.00' L Effective Size= 28.9"W x 16.0"H => 2.07 sf x 300.00'L = 621.2 cf Overall Size= 34.0"W x 16.0"H x 7.56'L with 0.44' Overlap
#2	0.00'	195 cf	50.00"W x 19.50'L x 0.50'H Prismaoid 488 cf Overall x 40.0% Voids
#3	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0050 'f
#4	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0500 'f
		885 cf	Total Available Storage

Device	Routing	Invert	Outlet Devices
#1	Primary	0.00'	2.5" Vert. Orifice/Grate C= 0.600
#2	Primary	0.40'	2.0" Vert. Orifice/Grate C= 0.600
#3	Discarded	0.00'	0.830 in/hr Exfiltration over Surface area

Discarded OutFlow Max=0.03 cfs @ 4.55 hrs HW=0.02' (Free Discharge)
└3=Exfiltration (Exfiltration Controls 0.03 cfs)

Primary OutFlow Max=0.09 cfs @ 8.35 hrs HW=0.40' (Free Discharge)
└1=Orifice/Grate (Orifice Controls 0.09 cfs @ 2.61 fps)
└2=Orifice/Grate (Controls 0.00 cfs)

prelim storm runoff 310chamber

Type IA 24-hr 5yr Rainfall=4.50"

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Summary for Subcatchment 5S: Fun Time RV-Pre

Runoff = 0.16 cfs @ 8.02 hrs, Volume= 0.063 af, Depth> 2.36"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 5yr Rainfall=4.50"

Area (sf)	CN	Description
13,985	79	Pasture/grassland/range, Fair, HSG C
13,985		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.6	160	0.0400	0.17		Sheet Flow, Grass: Dense n= 0.240 P2= 3.25"

prelim storm runoff 310chamber

Type IA 24-hr 5yr Rainfall=4.50"

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Summary for Subcatchment 7S: Fun Time RV-Post

Runoff = 0.32 cfs @ 7.92 hrs, Volume= 0.109 af, Depth> 4.07"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 5yr Rainfall=4.50"

Area (sf)	CN	Description
12,586	98	Paved parking & roofs
1,399	79	50-75% Grass cover, Fair, HSG C
13,985	96	Weighted Average
1,399		10.00% Pervious Area
12,586		90.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
6.0					Direct Entry,

prelim storm runoff 310chamber

Type IA 24-hr 5yr Rainfall=4.50"

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Summary for Pond 9P: storm tech

Inflow Area = 0.321 ac, 90.00% Impervious, Inflow Depth > 4.07" for 5yr event
 Inflow = 0.32 cfs @ 7.92 hrs, Volume= 0.109 af
 Outflow = 0.18 cfs @ 8.28 hrs, Volume= 0.108 af, Atten= 45%, Lag= 21.9 min
 Discarded = 0.03 cfs @ 2.95 hrs, Volume= 0.059 af
 Primary = 0.15 cfs @ 8.28 hrs, Volume= 0.049 af

Routing by Stor-Ind method, Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Peak Elev= 0.58' @ 8.28 hrs Surf.Area= 1,545 sf Storage= 593 cf

Plug-Flow detention time= 40.5 min calculated for 0.108 af (99% of inflow)
Center-of-Mass det. time= 36.8 min (702.2 - 665.4)

Volume	Invert	Avail.Storage	Storage Description
#1	0.00'	621 cf	ADS_StormTech SC-310 @ 300.00' L Effective Size= 28.9"W x 16.0"H => 2.07 sf x 300.00'L = 621.2 cf Overall Size= 34.0"W x 16.0"H x 7.56'L with 0.44' Overlap
#2	0.00'	195 cf	50.00"W x 19.50"L x 0.50'H Prismatic 488 cf Overall x 40.0% Voids
#3	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0050 'f
#4	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0500 'f
		885 cf	Total Available Storage

Device	Routing	Invert	Outlet Devices
#1	Primary	0.00'	2.5" Vert. Orifice/Grate C= 0.600
#2	Primary	0.40'	2.0" Vert. Orifice/Grate C= 0.600
#3	Discarded	0.00'	0.830 in/hr Exfiltration over Surface area

Discarded OutFlow Max=0.03 cfs @ 2.95 hrs HW=0.02' (Free Discharge)
↳3=Exfiltration (Exfiltration Controls 0.03 cfs)

Primary OutFlow Max=0.15 cfs @ 8.28 hrs HW=0.58' (Free Discharge)
↳1=Orifice/Grate (Orifice Controls 0.11 cfs @ 3.32 fps)
↳2=Orifice/Grate (Orifice Controls 0.03 cfs @ 1.50 fps)

prelim storm runoff 310chamber

Type IA 24-hr 10yr Rainfall=4.80"

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Summary for Subcatchment 5S: Fun Time RV-Pre

Runoff = 0.17 cfs @ 8.02 hrs, Volume= 0.070 af, Depth> 2.61"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 10yr Rainfall=4.80"

Area (sf)	CN	Description
13,985	79	Pasture/grassland/range, Fair, HSG C
13,985		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.6	160	0.0400	0.17		Sheet Flow, Grass: Dense n= 0.240 P2= 3.25"

prelim storm runoff 310chamber

Type IA 24-hr 10yr Rainfall=4.80"

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Summary for Subcatchment 7S: Fun Time RV-Post

Runoff = 0.34 cfs @ 7.92 hrs, Volume= 0.117 af, Depth> 4.36"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 10yr Rainfall=4.80"

Area (sf)	CN	Description
12,586	98	Paved parking & roofs
1,399	79	50-75% Grass cover, Fair, HSG C
13,985	96	Weighted Average
1,399		10.00% Pervious Area
12,586		90.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
6.0					Direct Entry,

prelim storm runoff 310chamber

Type IA 24-hr 10yr Rainfall=4.80"

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Summary for Pond 9P: storm tech

Inflow Area = 0.321 ac, 90.00% Impervious, Inflow Depth > 4.36" for 10yr event
 Inflow = 0.34 cfs @ 7.92 hrs, Volume= 0.117 af
 Outflow = 0.19 cfs @ 8.27 hrs, Volume= 0.116 af, Atten= 44%, Lag= 21.0 min
 Discarded = 0.03 cfs @ 23.80 hrs, Volume= 0.060 af
 Primary = 0.16 cfs @ 8.27 hrs, Volume= 0.056 af

Routing by Stor-Ind method, Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
 Peak Elev= 0.65' @ 8.27 hrs Surf.Area= 1,517 sf Storage= 634 cf

Plug-Flow detention time= 41.8 min calculated for 0.116 af (100% of inflow)
 Center-of-Mass det. time= 38.0 min (702.0 - 664.0)

Volume	Invert	Avail.Storage	Storage Description
#1	0.00'	621 cf	ADS_StormTech SC-310 @ 300.00' L Effective Size= 28.9"W x 16.0"H => 2.07 sf x 300.00'L = 621.2 cf Overall Size= 34.0"W x 16.0"H x 7.56'L with 0.44' Overlap
#2	0.00'	195 cf	50.00"W x 19.50'L x 0.50'H Prismatic 488 cf Overall x 40.0% Voids
#3	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0050 'f
#4	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0500 'f
		885 cf	Total Available Storage

Device	Routing	Invert	Outlet Devices
#1	Primary	0.00'	2.5" Vert. Orifice/Grate C= 0.600
#2	Primary	0.40'	2.0" Vert. Orifice/Grate C= 0.600
#3	Discarded	0.00'	0.830 in/hr Exfiltration over Surface area

Discarded OutFlow Max=0.03 cfs @ 23.80 hrs HW=0.02' (Free Discharge)
 ↑3=Exfiltration (Exfiltration Controls 0.03 cfs)

Primary OutFlow Max=0.16 cfs @ 8.27 hrs HW=0.65' (Free Discharge)
 ↑1=Orifice/Grate (Orifice Controls 0.12 cfs @ 3.55 fps)
 ↓2=Orifice/Grate (Orifice Controls 0.04 cfs @ 1.96 fps)

prelim storm runoff 310chamber

Type IA 24-hr 25yr Rainfall=5.50"

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Summary for Subcatchment 5S: Fun Time RV-Pre

Runoff = 0.22 cfs @ 8.02 hrs, Volume= 0.086 af, Depth> 3.21"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 25yr Rainfall=5.50"

Area (sf)	CN	Description
13,985	79	Pasture/grassland/range, Fair, HSG C
13,985		100.00% Pervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
15.6	160	0.0400	0.17		Sheet Flow, Grass: Dense n= 0.240 P2= 3.25"

prelim storm runoff 310chamber

Type IA 24-hr 25yr Rainfall=5.50"

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Summary for Subcatchment 7S: Fun Time RV-Post

Runoff = 0.40 cfs @ 7.92 hrs, Volume= 0.135 af, Depth> 5.05"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Type IA 24-hr 25yr Rainfall=5.50"

Area (sf)	CN	Description
12,586	98	Paved parking & roofs
1,399	79	50-75% Grass cover, Fair, HSG C
13,985	96	Weighted Average
1,399		10.00% Pervious Area
12,586		90.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
6.0					Direct Entry,

prelim storm runoff 310chamber

Type IA 24-hr 25yr Rainfall=5.50"

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Summary for Pond 9P: storm tech

Inflow Area = 0.321 ac, 90.00% Impervious, Inflow Depth > 5.05" for 25yr event
 Inflow = 0.40 cfs @ 7.92 hrs, Volume= 0.135 af
 Outflow = 0.23 cfs @ 8.25 hrs, Volume= 0.134 af, Atten= 42%, Lag= 20.0 min
 Discarded = 0.03 cfs @ 2.05 hrs, Volume= 0.060 af
 Primary = 0.20 cfs @ 8.25 hrs, Volume= 0.074 af

Routing by Stor-Ind method, Time Span= 0.00-24.00 hrs, dt= 0.05 hrs
Peak Elev= 0.83' @ 8.25 hrs Surf.Area= 1,417 sf Storage= 735 cf

Plug-Flow detention time= 44.5 min calculated for 0.134 af (99% of inflow)
Center-of-Mass det. time= 37.5 min (698.7 - 661.2)

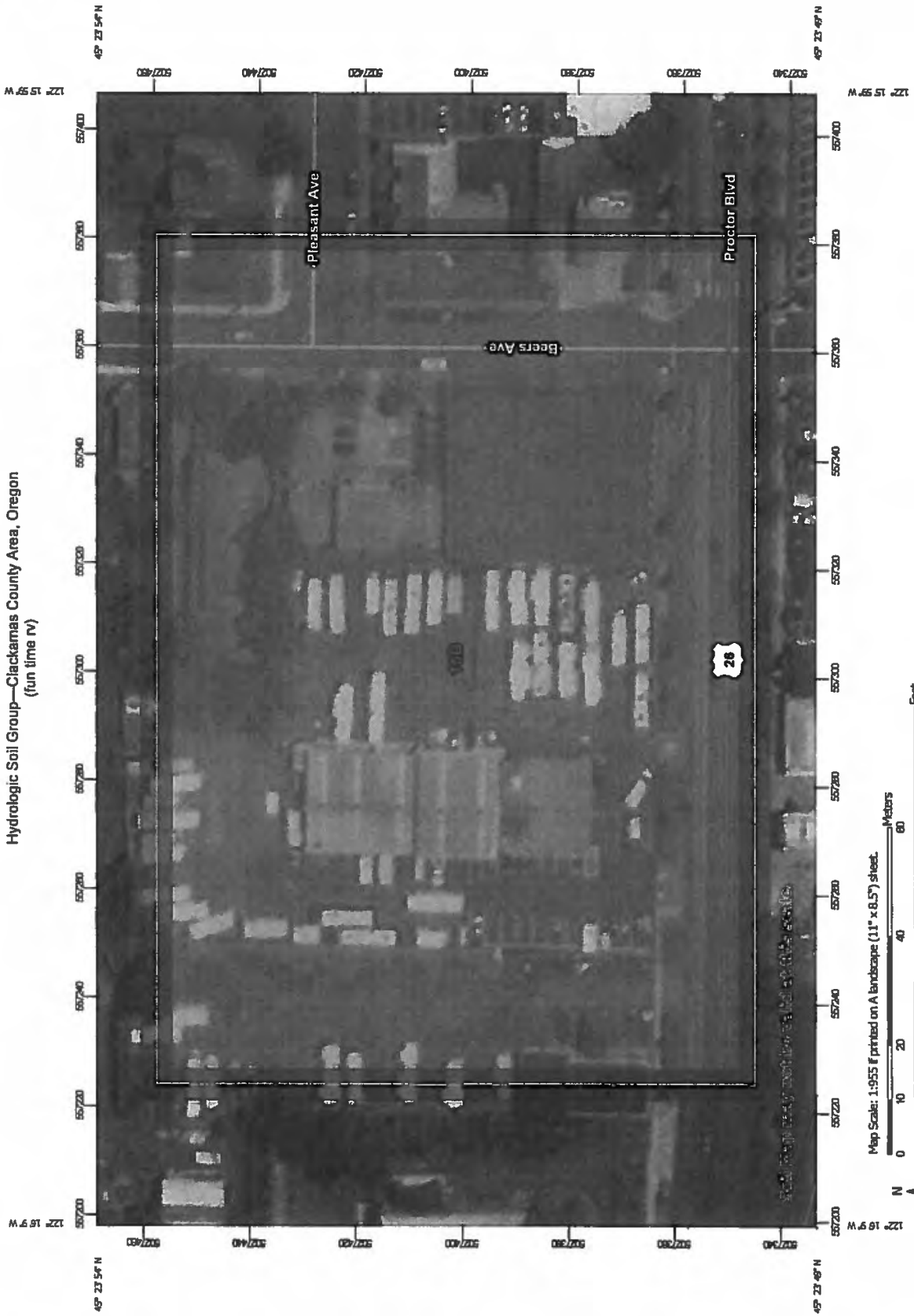
Volume	Invert	Avail.Storage	Storage Description
#1	0.00'	621 cf	ADS_StormTech SC-310 @ 300.00' L Effective Size= 28.9"W x 16.0"H => 2.07 sf x 300.00'L = 621.2 cf Overall Size= 34.0"W x 16.0"H x 7.56'L with 0.44' Overlap
#2	0.00'	195 cf	50.00"W x 19.50'L x 0.50'H Prismaoid 488 cf Overall x 40.0% Voids
#3	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0050 'f
#4	0.00'	34 cf	18.0" Round Pipe Storage -Impervious L= 19.5' S= 0.0500 'f
885 cf			Total Available Storage

Device	Routing	Invert	Outlet Devices
#1	Primary	0.00'	2.5" Vert. Orifice/Grate C= 0.600
#2	Primary	0.40'	2.0" Vert. Orifice/Grate C= 0.600
#3	Discarded	0.00'	0.830 in/hr Exfiltration over Surface area

Discarded OutFlow Max=0.03 cfs @ 2.05 hrs HW=0.03' (Free Discharge)
↳3=Exfiltration (Exfiltration Controls 0.03 cfs)

Primary OutFlow Max=0.20 cfs @ 8.25 hrs HW=0.83' (Free Discharge)
↳1=Orifice/Grate (Orifice Controls 0.14 cfs @ 4.12 fps)
↳2=Orifice/Grate (Orifice Controls 0.06 cfs @ 2.85 fps)

Hydrologic Soil Group—Clackamas County Area, Oregon
(run time rv)



Map Scale: 1:955 if printed on A landscape (11" x 8.5") sheet.

Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 10N WGS84

USDA
Natural Resources
Conservation Service

Web Soil Survey
National Cooperative Soil Survey

4/24/2018
Page 1 of 4

MAP LEGEND

- Area of Interest (AOI)
 - Area of Interest (AOI)
- Soils
 - A
 - A/D
 - B
 - B/D
 - C
 - C/D
 - D
 - Not rated or not available
- Water Features
 - Streams and Canals
- Transportation
 - Ralls
 - Interstate Highways
 - US Routes
 - Major Roads
 - Local Roads
- Background
 - Aerial Photography
- Soil Rating Lines
 - A
 - A/D
 - B
 - B/D
 - C
 - C/D
 - D
 - Not rated or not available
- Soil Rating Points
 - A
 - A/D
 - B
 - B/D

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:20,000.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service
 Web Soil Survey URL:
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distances or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Clackamas County Area, Oregon
 Survey Area Data: Version 12, Sep 19, 2017

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jul 2, 2015—Sep 21, 2016

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
15B	Cazadero silty clay loam, 0 to 7 percent slopes	C	4.4	100.0%
Totals for Area of Interest			4.4	100.0%

Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

Rating Options

Aggregation Method: Dominant Condition

Component Percent Cutoff: None Specified

Tie-break Rule: Higher

EXHIBIT F

Rev. 062718

KH45Q DuraLED Large Hampton Kitty Hawk Area, Wall and Flood Light

DuraGuard
PRODUCTS INC.
A QSSI Company



Project Information	
Project Name:	Fixture Type:
Complete Catalog #:	Date:
Comments:	

The DuraGuard KH45Q luminaire is available in two wattages with a wide choice of mounting configurations and optical distributions designed to replace HID lighting systems from 250w to 1000w MH or HPS. Typical applications include general area parking, flood, security and accent lighting for retail centers, industrial parks, schools and universities, public transit and airports, office buildings and medical facilities. Mounting heights of 16 to 35 feet can be used based on light level and uniformity requirements.

SPECIFICATIONS AND FEATURES:

HOUSING:

Die Cast Aluminum Housing and Front Frame
Integral Heat Sinking and Driver Compartment.
Photocell Adaptable, Nickel-Plated Stainless Steel Hardware

LISTING & RATINGS:

CSA. Listed for Wet Locations, ANSI/UL 1598, 6750
IP66 Sealed LED Compartment.

FINISH:

Textured Architectural Bronze Powdercoat Finish
Over a Chromate Conversion Coating. Custom Colors Available Upon Request.

LENS:

Tempered Clear Flat, SoftLED Flat Glass Lens, or
Tempered Clear Flat Prismatic Glass Lens.

MOUNTING OPTIONS:

Mounts with Kitty Hawk Arm, Adjustable Slipfitter, 2 1/2" Diameter Mounting Arm Adapter, Yoke, Two-Piece Bracket or Wall Mount Bracket. (Factory Installed)

DURALED LED:

Aluminum Boards

WATTAGE:

167w: Array: 167.3w, System: 177w
(250-400w HID Equivalent)
256w: Array: 255.7w, System: 281w
(400-1000w HID Equivalent)

DRIVER:

Electronic Driver, 120-277V, 50/60Hz or 347-480V,
50/60Hz; Less Than 20% THD and PF>0.90 Standard
Internal Surge Protection 6KV, 0-10V Dimming Standard
for a Dimming Range of 100% to 10%; Dimming Source
Current is 150 Microamps.

CONTROLS:

Fixtures Ordered with Factory-Installed Photocell or Motion
Sensor Controls are Internally Wired for Switching and/or
1-10V Dimming Within the Housing. Remote Direct Wired
Interface of 1-10V Dimming is Not Implied and May Not Be
Available. Please Consult Factory. Fixtures are Tested with
DuraGuard Controls and May Not Function Properly With
Controls Supplied by Others. Fixtures are NOT Designed
for Use with Line Voltage Dimmers.

WARRANTY:

5-Year Warranty for -40°C to +50°C Environment.

See Page 4 for Projected Lumen Maintenance Table.



Shown with "KH45A" Kitty Hawk Arm Mounting Option.



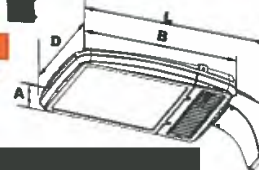
Shown with "WM" Wall Mount Bracket Option.



Shown with "SSF" Slipfitter Mounting Option.

Dimensions

Width (D) 15 1/4" (400mm)
Length (B) 22" (559mm)
Height (A) 4" (102mm)



Length with Mount (L)

Kitty Hawk Arm (KH45A): 27 1/2" (699mm)
Slipfitter (SSF): 29 1/4" (743mm)
Mounting Arm Adapter (ALMAAA): 26 1/4" (664mm)
Yoke (KH45Y): 26 1/4" (667mm)
Bracket (SSB): 25 1/4" (654mm)

DuraLED TECHNOLOGY

Complete Units
Ordering Information
Example: KH45QF1X256U5KCZSSFSF

KH45Q DuraLED Large Hampton Kitty Hawk Area, Wall and Flood Light

Model	Optics/Beams	Wattage	Driver	CCT	Lens	Color	Mounting	Options
KH45Q								
KH45Q =Large Hampton Kitty Hawk Area, Wall and Flood Light	A=Type I/NEMA 7H x 5V B=Type II/NEMA 7H x 7V C=Type III/NEMA 7H x 7V D=Type IV/NEMA 7H x 6V F=Type V/NEMA 7H x 7V I=Narrow Beam/NEMA 4H x 4V *Use with 5K Model Only.	1X167=167w 1X256=256w	U=120-277V H=347-480V	3K=3000K* 4K=4000K 5K=5000K	*Only for C & F Optics. C=Standard Clear Flat Glass Lens S=SoftLED Flat Glass Lens P=Clear Flat Prismatic Glass Lens* *Use with Type V Optic Only.	Z=Bronze C=Custom (Consult Factory) KH45A=Kitty Hawk Arm SSF= Slipfitter ALMAA= Mounting Arm Adapter KH45Y=Yoke SSB=Two-Piece Swivel Bracket WM=Wall Mount Bracket NM=No Mount		SF=Single Fuse DF=Double Fuse SP=Surge Protection M1=Motion Sensor, IR for mounting heights of 20ft to 35ft M2=Motion Sensor, IR for mounting heights of 18ft or less R3=3-Pin Twist Lock Photocell Receptacle R7=7-Pin ANSI C136.41—2013 Twist Lock Photocell Receptacle S23=Internal Microwave Sensor with Dimming for mounting heights of 35 or less



DesignLights Consortium™
Qualified Luminaires:
KH45QA1X[167 256][U H]5KC***
KH45QB1X[167 256][U H]5KC***
KH45QC1X[167 256][U H]5KC***
KH45QD1X[167 256][U H]5KC***
KH45QE1X[167 256][U H]5KC***



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KH45Q DuraLED Large Hampton Kitty Hawk Area, Wall and Flood Light



MOUNTING OPTIONS:



KH45Q Shown with "KH45A" Kitty Hawk Arm, Kitty Hawk Die-Cast Mounting Arm, Bronze Powdercoat Finish, Includes Hardware. Mounts Directly to Square Poles.



KH45Q Shown with "ALMAA" Pole Mounting Arm Adaptor, Die-Cast Adaptor for 2 1/2" Horizontal Mounting Arms, Bronze Powdercoat Finish, Includes Hardware



KH45Q Shown with "KH45Y" Yoke, Stamped Heavy-Duty Steel Yoke, Bronze Powdercoat Finish, Includes Hardware.



KH45Q Shown with "SSF" External Mount Slipfitter, External Mount Die-Cast Adjustable Slipfitter for 2 1/2" Tenons, Bronze Powdercoat Finish, Includes Hardware.





KH45Q Shown with "SSB" Two-Piece Swivel Bracket, Two-Piece Stamped Steel Adjustable Bracket, Bronze Powdercoat Finish, Includes Hardware.






KH45Q Shown with "WM" Wall Mount Bracket, Wall Bracket, Heavy-Duty Stamped Steel, Bronze Powdercoat Finish, Includes Hardware.

EPA (EFFECTIVE PROJECTED AREA) SHOWN WITH LARGE KITTY HAWK ARM MOUNTING

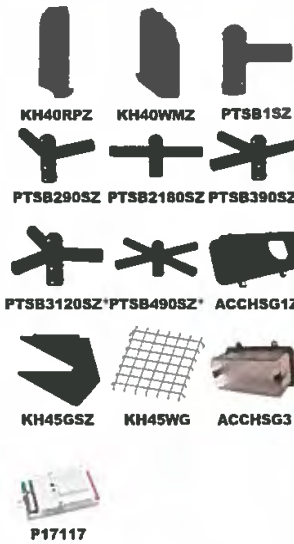
Configuration	EPA (Sq. Ft.)	Weight (Lbs.)
 1	1.00	33 Lbs

Configuration	EPA (Sq. Ft.)	Weight (Lbs.)
 2Q180° Mount	2.00	66 Lbs
 2Q90° Mount	1.36	66 Lbs


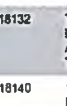
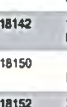
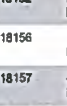








Configuration	EPA (Sq. Ft.)	Weight (Lbs.)
 3Q90° Mount	2.20	99 Lbs
 3Q120° Mount	2.00	99 Lbs

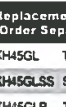
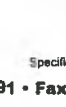

Configuration	EPA (Sq. Ft.)	Weight (Lbs.)
 4Q90° Mount	2.20	132 Lbs

ACCESSORIES & REPLACEMENT PARTS:



Mounting Accessories (Order Separately, Field Installed)	
	KH40RPZ Die-Cast Adaptor for 4" to 6" Round Poles, Bronze Powdercoat Finish. Use with KH45A Kitty Hawk Mounting Arm only.
	KH40WMZ Die-Cast Wall Mount Adaptor, Bronze Powdercoat Finish. Use with KH45A Kitty Hawk Mounting Arm only.
	PTS81S2 Single Pole Tenon Spoke Bracket, 2 1/2" Horizontal Mounting Arm, Bronze Powdercoat Finish, Includes Hardware.
	PTS8290S2 Twin Pole Tenon Spoke Bracket, 2 1/2" Horizontal 90° Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.
	PTS82180S2 Twin Pole Tenon Spoke Bracket, 2 1/2" Horizontal 180° Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.
	PTS8390S2 Triple Pole Tenon Spoke Bracket, 2 1/2" Horizontal 90° Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.
	PTS83120S2 Triple Pole Tenon Spoke Bracket, 2 1/2" Horizontal 120° Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.
	PTS8490S2 Quad Pole Tenon Spoke Bracket, 2 1/2" Horizontal 90° Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.

Accessories (Order Separately, Field Installed)	
	P18131 Twist Lock Non-Shorting (Open) Cap Disconnects Service to Fixture for Temporary or Permanent Disabling (Fixture Always Off), IP65, 480V Maximum.
	P18132 Twist Lock Shorting Cap Provides Fixed Service to Fixture (Fixture Always on), IP65, Rated Load 7200w Tungsten.
	P18140 110-120VAC Instant Twist Lock Photocell
	P18142 105-287VAC Instant Twist Lock Photocell
	P18150 120VAC Time Delay Twist Lock Photocell
	P18152 277VAC Time Delay Twist Lock Photocell
	P18156 120-277VAC Universal Twist Lock Photocell
	P18157 480VAC Time Delay Twist Lock Photocell, For 480V use only.
	ACCHSG12 Die Cast Accessory Housing for Installing Sensors or Other Lighting Controls, Bronze Powdercoat Finish
	KH45GSZ Aluminum Glass/House Side Shield, Bronze Powdercoat Finish, Includes Hardware.
	KH45WG Wire Guard, Stainless Steel Construction
	ACCHSG3 Bronze Polycarbonate Housing for Installing Sensors, Includes Sensor Bracket and Gasket.

Replacement Parts (Order Separately, Field Installed)	
	KH45GL Tempered Clear Flat Glass Lens.
	KH45GLSS SoLED Flat Glass Lens
	KH45GLP Tempered Prismatic Flat Glass Lens.
	KH45AZ Kitty Hawk Die-Cast Mounting Arm, Bronze Powdercoat Finish, Includes Hardware. Mounts Directly to Square Poles.
	SSFZ External Mount Die-Cast Adjustable Slipfitter for 2 1/2" Tenons, Bronze Powdercoat Finish, Includes Hardware.
	ALMAAZ Die-Cast Adaptor for 2 1/2" Horizontal Mounting Arms, Bronze Powdercoat Finish, Includes Hardware.
	KH45YZ Stamped Heavy-Duty Steel Yoke, Bronze Powdercoat Finish, Includes Hardware.
	SS8Z Two-Piece Stamped Steel Adjustable Bracket, Bronze Powdercoat Finish, Includes Hardware.
	WBR2Z Wall Bracket, Heavy-Duty Stamped Steel, Bronze Powdercoat Finish, Includes Hardware.
	P17117 Internal Microwave Sensor with Dimming, 120-277VAC, 50/60Hz. See P17117 Specification Page for Details.

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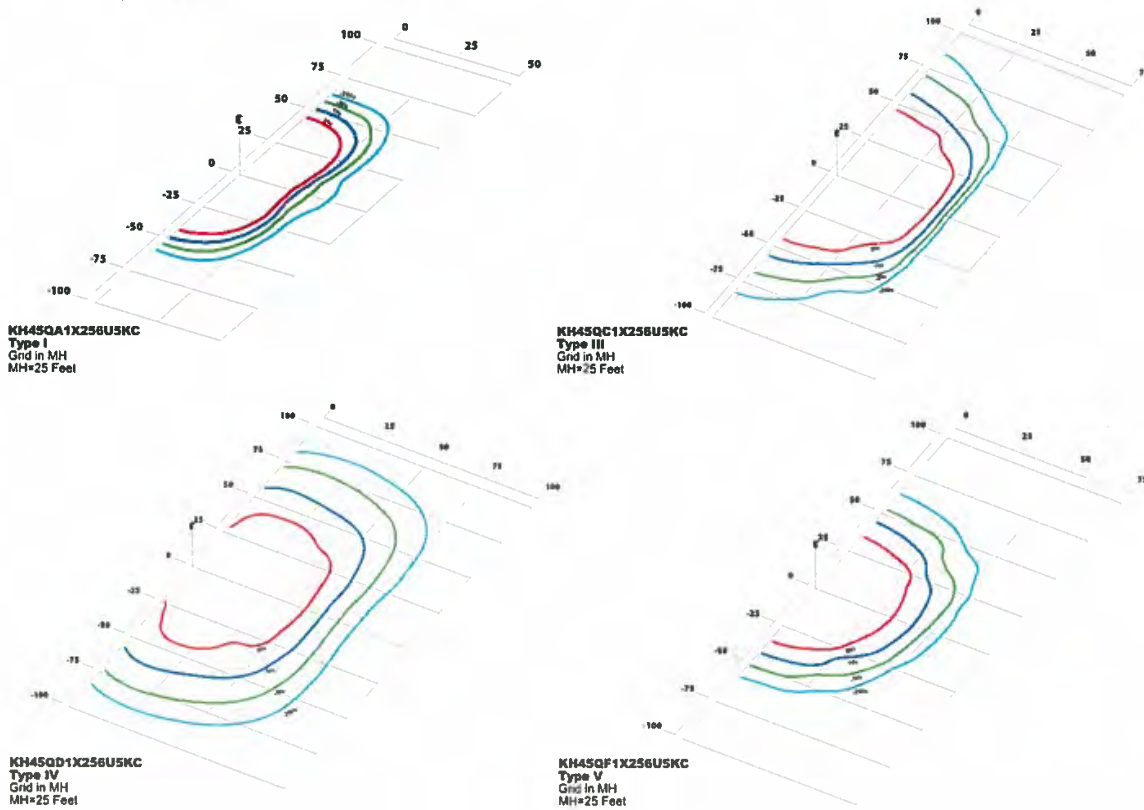
KH45Q DuraLED Large Hampton Kitty Hawk Area, Wall and Flood Light



PHOTOMETRIC PERFORMANCE

LED Board Watts	Drive Current (mA)	Input Watts	Optics	5000 CCT 80 CRI					4000 CCT 80 CRI				
				Lumens	LPW	B	U	G	Lumens	LPW	B	U	G
DuraLED 167w	525	177	A Type I	20,821	118	4	0	3	19,416	109	4	0	2
			B Type II	19,863	111	3	0	3	18,522	104	3	0	2
			C Type III	21,820	122	3	0	3	20,347	114	3	0	3
			D Type IV	22,118	124	3	1	4	20,625	115	3	1	4
			F Type V	20,814	116	4	0	1	19,409	109	4	0	1
DuraLED 256w	525	281	A Type I	31,918	116	5	0	3	29,763	108	5	0	3
			B Type II	30,449	111	3	0	3	28,393	103	3	0	3
			C Type III	33,449	122	4	0	4	31,191	114	4	0	4
			D Type IV	27,600	100	3	0	4	25,737	94	3	0	4
			F Type V	31,907	116	5	0	2	29,753	108	5	0	2

PHOTOMETRIC DATA



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KH45Q DuraLED Large Hampton Kitty Hawk Area, Wall and Flood Light



PHOTOMETRIC PERFORMANCE

LED Board Watts	Drive Current (mA)	Input Watts	Beam	5000 CCT 80 CRI		4000 CCT 80 CRI	
				Lumens	LPW	Lumens	LPW
DuraLED 167w	525	177	A 130°H x 70°V, NEMA 7H x 5V	20,807	116	19,403	109
			B 110°H x 30°V, NEMA 7H x 6V	19,889	111	18,547	104
			C 120°H x 70°V, NEMA 7H x 7V	21,664	121	20,201	113
			D 110°H x 30°V, NEMA 6H x 5V	21,875	122	20,399	114
			F 115°H x 110°V, NEMA 7H x 7V	20,867	117	19,459	109
			I 30°H x 30°V, NEMA 4H x 4V	20,409	115	-	-
DuraLED 256w	525	281	A 130°H x 70°V, NEMA 7H x 5V	31,896	116	29,743	108
			B 110°H x 30°V, NEMA 7H x 6V	30,489	111	28,431	104
			C 120°H x 70°V, NEMA 7H x 7V	33,209	121	30,968	113
			D 110°H x 50°V, NEMA 7H x 6V	27,299	99	25,456	93
			F 115°H x 110°V, NEMA 7H x 7V	31,988	116	29,829	109
			I 30°H x 30°V, NEMA 4H x 4V	31,286	111	-	-

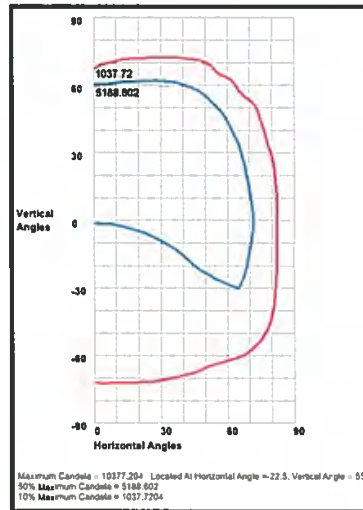
PROJECTED LUMEN MAINTENANCE

Data shown for 5000 CCT		Compare to MH				
TM-21-11	Input Watts	Initial	25,000 Hrs	50,000 Hrs	100,000 Hrs	Calculated L70 @ 25°C
L70 Lumen Maintenance @ 25°C / 77°F	177	1.00	0.98	0.96	0.92	393,000
L70 Lumen Maintenance @ 25°C / 77°F	281	1.00	0.97	0.95	0.90	290,000
TM-21-11	Input Watts	Initial	25,000 Hrs	50,000 Hrs	100,000 Hrs	Calculated L70 @ 50°C
L70 Lumen Maintenance @ 50°C / 122°F	177	1.00	0.96	0.91	0.82	114,000
L70 Lumen Maintenance @ 50°C / 122°F	281	1.00	0.94	0.88	0.76	84,000
TM-21-11	Input Watts	Initial	25,000 Hrs	50,000 Hrs	100,000 Hrs	Calculated L80 @ 40°C
L80 Lumen Maintenance @ 40°C / 104°F	177	1.00	0.97	0.94	0.89	180,000
L80 Lumen Maintenance @ 40°C / 104°F	281	1.00	0.95	0.91	0.81	108,000

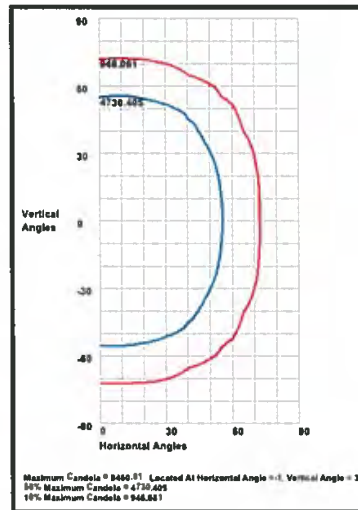
NOTES:

1. Projected per IESNA TM-21-11. Data references the extrapolated performance projections for the 525mA base model in a 25°C ambient, based on 10,000 hours of LED testing per IESNA LM-80-08.
2. Compare to MH box indicates suggested Light Loss Factor (LLF) to be used when comparing to Metal Halide (MH) systems.

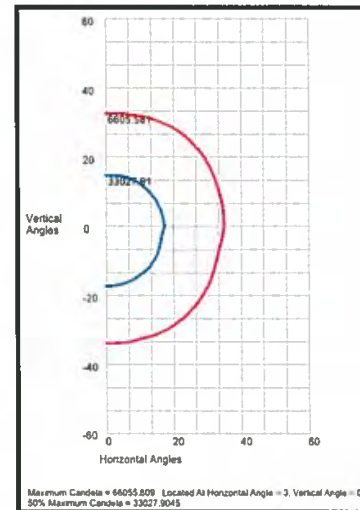
PHOTOMETRIC DATA



KH45QC1X256U5KC
120°H x 70°V, NEMA 7H x 7V



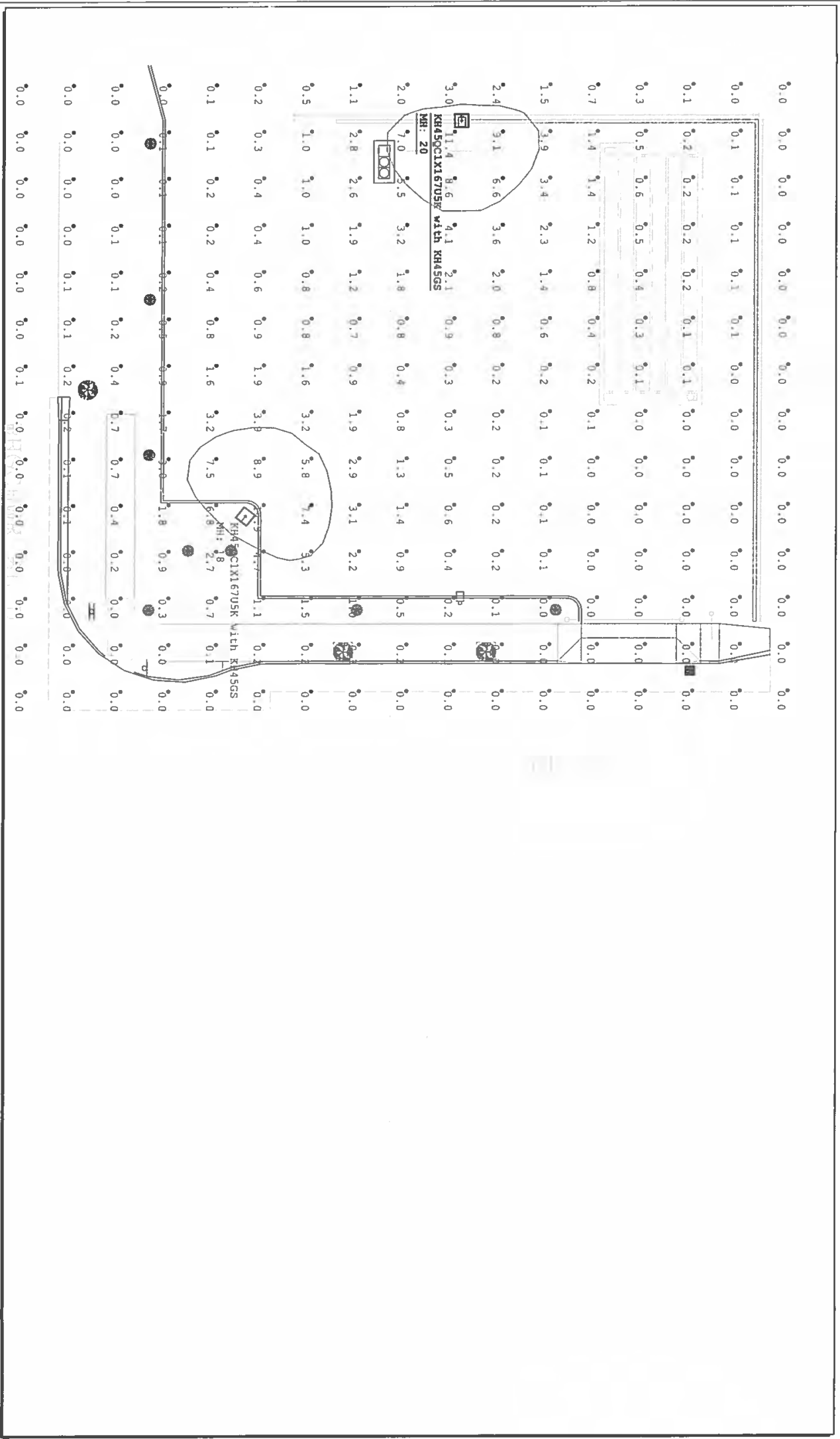
KH45QF1X256U5KC
115°H x 110°V, NEMA 7H x 7V



KH45QI1X256U5KC
30°H x 30°V, NEMA 4H x 4V

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Calculation Summary						
Label	CalcType	Units	Avg	Max	Min	Avg/Min
PARKING AND PROPERTY	Illuminance	Fc	1.00	14.9	0.0	N.A.

Luminaire Schedule						
Symbol	Qty	Label	Arrangement	LLF	Description	Arr. Watts
☐	2	KH45QC1X167U5K with KH45GS	SINGLE	0.900	KH45QC1X167U5K with KH45GS	181.4
						Lum. Watts
						181.4

Page 1 of 2	Fun Time RV	Drawn By:	
	L-1 OUTDOOR LIGHTING	Checked By: JT	
		Date: 2/26/2019	
		Scale:	

#	Date	Comments

DuraGuard
PRODUCTS INC.
A QSSI Company

EXHIBIT G



James Cramer

Thu, Jan 3, 9:14 AM (9 days ago)

to Jerry, me

Tracy,

Thank you for your patience as this project presents some unique elements when determining use and design regulations. Based on our initial review, our follow-up meeting with the property owner on November 21, 2018 and discussion with our City attorney the following explanations are in response to the letter received December 10, 2018:

1. Staff acknowledges the applicant's intent to apply for a minor conditional use for outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area. Staff would like to point out the intent of the C-1 Zone "district is not intended for intensive automobile or industrial uses" (Section 17.42.00).
2. Staff agrees with the applicant's interpretation and deviations will not be required to Section 17.90.00. Staff would like to point out that a reviewing body may deny, or require modifications to, a project with any of the items 1-11 identified in Subsection 17.90.00(D) of which the items identified (items 3, 10 & 11) are located. The C-1 zone district is intended to provide the principal focus for civil and social functions within the community. Based on the intent of the C-1 zone, the proposed use, and guidelines established by the code it becomes difficult for staff to support the proposal unless measurements are taken to support the pedestrian environment.
3. This project is unique as the intent of obtaining the required minor conditional use is to display RVs for sale which logically requires them to be parked off-street on a paved surface. However, after further review staff agrees with the applicant's interpretation that the proposed minor conditional use is for outdoor display of merchandise and a special variance is not required for off-street parking.

4. Subsection 17.42.10(B) states commercial recreational vehicle sale is an outright use associated “in buildings with up to 30,000 square feet”. Based on the language as written, a structure shall be associated with each commercial use identified in Subsection 17.42.10(B). The proposed minor conditional use of an “outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area” requires a building with retail sales area to determine the percentage of outdoor product display and storage, further supporting staff’s interpretation. This project is unique in that the applicant seeks a minor conditional use to display RVs, however the applicant does not propose a building with retail space on the subject vacant lot of record. After speaking with the City attorney staff is comfortable moving forward with this property’s conditional use being associated with the neighboring property to the west (i.e. the existing Fun Time RV site) as it is under the same ownership; however, this will lead to an increase in non-conformity as no structure is present on the subject lot of record. If the applicant requests the special variance to Subsection 17.90.110(D)(1) for the subject lot of record then staff is comfortable reviewing the subject property on its own (i.e. not including the existing Fun Time RV site) for code compliance.

Should the applicant not wish to apply for a variance from Subsection 17.90.110(D)(1), staff would have to look at the property as a whole (i.e. contiguous) with connection to the property to the west (the existing Fun Time RV site), and apply the existing development standards to the entire site.

5. Due to the interpretation identified in No. 3 above the previously mentioned special variance to Subsection 17.98.120(A) will not be required. Staff would like to point out that Subsection 17.90.100(J)(2) states that outdoor storage must be screened from view from public rights of way and civic spaces at least eight (8) feet. The screening requirements of Subsection 17.92.90 will apply to the property.
6. Due to the interpretation identified in No. 3 above the previously mentioned special variance to Subsection 17.98.120(C) will not be required. Staff would like to point out that by incorporating the 10% landscaping requirements of Subsection 17.98.120(C) it would bring the property more in compliance with the guidelines of Subsection 17.90.00(D) mentioned in No. 2 above.
7. Due to the interpretation identified in No. 3 above the previously mentioned special variance to Subsection 17.98.120(F) will not be required. Incorporating the standard of Subsection 17.98.120(F) will help ensure the

landscaping is not negatively affected by the outdoor display of merchandise (RVs) and the expectations of Chapter 17.92 are met.

Please take some time to review staff's response and let me know if you have any questions.

Tracy Brown <tbrownplan@gmail.com>

Jan 3, 2019, 12:52 PM (9 days ago)
to James, Jerry

Hi, I appreciate your response and clarification. In an effort to make sure that we know exactly what is expected so there will be no surprises later, I have highlighted a few items and have questions for clarification noted in blue below. As you know time is of the essence so I hope to receive your response to my questions as soon as possible so we can prepare the application package. Thanks, Tracy

1. Staff acknowledges the applicant's intent to apply for a minor conditional use for outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area. Staff would like to point out the intent of the C-1 Zone "district is not intended for intensive automobile or industrial uses" (Section 17.42.00).
2. Staff agrees with the applicant's interpretation and deviations will not be required to Section 17.90.00. Staff would like to point out that a reviewing body may deny, or require modifications to, a project with any of the items 1-11 identified in Subsection 17.90.00(D) of which the items identified (items 3, 10 & 11) are located. The C-1 zone district is intended to provide the principal focus for civil and social functions within the community. Based on the intent of the C-1 zone, the proposed use, and guidelines established by the code it becomes difficult for staff to support the proposal unless measurements are taken to support the pedestrian environment. I am confused by this last sentence. We intend to build required sidewalk public improvements along the site. What other "measurements" "to support the pedestrian environment" are you referring to?
3. This project is unique as the intent of obtaining the required minor conditional use is to display RVs for sale which logically requires them to be parked off-street on a paved surface. However, after further review staff agrees with the applicant's interpretation that the proposed minor conditional use is for outdoor display of merchandise and a special variance is not required for off-street parking.
4. Subsection 17.42.10(B) states commercial recreational vehicle sale is an outright use associated "in buildings with up to 30,000 square feet". Based on the language as written, a structure shall be associated with each

commercial use identified in Subsection 17.42.10(B). The proposed minor conditional use of an “outdoor product display or storage of merchandise covering greater than 10% of the total retail sales area” requires a building with retail sales area to determine the percentage of outdoor product display and storage, further supporting staff’s interpretation. This project is unique in that the applicant seeks a minor conditional use to display RVs, however the applicant does not propose a building with retail space on the subject vacant lot of record. After speaking with the City attorney staff is comfortable moving forward with this property’s conditional use being associated with the neighboring property to the west (i.e. the existing Fun Time RV site) as it is under the same ownership; however, this will lead to an **increase in non-conformity** as no structure is present on the subject lot of record. If the applicant requests the special variance to Subsection 17.90.110(D)(1) for the subject lot of record then staff is comfortable reviewing the subject property on its own (i.e. not including the existing Fun Time RV site) for code compliance.

Should the applicant not wish to apply for a variance from Subsection 17.90.110(D)(1), staff would have to look at the property as a whole (i.e. contiguous) with connection to the property to the west (the existing Fun Time RV site), and **apply the existing development standards to the entire site.**

We are evaluating the best course of action and whether to request a special variance to this section or not. I want to clarify that with a special variance request there would be no need to analyze any code for the other properties? In the alternative, if we chose not to request a special variance to this section, what additional requirements/sections would be applicable? We were hoping to submit the application as a Type II so we need to determine what would be required with this approach.

5. Due to the interpretation identified in No. 3 above the previously mentioned special variance to Subsection 17.98.120(A) will not be required. Staff would like to point out that Subsection 17.90.100(J)(2) states that outdoor storage must be screened from view from public rights of way and civic spaces at least eight (8) feet. The screening requirements of Subsection 17.92.90 will apply to the property. Section 17.90.100(J)(2) refers to submittal requirements. I believe the reference should have been 17.90.110(J)(2)? In reviewing this section I am not finding the reference to eight feet. We intend surrounding the site with a five foot planter and increase the size of this planter at the corner.

J. External Storage and Screening

Intent: To promote land use compatibility and aesthetics, particularly where development abuts public spaces.

- 1. Exterior storage of merchandise and/or materials, except as specifically authorized as a permitted accessory use, is prohibited.*
- 2. Where such storage is allowed, it must be screened from view from public rights of way and civic spaces.*

James Cramer

Jan 9, 2019, 11:16 AM (3 days ago)

to Jerry, Kelly, me

Tracey,

Please see my responses to the requested clarification below:

2. The C-1 zone district is intended to provide the principal focus for civil and social functions within the community (17.42.00). Subsection 17.90.00(C) states Sandy Style is based on a set of principles (1-5) and No. 3 states "*Emphasize a "village" scale and character in new development. Village scale means development is compact and walkable, building entrances are oriented to the street sidewalk or a plaza, and large building masses are broken down through a combination of design elements such as articulation, combinations of complementary building materials and detailing.*" Additionally as you mentioned, items 1-11 identified in Subsection 17.90.00(D) are guidelines for incompatibility to the Sandy Style which we have identified items 3, 10 & 11 are present within the proposal. Our last response (No. 4) established that a structure shall be associated with each commercial use identified in Subsection 17.42.10(B) and Subsection 17.90.110(D) states "*Intent: To maintain and enhance downtown and village commercial streetscapes as public spaces, emphasizing a pedestrian-scale and character in new development, consistent with the Sandy Style; and to provide for a continuous pedestrian network that promotes pedestrian safety, comfort and convenience, and provides materials and detailing consistent with the Sandy Style.*" Subsection 17.90.110 goes on to discuss additional design regulations such as Site Layout, Building Facades, Materials, Colors, Building Orientation and Entrances, Landscaping and Streetscape Design and more all with the intent of enhancing the pedestrian environment. When we review this proposal we have to determine if the intent of all the above guidelines have been met therefore it becomes difficult to support a proposal if these elements are not incorporated. At the meeting we had with the property owner I suggested incorporating additional pedestrian elements such as enhanced landscaping and/ or a knee wall at the property lines in the efforts to better meet the intent of the design guidelines and zone district.

4. By requesting a special variance to Subsection 17.90.110(D)(1) for the subject lot of record we will not be reviewing the site in combination with the existing Fun Time RV. In the alternative we would apply all applicable codes to the entire site

which would include compliance with 17.42 development standards, 17.90 Design Standards, 17.92 Landscape & Screening standards and 17.98 parking standards. In this alternative the property would still need a variance from 17.90.110(D)(1) and potentially additional variances/deviations regarding screening, landscaping and/or parking. Additionally, the existing conditional use would be subject to review for the entire site as the intent of the proposal is to increase the conditional use in conjunction with the existing parcel. Staff is comfortable reviewing the site on its own and associating it with the neighboring property, as it is under the same ownership. Our interpretation of the code and review of the property on its own is aimed at minimizing the needed requests. If you choose the alternative I would recommend an additional Pre-Application meeting so staff would have the opportunity to access all applicable codes as that has not been assessed at this time.

5. You are correct I was referencing 17.90.110(J)(2) and upon further review the 8 feet is associated with Subsection 17.90.120(J)(2) or the C-2 zone district therefore is not applicable to the proposed site. Please disregard.



EXHIBIT H

James Cramer <jcramer@ci.sandy.or.us>

Fun Time RV File 18-030 DR

1 message

Hassan Ibrahim <hai@curran-mcleod.com>
To: James Cramer <jcramer@ci.sandy.or.us>
Cc: Mike Walker <mwalker@ci.sandy.or.us>

Fri, Mar 22, 2019 at 6:13 AM

Hi James,

I have the following comments on the above noted project:

1. The access to Proctor Blvd (US 26) will be subject ODOT concurrence, however, we recommend its closure due to its the proximity to the intersection (Beers Avenue).
2. The curb return at Beers Avenue/Proctor Blvd intersection shall mimic the one on the east side to include dual ADA ramps with Crosswalk Closed sign placed in the SW corner (south direction).
3. Half Street improvements will be required on Beers Avenue to include extending the existing curb and sidewalks along the entire site frontage. The improvements shall include street lighting, landscaping and storm drainage. The existing right of way is adequate and meets the City local street standards. The pavement may need to extend to the centerline of the right of way if the existing structural section is determined to be deficient and doesn't meet the City structural section standards.
4. The stormwater calculation appear to meet the City water quality and quantity requirements, a final drainage report shall be submitted with the final plans for review and approval.

Should you have any questions, please let me know.

Regards,

Hassan Ibrahim, P.E.
CURRAN-McLEOD, INC.
6655 SW Hampton St, Ste. 210
Portland, OR 97223
Tel: 503-684-3478
Fax: 503-624-8247
Cell: 503-807-2737
email: hai@curran-mcleod.com



Oregon
Kate Brown, Governor

EXHIBIT I

Department of Transportation
Region 1 Headquarters
123 NW Flanders Street
Portland, Oregon 97209
(503) 731.8200
FAX (503) 731.8259

March 22nd, 2019

ODOT #8507

ODOT Response

Project Name: Fun Time RV Storage Expansion	Applicant: Jerry Fisk
Jurisdiction: City of Sandy	State Highway: US 26
Site Address: 38415-38477 Proctor Blvd., Sandy, OR	

The site of this proposed land use action is adjacent to Proctor Blvd (US 26). ODOT has permitting authority for this facility and an interest in ensuring that this proposed land use is compatible with its safe and efficient operation. **Please direct the applicant to the District Contact indicated below to determine permit requirements and obtain application information.**

COMMENTS/FINDINGS

Access

As identified in the submittal, the applicant is closing the existing curb cut to Proctor Boulevard. ODOT supports and recommends the closure of this access and replaced with curb and sidewalk to ODOT and ADA standards. To facilitate closure, the applicant will need a Miscellaneous Permit from ODOT to Occupy or Perform Operations on a State Highway.

Additionally, ODOT recommends that the applicant record cross-over access easements to the adjacent properties (TL03600, 03800, 04001) to facilitate shared access. Shared access will improve highway safety by reducing potential conflicts between vehicles and between vehicles and pedestrians and bicyclists at closely spaced driveways and will implement ODOT Access Management Program goals.

ADA Improvements

There is an existing marked crosswalk across US 26 on the east side of US 26 (Proctor Boulevard)/Beers Avenue intersection. ORS 801.220 says, in part “whenever marked crosswalks have been indicated, such crosswalks and no other shall be deemed lawful across such roadway at that intersection.” There are no ADA ramps on the west side of the intersection (crossing US 26), but there is not a record of formal crosswalk closure. ODOT is willing to process the crosswalk closure internally, versus requiring the developer to apply, if the developer/applicant installs the metal tube supports and signs per ODOT Standard Drawing TM490, Crosswalk Closure Detail (attached) on both the north and south sides of the west leg of the US 26 (Proctor Boulevard)/Beers Avenue intersection.

Otherwise, since the corner does not meet ODOT or ADA standards, the applicant will be required though ODOT’s Miscellaneous Permit process to apply for the Crosswalk Closure and install the metal tube support and sign per ODOT Standard Drawing TM490, Crosswalk Closure Detail (attached), on the north side of the west leg of the US 26 (Proctor Boulevard)/Beers Avenue intersection.

General

All alterations within the State highway right of way are subject to the ODOT Highway Design Manual (HDM) standards. Alterations along the State highway but outside of ODOT right-of-way may also be subject to ODOT review pending its potential impact to safe operation of the highway. If proposed alterations deviate from ODOT standards a Design Exception Request must be prepared by a licensed engineer for review by ODOT Technical Services. Preparation of a Design Exception request does not guarantee its ultimate approval. Until more detailed plans have been reviewed, ODOT cannot make a determination whether design elements will require a Design Exception. *Note: Design Exception Requests may take up to 3 months to process.*

All ODOT permits and approvals must reach 100% plans before the District Contact will sign-off on a local jurisdiction building permit, or other necessary requirement prior to construction.

ODOT RECOMMENDED LOCAL CONDITIONS OF APPROVAL

Frontage Improvements and Right of Way

- Curb, sidewalk, cross walk ramp(s) bikeways and road widening shall be constructed as necessary to be consistent with local, ODOT and ADA standards.
- The developer/applicant shall install metal tube supports and signs per ODOT Standard Drawing TM490, Crosswalk Closure Detail (attached) on both the north and south sides of the west leg of the US 26 (Proctor Boulevard)/Beers Avenue intersection.

Access to the State Highway

- The applicant shall close the access (i.e. curb cut) to US 26 (Proctor Boulevard) and replace with curb and sidewalk to ODOT and ADA standards.
- The applicant shall record cross-over access easements to the adjacent properties with state highway frontage with the County Assessor to facilitate shared access. Shared access will improve highway safety by reducing potential conflicts between vehicles and between vehicles and pedestrians and bicyclists at closely spaced driveways and will implement ODOT Access Management Program goals.

Permits and Agreements to Work in State Right of Way

- An ODOT Miscellaneous Permit must be obtained for all work in the highway right of way. When the total value of improvements within the ODOT right of way is estimated to be \$100,000 or more, an agreement with ODOT is required to address the transfer of ownership of the improvement to ODOT. An Intergovernmental Agreement (IGA) is required for agreements involving local governments and a Cooperative Improvement Agreement (CIA) is required for private sector agreements. The agreement shall address the work standards that must be followed, maintenance responsibilities, and compliance with ORS 276.071, which includes State of Oregon prevailing wage requirements. *Note: If a CIA is required, it may take up to 6 months to process.*
- The applicant must obtain an ODOT permit to place trees in the state right of way. Tree spacing and design must be consistent with the ODOT Highway Design Manual section 4.2.6 (http://www.oregon.gov/ODOT/Engineering/Documents_RoadwayEng/HDM_04-Cross-Sections.pdf).

If proposed tree placement deviate from ODOT standards (such as placement in a planter strip), a Design Exception Request for clear zone must be prepared by a licensed engineer for review by ODOT Technical Services. Preparation of a Design Exception request does not guarantee its ultimate approval. *Note: It may take up to 3 months to process a Design Exception.*

ADVISORY INFORMATION

Use of State Highway Right of Way:

- Stopping and/or parking vehicles upon State highway right of way for the maintenance of adjoining property or in furtherance of any business transaction or commercial establishment is strictly prohibited. Loading and unloading of vehicles within the state highway right of way is not permitted. The applicant must provide adequate on-site circulation for the parking and maneuvering of all vehicles anticipated to be displayed or parked on the lot (ORS 811.346, 811.550, and 811.580).

Signs:

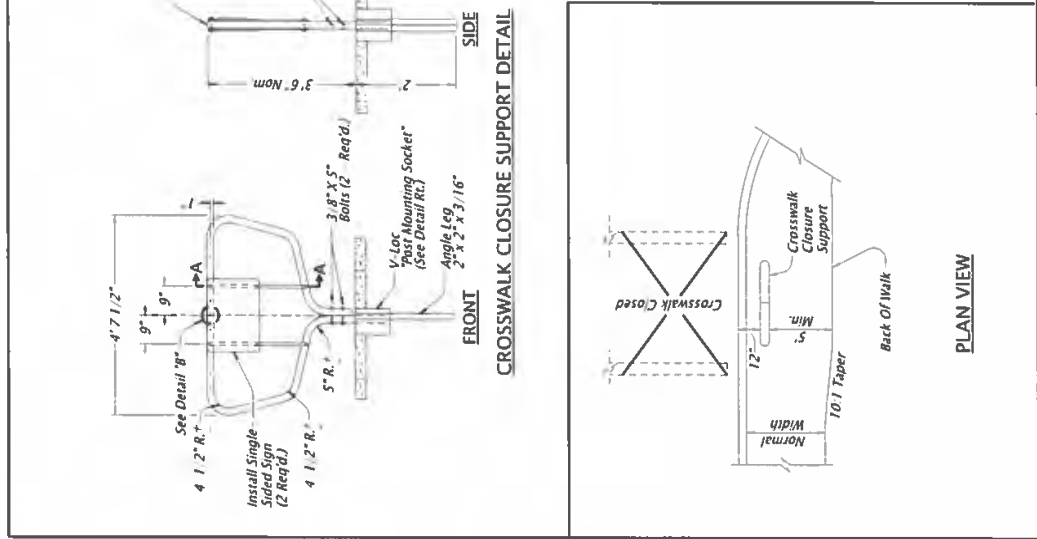
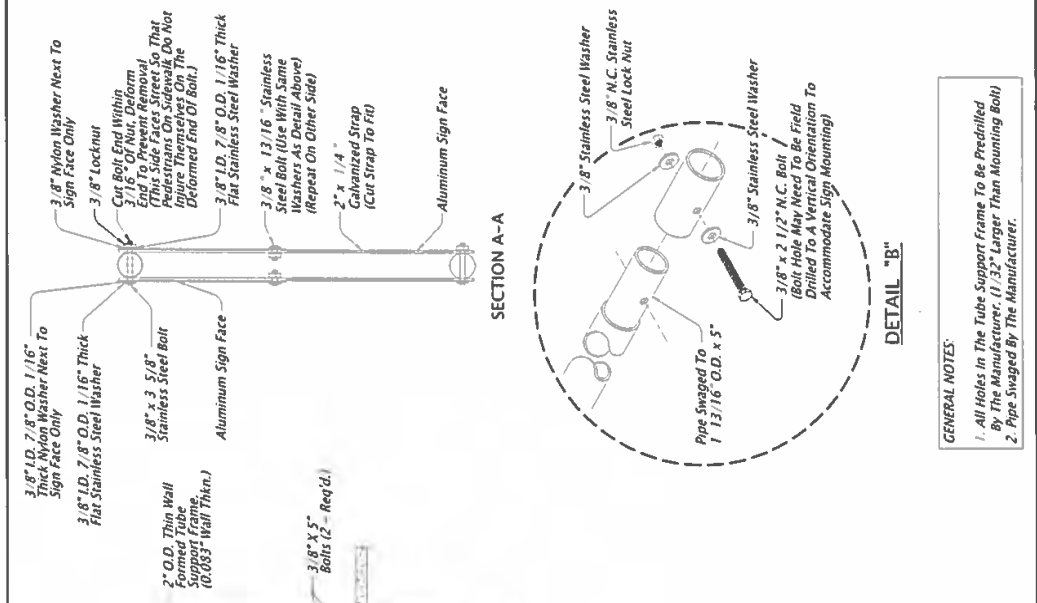
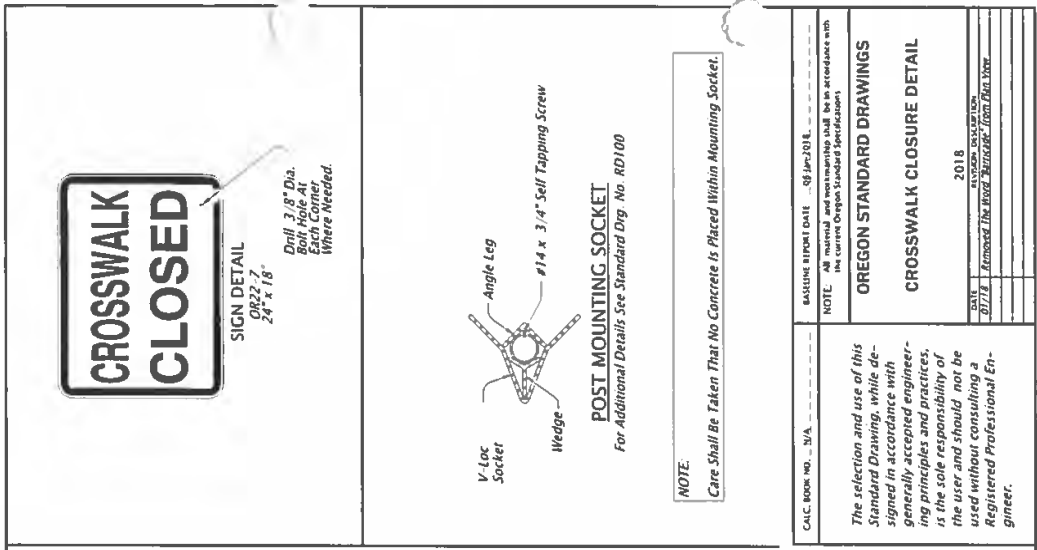
- Private signs are not permitted in the state highway right of way (ORS 377.700-377.840).

Please send a copy of the Notice of Decision including conditions of approval to:

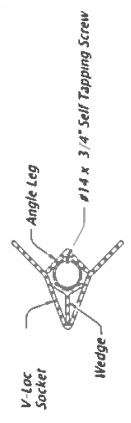
ODOT Region 1 Planning
Development Review
123 NW Flanders St
Portland, OR 97209

Region1_DEVREV_Applications@odot.state.or.us

Development Review Planner: Marah Danielson	503.731.8258, marah.b.danielson@odot.state.or.us
Traffic Contact: Avi Tayar, P.E.	503.731.8221
District Contact: Loretta Kieffer	503.667.7441



TM490
 DD MON-YYY
 filename.dgn



POST MOUNTING SOCKET
 For Additional Details See Standard Dwg. No. RD100

NOTE:
 Care Shall Be Taken That No Concrete Is Placed Within Mounting Socket.

CALC. BOOK NO. - N/A	BASELINE REPORT DATE - 09 JAN 2018.
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications.	
OREGON STANDARD DRAWINGS	
CROSSWALK CLOSURE DETAIL	
DATE	2018
BY	W. J. WILSON
DTL	Revised The Above Standard - General Note

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.

TM490

Effective Date: June 1, 2018 - November 30, 2018

EXHIBIT J

MEMORANDUM

TO: JAMES CRAMER, ASSOCIATE PLANNER
FROM: MIKE WALKER, PUBLIC WORKS DIRECTOR
RE: FUNTIME RV FILE No. 18-030DR
DATE: MARCH 22, 2019

The following are Public Works' comments on the above-referenced application:

Transportation

The Beers Ave. frontage of the site is unimproved. Street frontage improvements conforming to the requirements of 17.84.30 and 17.84.50 Sandy Municipal Code (SMC) shall be constructed along the Beers Ave. frontage of the site. The existing improved street section north of the site is 34 ft. wide. An additional 3 ft. of right-of-way must be dedicated along the Beers Ave. frontage in order to provide the required minimum street section per 17.84.30(A)1 SMC. The applicant shall provide a legal description and sketch of the dedication area and pay all recording costs for the Dedication Deed.

Street frontage improvements include lighting and storm drainage per 17.84.80 (G) SMC.

As an alternative to dedicating right-of-way on Beers Ave. the City would accept an eight-foot wide curb-tight sidewalk with the brick paver strips and scoring pattern matching the existing sidewalks in the Urban Renewal Area with street trees in 48" x 48" tree wells 30 ft. on center.

The existing curb cut on Proctor is not in use and has been abandoned for some time. The curb cut shall be eliminated and replaced with standard Type C curb per 17.100.90(C) SMC.

Based on the applicant's submission the existing sidewalk on the Proctor Blvd. frontage of the development site is only five feet wide. The Proctor Blvd. frontage of the site shall be improved with a six-foot wide sidewalk adjacent to the property line with the remaining right-of-way developed as a planter strip with approved street trees 30 feet on center per 17.84.30(A)2 SMC.

OR

The Proctor Blvd. frontage shall be improved with a curb-tight sidewalk between the property line and the curb. This sidewalk shall be constructed with the same scoring pattern, decorative brick paver strip and street trees in tree wells on 30 ft. centers. Tree wells shall include conduit and vertical risers for future tree illumination.

Street frontage improvements include lighting and storm drainage per 17.84.80 (G) SMC.

The existing ADA ramp at the SE corner of the site shall be replaced with ADA-compliant ramps and the new curb return transition to the improvements on Beers St. shall mirror the existing curb extension on the east side of Beers.

A gate is proposed at the proposed Beers Ave. driveway approach to the sales lot. The proposed driveway throat shall be deep enough to allow an RV and tow vehicle to pull into the site without interfering with traffic on Beers Ave. or blocking the sidewalk.

OR

A gate is proposed at the entrance to the sales lot. In order to prevent an RV and tow vehicle entering or exiting the site from interfering with traffic on Beers Ave. or blocking the sidewalk the gate shall be motorized and remotely operated to allow entrance and exit to and from the approach without stopping in the public right-of-way .



**CITY OF
SANDY**

39250 Pioneer Boulevard • Sandy, OR 97055

EXHIBIT K

Phone 503-668-5533
Fax 503-668-8714
www.cityofsandy.com

Gateway to Mt. Hood

**FINDINGS OF FACT and FINAL ORDER
TYPE III DECISION**

DATE: March 30, 2009

FILE NO.: 09-001 CUP

PROJECT NAME: Fred's RV Conditional Use Permit

APPLICANT: Dolores McNally

OWNER: Jerry Carlson (property owner)

LEGAL DESCRIPTION: T2S R4E Section 13BC Tax Lot 3600

DECISION: The use of automobile, truck, marine, and recreational vehicle repair and retail sales of pre-owned automobiles, trucks, and parts and accessories is approved under the exception in Section 17.08.50(A)4 of the Development Code. Site improvements including painting, lighting and sign and landscaping upgrades, and screening at 38415 SE Proctor are in conformance with applicable conditional use permit criteria as approved with conditions outline in this Final Order and Findings of Fact.

Applicant's Submittals

- A. Land Use Application
- B. Project Narrative
- C. Site Improvement Plans

Agency Comment:

- D. Email from Mike Walker, Public Works Director (2/18/2009)
- E. Letter from ODOT (2/20/2009)

Public Comments:

- F. Mr. and Mrs. Sliger
- G. Mr. William Granstrom

The above-referenced proposal was reviewed through a Type III Conditional Use Permit review. The following Findings of Fact are adopted supporting approval of the conditional use permit in accordance with Chapter 17 of the Sandy Municipal Code.

1. These findings supplement and are in addition to the Staff Report dated March 17, 2009, which are incorporated herein by reference. Where there is a conflict between these findings and the staff report, these findings shall control.
2. The applicant submitted the application for conditional use permit approval on January 30, 2009 (Exhibit A). Staff reviewed the application to determine compliance with submittal requirements and deemed the application complete on February 4, 2009 in accordance with ORS 227.178 (2.b).
3. The staff report, memorandums and this final order are based upon the Exhibits listed above, as well as the testimony and discussion at the Planning Commission hearing on March 23, 2009.
4. The site is located at 38415 SE Proctor, on the north side of Proctor between Bluff Road and Beers Avenue.
5. The parcel has a Comprehensive Plan Map designation of Commercial and a Zoning Map designation of C-1, Central Business District.
6. Notification of the proposed application was mailed to property owners within 300 feet of the subject property and affected agencies on February 5, 2009. In addition, a legal notice was placed in the local paper on March 5, 2009.
7. Written testimony was received from the following individuals:
 - Mr. and Mrs. Sliger, 38432 Hood Street, Sliger Apartments
 - Mr. William Granstrom, 17321 Beers Avenue, Suburban Apartments
8. The following individuals spoke at the March 23, 2009 public hearing:
 - Cory Stone, applicant
 - Jerry Carlson, property owner
9. The site was originally constructed for a retail auto dealership and associated repairs in the mid-1960's. The existing building on the site is approximately 10,043 total square feet total including a 2,601 square foot showroom and 7,442 square foot shop area. Historically, the site has been the location of various retail auto and other vehicle related businesses. At some point after the establishment of the original retail auto dealership, zoning changes took place which rendered the use non-conforming. Originally the building was constructed for Richardson Chevrolet and then later became Carlson Chevrolet. According to available business license records, the location became Suburban Suzuki in 2000. Use of the site changed to include some autos, horse and utility type trailers, and motorcycles. An interpretation of the Sandy Municipal Code, 01-007 INT Carlson Trailer Sale, stated that the trailer sales were a permitted accessory use subordinate to the primary use of retail auto sales on the site. In 2007, the business became Certified Auto Truck and conducted retail auto and truck sales as well as repair. The site has been vacant since approximately June of 2008 after Certified Auto Truck moved outside of Sandy.

10. In 1999, Pete Carlson filed an application (File No. 99-036 DCA) to amend the Development Code to allow automobile sales as a Conditional Use in the C-1 zoning district. The applicant then modified the application to request that Section 17.08.50 be amended to allow automobile sales to be classified as an exception rather than a non-conforming use in the zone. The Planning Commission reviewed the application and adopted Findings denying the request. The City Council then considered the request and approved it noting the following factors: 1) the exception would be limited to the Applicant's existing auto retail business; 2) the exception would allow expansion of the business only on the adjoining .32 acre lot; 3) the City would retain control over the expansion through the Conditional Use Permit process; 4) the expansion would result in a better use of the .32 acre lot than currently exists; 5) there was no opposition to the amendment; 6) the applicant's existing business has been a model business in the city; 7) the exception would further the City's goal of keeping auto retail business in consolidated locations; and 8) the exception would be consistent with previous exceptions adopted in 1997 related to auto fueling stations and car washes. As specified in the Findings for Ordinance 2000-02, the Planning Commission finds the exception in Section 17.08.50(A)4 applies only to the subject property and approves the proposed use under this exception.

17.42 – C-1 Zoning District

11. According to Section 17.42.00, Intent, the C-1 zone is not intended for intensive automobile or industrial uses. Section 17.08.50(A)4 of the Code allows a retail auto dealership on this site without being subject to non-conforming status. The proposed use was reviewed according to the exception in Section 17.08.50(A)4. The Planning Commission finds the proposed use can be approved under this exception. In addition, the Planning Commission finds the proposal cannot be approved as either a permitted or conditional use in the C-1 Zone.
12. The site was developed in the mid 1960's, and does not meet current standards. There is no change in use proposed. The request for a Conditional Use Permit does not require the site to be brought into compliance with current standards of the C-1 zoning district.

17.68 – Conditional Uses

13. The Planning Commission reviewed the proposed site improvements to allow automobile, truck, marine, and recreational vehicle repair and retail sales of pre-owned automobiles, trucks, and parts and accessories under the Conditional Use Permit criteria in Section 17.68.20 below.
14. Section 17.68.20(A) requires that: *"The use is listed as a conditional use in the underlying zoning district or has been interpreted to be similar in use to other listed conditional uses."* Vehicle repair is not identified as a conditional use in the C-1 zone; however, the site is governed by the exception in Section 17.08.50(A)4 of the Code. The proposed use of automobile, truck, marine, and recreational vehicles repair and retail sales of pre-owned automobiles, trucks and accessories has been determined by the Planning Commission to be similar to the use permitted by the exception.
15. Section 17.68.20(B) requires that: *"The characteristics of the site are suitable for the proposed use considering the size, shape, location, topography, and natural features."* The site was constructed to accommodate retail auto sales and associated vehicle repairs, and is designed in such

a way that would accommodate the proposed use. A large portion (approximately 7,442 square feet) of the existing building is a shop area with overhead doors designed to accommodate vehicle repair. Additionally, the applicant proposes site improvements that will enhance the overall appearance and function of the site. As such, the Planning Commission determines the characteristics of the site are suitable for the proposed use.

16. Section 17.68.20(C) requires that: *"The proposed use is timely considering the adequacy of the transportation systems, public facilities and services existing or planned for the area affected by the use."* The site is served by water, sewer and stormwater facilities. Due to the similarities between the previous uses of the site and the proposed use, traffic generated by the proposed use is not anticipated to change. As such, the Planning Commission determines the proposed use is timely considering the adequacy of transportation systems and public facilities.
17. Section 17.68.20(D) requires that: *"The proposed use will not alter the character of the surrounding area in a manner which substantially limits, precludes, or impairs the use of surrounding properties for the primary uses listed in the underlying zoning district."* Past uses on the site have included both retail auto sales and repair. The applicant proposes site upgrades including lighting, screening, and landscaping to improve the overall condition of the property. For these reasons, the Planning Commission determines the use will not alter the character of the surrounding area.
18. Section 17.68.20(E) requires that: *"The proposed use will not result in the use of land for any purpose which may create or cause to be created any public nuisance including, but not limited to, air, land, or water degradation, noise, glare, heat, vibration, or other considerations which may be injurious to the public health, safety, and welfare."* The potential impacts of the proposed use are similar to the impacts of the previous uses of the site. Automobile repair has been conducted on the site previously by past businesses. The proposed use would create similar noise and outdoor storage conditions as those uses in the past. Proposed site improvements will mitigate potential disturbances to adjacent properties identified through public comments regarding noise and glare. For these reasons, the Planning Commission determines that the use will not create a public nuisance.
19. Section 17.68.20(F) requires that: *"The proposed use will be reasonably compatible with existing or planned neighboring uses based on review of the following: Basic site design (organization of uses on the site); visual elements (scale, structural design and form, materials, and so forth); noise; noxious odors; lighting; signage; landscaping for buffering and screening; traffic; effects on off-street parking; effects on air quality and water quality."* The site was developed before current standards were established. The applicant is proposing improvements to the site including landscaping, lighting and signage upgrades, screening, and painting that will enhance the overall appearance and function of the site. Landscaping improvements include removing existing shrubs and trees and planting new shrubs and five Red Spire Pear trees along the Proctor Blvd. frontage. Lighting improvements proposed include the removal of all building and pole mounted flood lights. Signage modifications include removal of one of the pole mounted signs. In addition, the applicant proposes installing a rock base on the existing freestanding sign. To screen the outdoor storage areas, the applicant proposes to install a 6-foot high chain link fence with slats on either

side of the building and along the west property line. The applicant proposes repainting existing building trim from blue to green.

20. Regarding visual elements, the existing building is painted white with a primary blue trim. The applicant proposes repainting the building trim green. The paint color must comply with the approved Color Palette in Section 17.90.110. For this reason, The Planning Commission requires the applicant to submit a paint color to be approved.
21. Regarding noise, the repairs will take place inside the building and the business hours of operation will be during the day. Accordingly, noise levels associated with the repairs will not conflict with proposed uses on the subject property or existing or planned uses on surrounding adjacent properties.
22. Regarding lighting, the existing building and pole mounted flood lights will be removed. This will prevent light from shining onto the adjacent properties.
23. Regarding signage, there are two existing signs on the property, a freestanding sign and a pole mounted sign. The pole mounted sign will be removed. A rock base will be installed on the freestanding. Section 15.32.020(D)8 of the Code requires the base to extend 36 inches above the adjacent grade and wrap around all sides of the sign base. For these reasons, the Planning Commission requires the proposed rock base to conform to the requirements of Section 15.32.020(D)8.
24. Regarding landscaping, existing low-quality trees and shrubs on the site will be removed and replaced with new landscaping. The applicant proposes planting five Red Spire Pear trees as part of the landscape improvements. The Planning Commission requires the tree species be Chanticleer Pear (*Pyrus Calleryana* 'Chanticleer') to match other trees planted in the downtown. Additionally, the applicant proposes installing a 6-foot high chain link fence with slats along the west and east portions of the site to screen the outdoor storage areas. Written testimony received by the Planning Commission requested the installation of a sound proofing wall to be installed along the north property line of the site. The Planning Commission determines because the proposed use of the site is similar to previous uses and the zoning of the adjoining property is the same as the subject property, installation of an eight-foot tall chain link fence with slats will be adequate to screen the site. The Planning Commission requires the applicant to submit a revised site plat featuring a fence in this location. In addition, the Planning Commission requires the applicant to submit a color sample of the proposed fence slats in compliance the approved Color Palette in Section 17.90.110 for approval prior to installation.
25. Regarding traffic, there should not be a significant difference in the trip generation of the proposed use and previous uses of the site. There are three existing access points on the site. To improve access management, the Planning Commission requires the easternmost access point to be closed. The applicant must submit a Public Works Department Driveway Demolition permit to close the easternmost access point according to the standard city detail.
26. The Planning Commission finds the proposed use including site improvements as modified in this Order is reasonably compatible with existing or planned neighboring uses.

17.98 – Parking, Loading and Access Requirements

27. Parking has already been constructed with development of the existing building. As such, review of the application was limited to ensuring an adequate number of parking spaces are available to serve the proposed use.
28. Section 17.98.20 contains off-street parking requirements. The applicant proposes providing nine parking spaces as shown on the Site Plan submitted with the application. Five parking spaces are proposed to be head-in 90 degrees and four angled. The existing building contains 10,043 square feet and 10 employees are proposed on any one shift. Section 17.98.20 (H) requires one parking space per 800 square feet for bulk merchandise operations including motor vehicles. In addition, Section 17.98.20 (C) requires one space per every two employees. As such, the calculation for determining required parking is $10,043/800 = 12.5$ (rounded to 13), $10/2 = 5$, $13+5 = 18$ required parking spaces, including one ADA parking space. As discussed at the public hearing, the Planning Commission requires all parking spaces to be head-in 90 degrees to facilitate proper on-site traffic circulation. Section 17.98.60 (B) requires parking spaces to be 9 feet wide by 18 feet deep, and section 17.98.60 (C) requires an aisle width of 25 feet for 90 degree parking spaces. As such, the Planning Commission requires the applicant to submit a revised site plan featuring 18 parking spaces including one ADA accessible space. In addition, the Commission requires all parking spaces to be head-in 90 degrees with aisle widths meeting Code requirements.

DECISION

For these reasons described above, the request by Fred’s RV to use the property located at 38415 Proctor Blvd. for the specified purposes is approved as modified by the conditions of approval below.

CONDITIONS OF APPROVAL

The following conditions of approval are adopted:

A. Prior to the issuance of a temporary Certificate of Occupancy or use of the site for the proposed purpose, the applicant shall submit additional information and complete required items as specified below:

1. Submit a revised site plan drawn to scale to include the following changes:
 - Provide 18 parking spaces including one ADA accessible parking space.
 - Parking spaces shall be at least nine (9) feet wide by 18 feet deep;
 - All spaces shall be head-in 90 degrees to facilitate onsite traffic circulation
 - A 25-foot aisle width between rows of spaces shall be provided.
 - Provide an eight foot tall chain link fence with slats along the north property line.
2. Submit a revised landscape plan to include the following:
 - Change the tree species proposed along Proctor Boulevard to Chanticleer Pear (Pyrus Calleryana ‘Chanticleer’) to match other trees planted in the downtown.

- Specify landscape materials within the new planter area created by the closing of the eastern access drive.
3. Submit a paint color sample for the building trim in compliance with the Approved Color Palette in Section 17.90.110 for approval.
 4. Submit a fence slat color sample in compliance with the Approved Color Palette in Section 17.90.110 for approval.
 5. Install a rock base on the existing freestanding sign. The base shall be at least 18-inches wide and extend at least 36-inches above the adjacent finished grade as required in the Code.
 6. Install all fencing as specified on the approved plan including along the west, north, and eastern portions of the site.
 7. Install bumper guards spaced a maximum of six feet apart and set back eight feet from the north edge of the planter strip between the western and center access points.
 8. Remove the existing pole mounted sign.
 9. Remove all existing building and pole mounted flood lights. Downward facing pole lights are allowed to be retained.
 10. Stripe parking spaces and directional arrows within the parking lot.

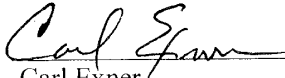
B. Within 60 days from the date the business opens and prior to the issuance of a final Certificate of Occupancy, the following items shall be completed unless a bond is submitted guaranteeing their completion is submitted and approved:

1. Complete improvements to close the eastern access drive by constructing a new curb, planter, and sidewalk in this location. Prior to construction, submit a Public Works Department Driveway Demolition Permit in accordance with the City's standard detail.
2. Install landscape materials as identified on the revised and approved Landscape Plan.

C. General Conditions

1. All site signage will require submittal of a sign permit in compliance with Chapter 15.32, Signs and clear vision area standards of Section 17.74.30.
2. Approval of this conditional use may be revoked in accordance with the Sandy Municipal Code if conditions of approval are not met. Conditional Use Permit approval does not grant authority for the unrestricted use of the structure or site. Any use of this site may be prohibited until such time as all required improvements are completed.

3. Comply with all other conditions or regulations imposed by Clackamas County, Fire District No. 72, or state and federal agencies. Compliance is made a part of this approval and any violations of these conditions and/or regulations may result in the review of this approval and/or revocation of approval.

 3-30-09
Carl Exner Date
Vice Chair, Planning Commission

RIGHT OF APPEAL

A decision on a land use proposal or permit may be appealed to the City Council by an affected party by filing an appeal with the Director within 10 calendar days of notice of the decision. Any person interested in filing an appeal should contact the city to obtain the form, "Notice of Appeal", and Chapter 17.28 of the Sandy Development Code regulating appeals. All applications for an appeal shall indicate the nature of the interpretation that is being appealed and the matter at issue will be a determination of the appropriateness of the interpretation of the requirements of the Code.

An application for an appeal shall contain:

1. An identification of the decision sought to be reviewed, including the date of the decision;
2. A statement of the interest of the person seeking review and that he/she was a party to the initial proceedings;
3. The specific grounds relied upon for review;
4. If de novo review or review by additional testimony and other evidence is requested, a statement relating the request to the factors listed in Chapter 17.28.50; and Payment of required filing fees.



**CITY OF
SANDY**

39250 Pioneer Boulevard • Sandy, OR 97055

Gateway to Mt. Hood

Exhibit L

Phone 503-668-5533
Fax 503-668-8714
www.cityofsandy.com

**FINDINGS OF FACT and FINAL ORDER
TYPE III LAND USE DECISION**

DATE: December 19th, 2014

FILE NO.: 14-026 VAR

PROJECT NAME: Fred's RV Variance

APPLICANT: Fred's RV Auto Marine Inc.

OWNER: Carlson Culver Property LLC

LEGAL DESCRIPTION (IMPACTED): T2S R4E Section 13BC 03900

DECISION: The Planning Commission approves a Special Variance to allow Fred's RV Auto Marine Inc. to use a vacant unimproved lot for RV display and sales for a period not to exceed 12 months as modified by the conditions listed below.

The above-referenced proposal was reviewed as a Type III Variance, the following Findings of Fact are adopted supporting approval of the plan in accordance with Chapter 17 of the Sandy Municipal Code.

EXHIBITS:

Applicant's Submission

- A. Land Use Application Form
- B. Narrative
- C. Cost Estimate for Site Development
- D. Google Earth Aerials

Agency Review Comments

None

Public Comments

- E. Rachel Smith (December 8, 2014)
- F. Lila Leathers (December 15, 2014)

R:\P2K\Orders\2014\14-026 VAR Fred's RV Variance Order.doc

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The City of Sandy is an equal opportunity employer and does not discriminate on the basis of race, religion, sex or handicapped status.

FINDINGS OF FACT

General

1. These findings are based on the applicant's original submittal received on November 3, 2014. The application was deemed complete on November 10, 2014.
2. The subject site has a total gross area of approximately 0.33 acres. The site is located north of Proctor Blvd., west of Beers Avenue, and east of Fred's RV Auto Marine.
3. The parcel has a Plan Map designation of Commercial and a Zoning Map designation of Central Business District, C-1.
4. Fred's RV Auto Marine Inc. submitted an application for a special variance to use a vacant unimproved lot for RV sales. The applicant is seeking approval to use the lot without installing required site improvements including paved surfacing, landscaping, and completing frontage improvements. The applicant's submittal indicates their goal is to comply with code requirements within a period of 36 months using business revenue generated from additional RV sales.
5. The applicant provided a preliminary Engineer's estimate (Exhibit C) to complete onsite and frontage improvements including \$4,200 for stormwater improvements, \$62,770 for on-site improvements (paving, landscaping, and lighting), and \$23,724 for street frontage improvements along Beers Avenue for a total project cost of \$90,694.
6. Notification of the proposal was mailed to property owners within 300 feet of the subject property, the applicant and property owner, and to affected agencies on November 25, 2014. A legal notice was published in the Sandy Post on Wednesday, December 3, 2014. No agency comments were submitted. One written comment was received from Rachel Smith during the comment period and one written comment was received from Lila Leathers after the staff report was published, as summarized below:
 - Rachel Smith (Exhibit E, December 8, 2014) – Rachel Smith at 17170 Beers Street submitted written testimony in favor of Fred's RV using the unimproved lot. Ms. Smith stated the expansion of the business could provide more job opportunities and increased tax revenue for the City of Sandy.
 - Lila Leathers (Exhibit F, December 15, 2014) – Lila Leathers who owns the Shell gas station at 38422 Proctor Blvd. submitted written testimony in favor of Fred's RV using the unimproved lot.
7. No proponent, opponent, or applicant testimony was presented at the Planning Commission public hearing on December 17, 2014.
8. The primary parcels used currently by Fred's RV include tax lots 3500 (storage behind Elite Car Audio), 3600 (RV sales and service structures), 3800 (RV sales with no structures), and 4001 (RV sales with no structures). The applicant is requesting this variance in order to use tax lot 3900 an unimproved grassy lot for RV display and sales.

9. Tax lots 3500, 3600, 3800, and 4001 were originally developed for a retail auto dealership and associated repairs in the 1960's. The buildings on those lots contain approximately 10,043 square feet including a 2,601 square foot showroom and 7,442 square foot shop area. At some point after the establishment of the original retail auto dealership, zoning changes took place which rendered the use non-conforming. Originally the building was constructed for Richardson Chevrolet and then later became Carlson Chevrolet. According to available business license records, the location then became Suburban Suzuki in 2000. In 2007, the site was used by Certified Auto Truck for retail auto and truck sales, as well as repair. Then in June of 2008, the site was vacated when Certified Auto Truck left Sandy.
10. The building was vacant until Fred's RV Auto Marine Inc. requested a Conditional Use Permit in 2009 (File No. 09-001 CUP) to use the existing structures on the site (Tax Lot 3600) in order to continue servicing RV's of existing clients. The applicant's narrative explains that Fred's RV Auto Marine Inc. was established in 1971 and moved to Sandy in the 1990's to a location along Highway 26 at the east end of town (current Johnson RV site). With the Great Recession of 2007-2008, Fred's decided to downsize its operation, sell the property and buildings, and seek approval to move to the current location in downtown Sandy. The conditional use permit application, site plan, and notice map at that time requested use of tax lot 3600 only in order to continue to provide service to their clients. The Planning Commission reviewed the application and granted a Conditional Use Permit with conditions to use tax lot 3600 and the buildings on this tax lot for RV service. Since the applicant's request did not include the adjoining lots and did not include extensive RV sales, these were not considered. Reviewing the operation today, it is apparent the area devoted to RV sales has expanded from what was originally considered. The approval did not include the degree of RV sales as seen on the site today.
11. In 2009 when the request was submitted, RV sales and service were considered a Conditional Use. Under the current development code, because the development code changed in December 2012, RV sales and service are now permitted outright in the C-1 zoning district. The property is relatively flat and does not contain any trees or landscaping, except for grass. Past uses of the site have included seasonal Christmas tree sales and summer equestrian activities. Staff is not aware of any past commercial operations on the site, other than temporary uses.
12. For the reasons discussed in this Order, the Planning Commission did not approve the applicant's request to use the property without making improvements for a period of 36 months. Rather, the Commission voted 6-0 to approve a variance to use the property for a period not to exceed 12 months subject to conditions, including the applicant provide a performance guarantee for improvements to Beers Avenue.

Chapter 17.42 – C-1 Central Business District

13. The use of the site for recreational vehicle sales and service in buildings with up to 30,000 square feet is a permitted use in the Central Business District zoning district. The proposal is not for a new use, but for expansion of the sales lot on to an undeveloped lot.

14. Section 17.42.30 states the development standards for the C-1 Central Business District zoning district. The subject lot contains 0.33 acres. Development of the site will require adherence to applicable setbacks in Chapter 17.98 for parking and storage and a minimum 10 percent of the site (1,430 square feet) in landscaping.

Chapter 17.84 – Improvements Required with Development

15. Section 17.84.30 contains requirements for pedestrian and bicyclist improvements. Currently the site contains frontage improvements along Proctor Blvd. and an unimproved frontage with a gravel shoulder along Beers Avenue. Development of the site will require right-of-way improvements along Beers Ave. and a ½-street improvement including construction of street paving, curb, five foot planter strip with street trees, and a five foot sidewalk. The applicant provided an Engineer’s estimate detailing the cost of these improvements.
16. Section 17.84.60 has requirements for public utility extensions. Development of the site will require connection to an approved stormwater system to accommodate onsite runoff and connection to other utilities as necessary.

Chapter 17.92 – Landscaping And Screening

17. Chapter 17.92 contains standards for landscaping and screening. Section 17.92.20 requires that property zoned Central Business District shall have at least 10 percent of the site landscaped (1,430 square feet).
18. Section 17.92.50 specifies the types and sizes of plant materials for landscaped areas. Development of the site will require installation of street trees spaced 30 feet on center along the west side of Beers Avenue. The site abuts approximately 130 feet of Beers Avenue requiring at least four street trees.

Chapter 17.98 – Parking, Loading, And Access Requirements

19. Section 17.98.20 contains off-street parking requirements. The applicant is proposing to use the site for storage of RV’s for sale rather than employee or customer parking.
20. Section 17.98.100 contains specifications for driveways. The applicant does not propose site improvements and a driveway to Beers Avenue is not proposed. Staff assumes the site would be accessed from the existing Fred’s RV Auto Marine sales lot when it is developed.
21. Section 17.98.130 specifies that parking areas, driveways, aisles and turnarounds shall be paved. Development of the site requires paving and installation of a stormwater system.

Chapter 17.66 – Adjustments and Variances

22. Section 17.66.60 specifies that variances are a means of requesting a complete waiver or major adjustment to certain development standards. The applicant requests a Special Variance to allow use of a vacant unimproved lot for parking RV’s for sale. The applicant is seeking to use the lot without installing required site improvements including paved surfacing, landscaping, and completing frontage improvements. The applicant requests

approval to use the site for RV sales and service for a period of 36 months using business revenue generated from additional RV sales. The applicant also submitted an Engineer's estimate (Exhibit C) to construct these improvements. The variance request requires a Type III procedure. The Sandy Development Code Chapter 17.66 contains two types of variances: a standard variance and a special variance. The standard variance criteria included below for a comparison to the special variance. These criteria are intended to address circumstances primarily related to unique topographic considerations of a specific property that makes development of the property impossible if current code is applied. The special variance does not include these same limiting criteria.

23. The Planning Commission determines the subject property is similar to a number of other vacant properties in the downtown and elsewhere in Sandy. The Commission also concludes that the property does not contain any unique characteristics which would justify granting a standard variance and economic considerations or the applicant's ability to fund necessary improvements are not a factor in evaluating variance requests.
24. Section 17.66.80(A.)(1.) specifies that the unique nature of the proposed development is such that the intent and purpose of the regulations and of the provisions to be waived will not be violated by permitting a variance. The applicant proposes using an unimproved lot in the central business district to display recreational vehicles for sale. As noted above, the proposed use is permitted outright in this zoning district under current code. Improvements required with this use include paving, landscaping, installation of a stormwater management system, and improvements to the unimproved Beers Avenue frontage. The applicant is requesting a waiver of these requirements for up to three years. The Planning Commission determines the subject property is similar in nature to a number of vacant unimproved lots both inside and outside of the downtown area. The regulations requested to be temporarily waived are generally intended to improve the physical appearance of the site with paving and landscaping and environmental aspects with stormwater quantity and quality improvements.
25. Section 17.66.80(A.)(2.) specifies that the authorization of the special variance will not be materially detrimental to the public welfare and will not be injurious to other property in the area when compared with the effects of development otherwise permitted. The Commission determines that it is unlikely that allowing a waiver of these improvements would be detrimental to the public welfare; however, it could be argued that using the unimproved lot for RV sales could have a negative impact on other properties in the area compared to developing the property as required. Only two property owners within close distance of the proposed unimproved lot commented on the proposal and are in favor of the variance.
26. Section 17.66.80(B.) specifies that the variance approved is the minimum variance needed to permit practical compliance with a requirement of another law or regulation. The applicant is requesting a waiver of all improvement requirements and to use the entire vacant parcel for a period of up to three years. The Commission discussed the applicant's request and concluded that limiting the variance not to exceed 12 months and requiring the applicant to provide a financial guarantee for the street frontage improvements, but not onsite improvements, was in the best interest of the City.

27. Section 17.66.80(C.) specifies that when restoration or replacement of a nonconforming development is necessary due to damage by fire, flood, or other casual or natural disaster, the restoration or replacement will decrease the degree of the previous noncompliance to the greatest extent possible. This criterion is not applicable to this request.

DECISION

For the reasons described above, the request by Fred's RV Auto Marine Inc. to obtain a special variance to use a vacant unimproved lot (Tax Lot 3900) to display and sell RV's for up to 36 months is modified to allow use of the unimproved property for RV sales for a maximum period of 12 months with the conditions listed below.

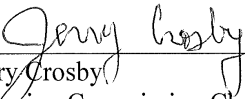
CONDITIONS OF APPROVAL

A. Prior to use of the site, the applicant shall complete the following:

1. Provide a performance guarantee equal to 110% of the cost of constructing frontage improvements along Beers Ave. as specified in Section 17.100.340 of the Sandy Municipal Code.

B. General Conditions of Approval

1. Approval of the Special Variance expires 12 months from the date of issuance of this Final Order (December 19, 2015). Any use of the property for RV sales or any other use beyond this date requires submittal and approval of a new variance application or design review approval and completion of required onsite improvements.
2. If frontage improvements along Beers Ave. are not completed by the applicant within the 12 month approval period, the City may collect on the performance guarantee causing these improvements to be completed.
3. The applicant shall receive all necessary permits and approvals for all work on and adjacent to the subject property prior to construction, including frontage and onsite improvements.
4. Approval of this use may be revoked if conditions of approval are not met. Approval does not grant authority for the unrestricted use of the site. Any use of the site may be prohibited until such time as all required improvements are completed.
5. Any conditions or regulations required by Clackamas County, Fire District No. 72, or state or federal agencies are hereby made a part of this permit and any violation of these conditions and/or regulations or conditions of this approval will result in the review of this permit and/or revocation.


Jerry Crosby
Planning Commission Chair

12/19/14
Date

RIGHT OF APPEAL

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1. An identification of the decision sought to be reviewed, including the date of the decision;
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3. The specific grounds relied upon for review;
4. If de novo review or review by additional testimony and other evidence is requested, a statement relating the request to the factors listed in Chapter 17.28.50; and Payment of required filing fees.

Exhibit M

When an inconsistency exists between criteria in a development code, courts will defer to a local government's interpretation if it is "plausible" considering the express language of the relevant provisions and the purposes behind them. This standard recognizes that, in many cases, no single interpretation of inconsistent code provisions is correct to the exclusion of all other interpretations. As such, the planning commission could consider another interpretation of 17.90.110(J), even if it is not necessarily the "most plausible."

In this case, the inconsistency involves the phrase "except as specifically authorized as a permitted accessory use" and the fact that any inconsistency is resolved in favor of Chapter 17.90's criteria. Staff and the city attorney believe that the most plausible interpretation is the one offered above: the code at 17.42.10(C)(8) specifically authorizes outdoor storage not exceeding 10 percent as a permitted accessory use. Therefore, pursuant to that interpretation, 17.90.110(J) prohibits outdoor storage exceeding 10 percent, as is the case in this application.

However, another interpretation could be that the phrase "except as specifically authorized as a permitted accessory use" means not as code language specifically authorizes, but instead as the planning commission or city council specifically authorize in a given case. Under that interpretation, the planning commission could specifically authorize outdoor storage in an amount that would exceed 10 percent, assuming the planning commission otherwise believed that amount complied with other relevant criteria in Chapter 17.90. This interpretation finds some support in the fact that Chapter 17.42.20(A)(3) allows storage in excess of 10 percent as a minor conditional use.

FUNTIME RV, INC. SITE DEVELOPMENT

NE BEERS AVE AND PROCTOR BLVD, SANDY, OREGON

Exhibit N

GENERAL NOTES:

- ALL WORK AND MATERIALS SHALL CONFORM TO THESE PLANS AND THE APPLICABLE PROVISIONS OF THE CITY OF SANDY PUBLIC WORKS STANDARDS.
- IN ORDER TO PROTECT UNDERGROUND FACILITIES, EXCAVATORS PERFORMING THE WORK SET FORTH ON THESE PLANS MUST COMPLY WITH THE PROVISIONS OF ORS 757.541 TO 757.571 (REQUIRES CONSTRUCTOR TO NOTIFY UTILITIES AT LEAST 48 HOURS, BUT NOT MORE THAN 10 BUSINESS DAYS, PRIOR TO ANY EXCAVATION).
- THE LOCATION OF EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND SHOWN FOR INFORMATION PURPOSES ONLY. THE CONTRACTOR SHALL HAVE ALL UTILITIES LOCATED PRIOR TO COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION. ADDITIONAL UNDERGROUND UTILITIES MAY EXIST.
- VERTICAL DATUM: ELEVATION ESTABLISHED USING GPS, NAVD 88 DATUM AND HOLDING OPUS SOLUTION COMPUTED USING GEOID28.
- TRENCHES WITHIN THE RIGHTS OF WAY SHALL BE BACKFILLED WITH AN APPROVED GRANULAR MATERIAL CONFORMING TO APWA CLASS B SPECIFICATIONS.
- TRENCHES OUTSIDE OF RIGHTS OF WAY MAY BE BACKFILLED IN ACCORDANCE WITH NATIVE MATERIAL AND COMPACTION SPECIFICATIONS FOR APWA CLASS A BACKFILL.
- VEGETATION AND TOPSOIL ARE TO BE STRIPPED TO MINERAL EARTH (AND INSPECTED BY THE PROJECT ENGINEER OR GEOTECHNICAL ENGINEER) PRIOR TO PLACEMENT OF FILL OR BASE MATERIALS.
- IN ADDITION TO ANY REQUIRED COMPACTION TESTING, THE CITY MAY REQUIRE A PROOF ROLL WITH A FULLY LOADED 10-YARD DUMP TRUCK TO CHECK SUBGRADE PRIOR TO PLACEMENT OF LOCAL REGIONAL AND AGAIN AT THE COMPLETION OF THE PLACEMENT OF THE BASE ROCK PRIOR TO PAVING THE FIRST LIFT OF ASPHALT.
- ASPHALTIC CONCRETE MIX IS TO BE BATCHED FROM A MIX FORMULA APPROVED BY OSHD FOR MATERIAL USED. PAVING CONTRACTOR SHALL PROVIDE A CERTIFICATE OF COMPLIANCE FROM ASPHALT PAVEMENT PLANT.
- SUBSEQUENT SETTLEMENT OR CRACKING OF FINISHED SURFACE WITHIN THE WARRANTY PERIOD SHALL BE CONSIDERED TO BE A FAILURE OF THE SUBGRADE AND REPAIRED AT NO COST TO THE CITY AND IN A MANNER ACCEPTABLE TO THE CITY. A PERFORMANCE BOND IS REQUIRED TO GUARANTEE REPAIRS UNDER THE WARRANTY PERIOD.
- THE CONTRACTOR SHALL CONTROL TRAFFIC THROUGH THE PROJECT SITE IN CONFORMANCE WITH THE LATEST EDITION OF "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", "OREGON SUPPLEMENTS". THE CONTRACTOR SHALL AT ALL TIMES MAINTAIN LOCAL ACCESS FOR HOMEOWNERS ALONG THE PROJECT SITE.
- THE CONTRACTOR AND/OR SUB-CONTRACTOR SHALL HAVE A MINIMUM OF ONE (1) SET OF APPROVED CONSTRUCTION PLANS ON THE JOB SITE AT ALL TIMES DURING THE CONSTRUCTION PHASES.
- CONTRACTOR SHALL REMOVE AND DISPOSE OF TREES, STUMPS, BRUSH, ROOTS, TOPSOIL AND OTHER MATERIAL ENCOUNTERED DURING THE CONSTRUCTION OF THE ROADWAY AND WHERE INDICATED ON THE PLANS. MATERIAL SHALL BE DISPOSED OF IN ACCORDANCE WITH LOCAL, REGIONAL AND STATE REGULATIONS AT FACILITIES AUTHORIZED TO ACCEPT SUCH MATERIAL.
- CONTRACTOR SHALL CAREFULLY MAINTAIN BENCHMARKS, PROPERTY CORNERS, MONUMENTS AND OTHER REFERENCE POINTS. IF SUCH POINTS ARE DISTURBED OR DESTROYED BY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND PAY FOR THEIR REPLACEMENT BY EMPLOYING A PROFESSIONAL LAND SURVEYOR TO RESET PROPERTY CORNERS AND OTHER SUCH MONUMENTS.
- EXCESS EXCAVATED MATERIAL SHALL BE HAULED AND DISPOSED OF AT SITES PROVIDED BY THE OWNER AND APPROVED PURSUANT TO AN APPROPRIATE GRADING PERMIT. FILL SITES SHALL BE LEVELED AND GRADED TO DRAIN. THE CONTRACTOR SHALL CORRECT ANY FILL RELATED CONDITIONS.
- FINAL CLEANUP - PRIOR TO FINAL ACCEPTANCE AND PAYMENT, THE CONTRACTOR SHALL CLEAN THE WORK SITE AND ADJACENT AREAS OF ANY DEBRIS, DISCARDED ASPHALTIC CONCRETE MATERIAL OR OTHER ITEMS DEPOSITED BY THE CONTRACTORS PERSONNEL DURING THE PERFORMANCE OF THIS CONTRACT.
- A PERMIT IS REQUIRED FOR ANY WORK IN THE PUBLIC RIGHT-OF-WAY, CONTACT THE CITY OF SANDY PUBLIC WORKS DEPARTMENT OR VISIT www.ci.sandy.or.us FOR PERMIT APPLICATION FORMS.
- ALL WORK MUST COMPLY WITH CITY OF SANDY GRADING AND EROSION CONTROL PERMIT #xxxx GR/EC ISSUED XX/XX/XX.
- ALL WORK MUST COMPLY WITH THE CITY OF SANDY'S EROSION CONTROL STANDARDS. CONTRACTOR TO SCHEDULE INSPECTIONS 48 NORMAL BUSINESS HOURS IN ADVANCE.
- IF, AT ANYTIME DURING THE ON-SITE CONSTRUCTION, THE CONTRACTOR OBSERVES AN ENVIRONMENTAL IMPACT ON-SITE SUCH AS OIL TANKS, CONTAMINATION RESIDUE OR HAZARDOUS MATERIALS, SPILLAGE, ETC., IT MUST BE REPORTED, INSPECTED, AND TREATED IN CONFORMANCE WITH THE APPROPRIATE AGENCY'S REQUIREMENTS.

UTILITY NOTES:

- THE CONTRACTOR IS REQUIRED TO OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF SANDY AND OREGON DEPARTMENT OF TRANSPORTATION PRIOR TO BEGINNING CONSTRUCTION.

NOTIFICATION:

GENERAL - THE CONTRACTOR SHALL, AS A MINIMUM, COORDINATE THE PROPOSED CONSTRUCTION ACTIVITIES WITH THE OWNER AND LOCAL PUBLIC AGENCIES, UTILITIES AND COMPANIES DURING CONSTRUCTION TO AVOID DAMAGE AND TO PREVENT THE INTERRUPTION OF SERVICES AND UTILITIES TO RESIDENTS AND BUSINESSES:

OREGON UTILITY NOTIFICATION CENTER
1-503-248-6699

CLACKAMAS COUNTY
(503) 353-4400

CITY OF SANDY ENGINEERING DEPT. (503) 668-5533
BUILDING DIVISION INSPECTIONS (503) 668-6941

PORTLAND GENERAL ELECTRIC
(503) 226-8111

VERIZON NORTHWEST (TELEPHONE)
1-800-483-4100

NORTHWEST NATURAL GAS
(503) 226-4211

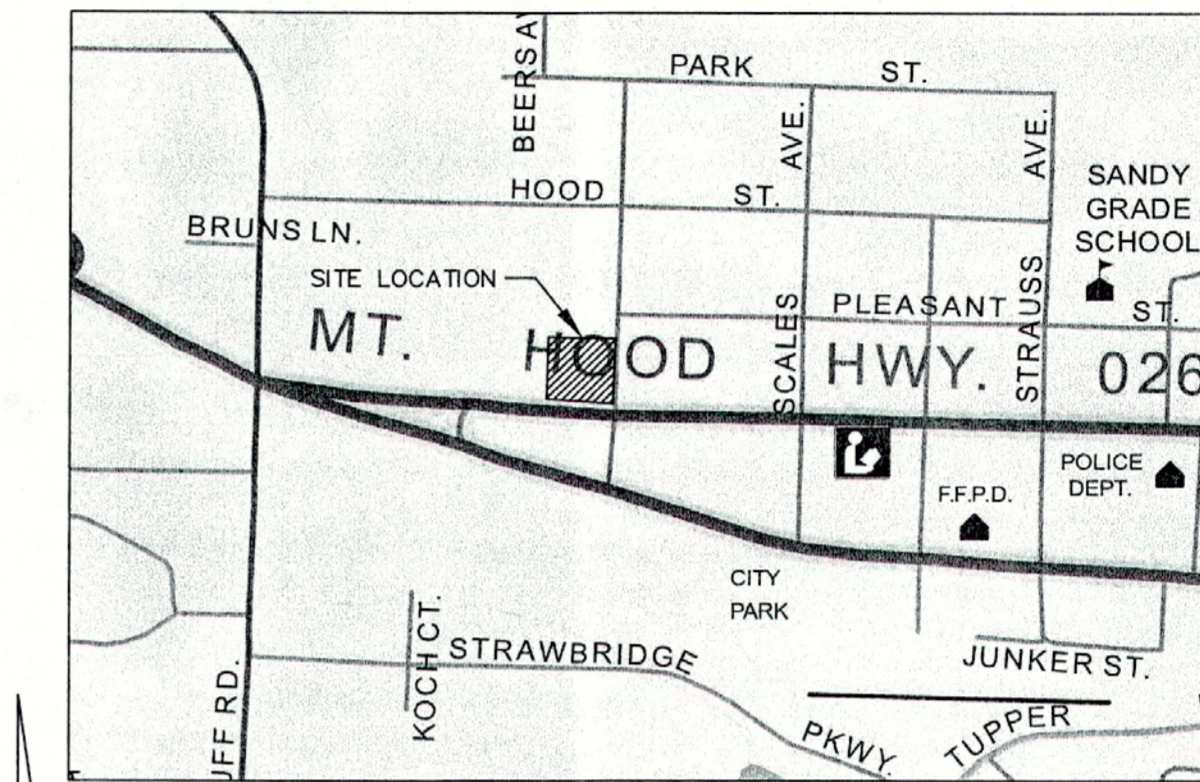
CHARTER COMMUNICATIONS (CABLE TV)
1-866-731-5420

STREET AND STORM DRAINAGE NOTES:

- STREET AND STORM DRAIN IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF SANDY STANDARDS AND REQUIREMENTS.
- ALL TRENCH EXCAVATION SHALL CONFORM TO STANDARD STORM SEWER SPECIFICATIONS AND SHALL BE UNCLASSIFIED.
- PIPE BEDDING AND PIPE ZONE SHALL CONFORM TO THE EXCAVATION AND BACKFILL DETAILS, AND SHALL BE 3/4"-0" CRUSHED ROCK.
- THE CITY REQUIRES COMPACTION WITHIN THE RIGHT-OF-WAY TO BE 95% OF MAXIMUM DENSITY AS DETERMINED BY AASHTO T-180. CONTRACTOR TO DETERMINE TYPE OF EQUIPMENT AND METHOD USED TO ACHIEVE REQUIRED COMPACTION.
- TRENCH BACKFILL OUTSIDE OF RIGHTS OF WAY OR PAVED AREAS MAY BE EXCAVATED TRENCH MATERIAL. TRENCH BACKFILL IN PAVED AREAS SHALL BE AN APPROVED GRANULAR MATERIAL.
- MATERIAL IN SOFT SPOTS WITHIN THE ROADWAY SHALL BE REMOVED TO THE DEPTH REQUIRED TO PROVIDE A FIRM FOUNDATION AND SHALL BE REPLACED WITH 1-1/2"-0" CRUSHED ROCK. THE ENTIRE SUBGRADE SHALL BE THOROUGHLY COMPACTED TO 95% AASHTO T-180.
- CONTRACTOR SHALL NOTIFY THE ENGINEER AND CITY OF SANDY WHEN SUBGRADE IS COMPLETE AND 24 HOURS PRIOR TO PLACEMENT OF ROCK BASE MATERIAL AND 24 HOURS PRIOR TO FINAL PAVING FOR AN INSPECTION OF THE WORK. FAILURE TO DO SO WILL MAKE ANY SUBGRADE FAILURE PROBLEMS THE RESPONSIBILITY OF THE CONTRACTOR. A PROOF ROLL WITH A FULLY LOADED 10-YARD DUMP TRUCK MAY BE REQUIRED TO CHECK SUBGRADE COMPACTION PRIOR TO PLACEMENT OF ROCK SUBBASE AND AGAIN AT THE COMPLETION OF THE PLACEMENT OF THE BASE ROCK PRIOR TO PAVING THE FIRST LIFT OF ASPHALT.
- ALL SAWCUT JOINTS SHALL BE STRAIGHT, TACKED AND SAND SEALED UPON PAVING.
- THE CITY REQUIRES A SUCCESSFUL MANDREL PULL ON ONE SECTION OF EACH DIAMETER OF STORM PIPE USED.
- ASPHALT COMPACTION SHALL BE PERFORMED USING NUCLEAR GAUGE. THE RICE DENSITY TESTS SHALL BE 91% FOR THE BASE LIFT AND 92% FOR THE TOP LIFT IN ACCORDANCE WITH ODOT TM305 OR AASHTO T-209. SUBMIT TESTING REPORTS TO THE CITY.

GRADING NOTES:

- FILLS SHALL BE COMPACTED TO 95 PERCENT OF MAXIMUM DENSITY IN THE BUILDING ENVELOPE AND 92 PERCENT OF MAXIMUM DENSITY ON THE REMAINDER OF THE LOT AS DETERMINED BY ASTM TEST SD 1557-91, METHOD A, OR AN EQUIVALENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING THE NECESSARY ARRANGEMENTS FOR SUCH TESTING AND FOR SUPPLYING THE RESULTS TO THE CITY OF SANDY.
- ALL CUTS SHALL BE MADE CONSISTENT WITH THE DETAILS NOTED IN THE PLANS. NO CUT SHALL EXCEED A GRADE OF 2 HORIZONTAL TO 1 VERTICAL UNLESS APPROVED BEFOREHAND BY THE ENGINEER AND THE CITY OF SANDY.
- APPROPRIATE BENCHING OF FILLS IS REQUIRED FOR FILLS OVER 5 FEET IN HEIGHT ON SLOPES IN EXCESS OF 5 HORIZONTAL TO 1 VERTICAL. BENCHING MUST BE DONE AS PER THE APPROVED PLANS. THE CITY OF SANDY SHALL INSPECT BENCHES PRIOR TO FILL PLACEMENT.
- CUT AND FILL SLOPES SHALL BE PROTECTED FROM EROSION. SUCH CONTROL MAY CONSIST OF APPROPRIATE REVEGETATION OR OTHER ACCEPTABLE MEANS AND METHODS. EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO EARTHWORK OR SITE STRIPPING.
- THE CONTRACTOR SHALL COORDINATE WITH CITY PERSONNEL BY CALLING (503) 668-6941 OR (503) 668-5533 ANYTIME FOR REQUIRED INSPECTIONS AT THE FOLLOWING STAGES OF CONSTRUCTION:
- THE CONTRACTOR SHALL NOT COMPACT SOIL/SUB-GRADE UNDER STORMTECH CHAMBERS TO PRESERVE INFILTRATION CAPACITY.



VICINITY MAP
SCALE: 1"=600'

SHEET INDEX:

- C1 - COVER SHEET
- C2 - EXISTING CONDITIONS & DEMO PLAN
- C3 - SITE, GRADING, & LANDSCAPING PLAN
- C4 - CONCEPT RV DISPLAY LAYOUT
- C5 - BEERS AVENUE PLAN & PROFILE
- C6 - UTILITY PLAN
- C7 - DETAILS SHEET - CITY OF SANDY
- C8 - DETAILS SHEET - CITY OF SANDY
- C9 - DETAILS SHEET - ODOT
- C10 - DETAILS SHEET - SITE
- C11 - DETAILS SHEET - DETENTION FACILITY



EXPIRES: 06/30/21
SIGNATURE DATE: _____
PRELIMINARY

DATE:	NO.	REVISION

DRAWN:	DESIGNED:	CHECKED:
SCALE: AS SHOWN	DATE: JULY 23, 2019	
PROJECT NO. E17-060		

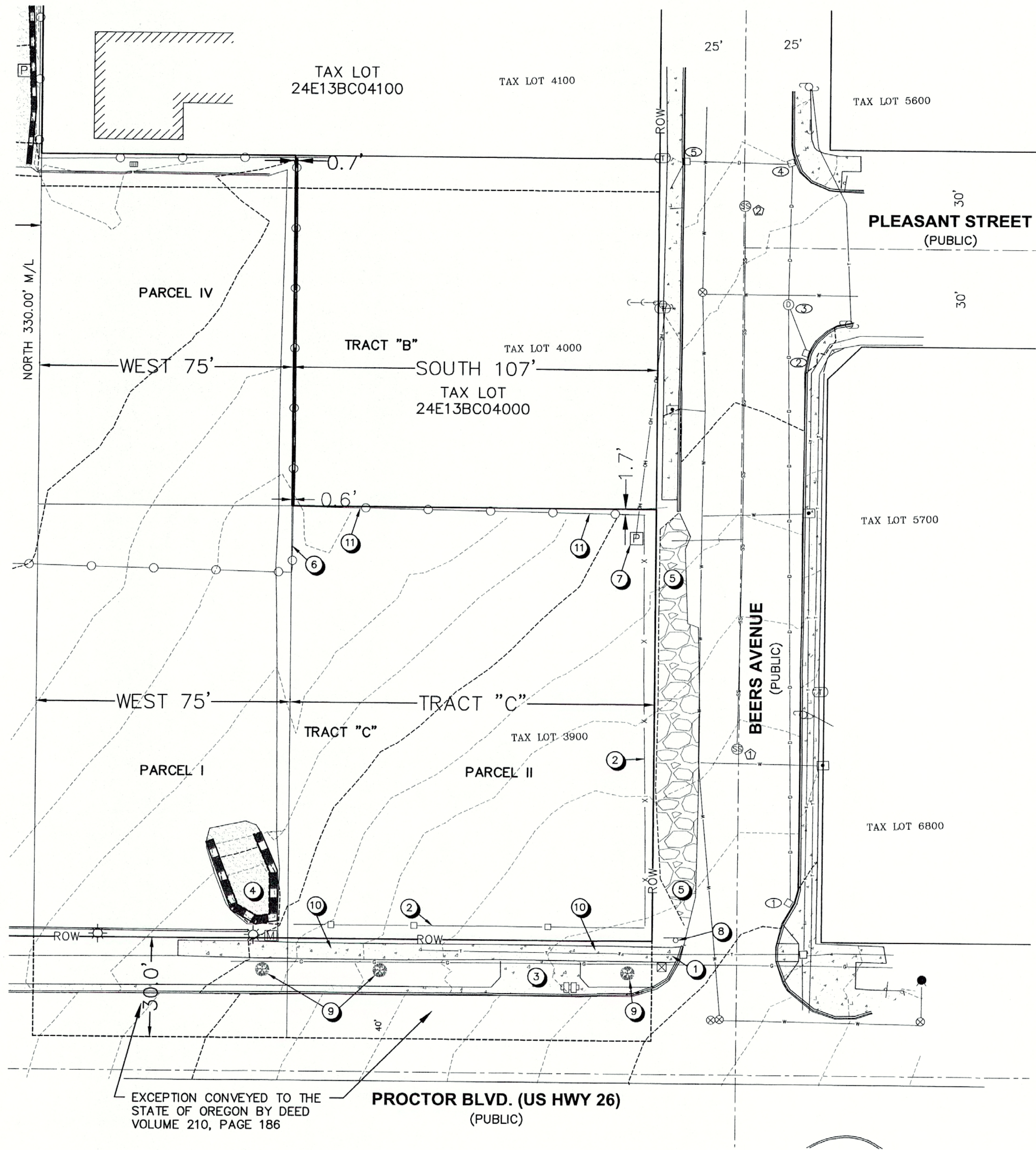
FDG
Firwood Design Group, LLC
SURVEYING • ENGINEERING • PLANNING

359 E. HISTORIC COLUMBIA RIVER HWY
TROUTDALE, OREGON 97069
BUS: (503) 668-3737 • FAX: (503) 668-3788

FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

COVER SHEET

C1
11

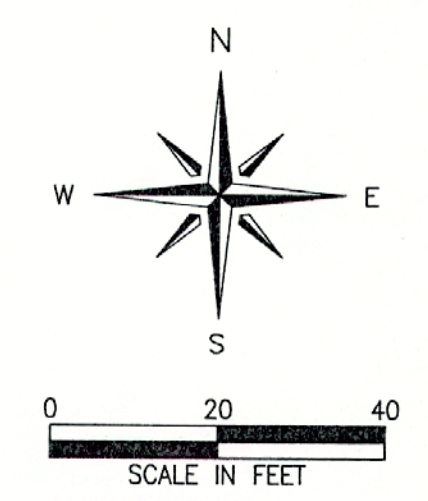


- ⊙ SANITARY MANHOLE
- ⊙ STORM MANHOLE
- ⊙ CLEANOUT
- CATCH BASIN
- FIRE HYDRANT
- WATER VALVE
- WATER METER
- ⊕ COMMUNICATION RISER
- ⊕ UTILITY POLE
- SIGN
- MAILBOX
- ✓ GUY ANCHOR
- ⊕ POWER POLE
- ⊙ DECIDUOUS TREE

- LEGEND**
- SEWER LINE
 - STORM LINE
 - WATERLINE
 - GAS LINE
 - UNDERGROUND COMMUNICATION
 - WOOD FENCE
 - WIRE FENCE
 - EDGE OF GRAVEL
 - MAJOR CONTOUR LINE
 - MINOR CONTOUR LINE
 - CHAINLINK FENCE
 - PROPERTY LINE
 - EDGE OF PAVEMENT
 - ROW
 - RIGHT OF WAY
- ▨ GRAVEL SURFACE
 - ▨ CONCRETE SURFACE
 - ▨ BUILDING

- KEY NOTES**
- 1 EXISTING RAMP AND CURB TO BE REMOVED AS NECESSARY FOR ADA RAMP CONSTRUCTION AND INTERSECTION IMPROVEMENTS
 - 2 EXISTING FENCE TO BE REMOVED
 - 3 EXISTING DRIVE ACCESS TO BE REMOVED FOR LANDSCAPING, ODOT MISC. PERMIT REQUIRED
 - 4 EXISTING CONCRETE VEHICLE RAMP TO BE REMOVED FOR VEHICLE ACCESS
 - 5 EXISTING PAVEMENT AND GRAVEL TO BE REMOVED FOR FRONTAGE IMPROVEMENTS
 - 6 EXIST CHAINLINK FENCE TO BE RELOCATED/REMOVED
 - 7 EXIST POWER POLE TO BE REMOVED
 - 8 RELOCATE STOP SIGN AS DIRECTED BY CITY
 - 9 PROTECT EXISTING TREES
 - 10 EXISTING SIDEWALK TO BE REMOVED AND REPLACED ALONG PROCTOR BLVD. FRONTAGE
 - 11 PROTECT EXISTING CHAIN LINK FENCE AND HEDGE TO REMAIN

- 1 RIM = 939.60'
IE 12" OUT NW. = 936.31'
- 2 RIM = 934.37'
IE 3" IN N. = 933.79'
IE 6" IN SE. = 941.09'
IE 3" IN SE. = 933.85'
IE 10" OUT W. = 931.91'
- 3 RIM = 934.45'
IE 10" IN SE. = 931.35'
IE 6" IN E. = 931.31'
IE 12" IN S. = 931.90'
IE 12" OUT N. = 930.80'
- 4 RIM = 933.00'
IE 12" IN S. = 930.32'
IE 12" OUT W. = 930.12'
- 5 RIM = 932.35'
IE 6" IN W. = 930.51'
IE 12" IN E. = 929.97'
IE 12" OUT N. = 929.52'
- 6 RIM = 938.22'
IE 8" OUT N. = 932.88'
- 7 RIM = 933.61'
IE 8" IN E. = 928.41'
IE 8" IN N. = 928.37'
IE 8" IN S. = 928.41'
IE 8" OUT W. = 928.29'



REGISTERED PROFESSIONAL ENGINEER
67149PE
FOR REVIEW
OREGON
JULY 14, 2019
KELLI A. GROVER
EXPIRES: 06/30/21
SIGNATURE DATE:
PRELIMINARY

DRAWN:	DESIGNED:	CHECKED:
SCALE: AS SHOWN	DATE: JULY 23, 2019	
PROJECT NO. E17-060		
DATE:	NO.	REVISION

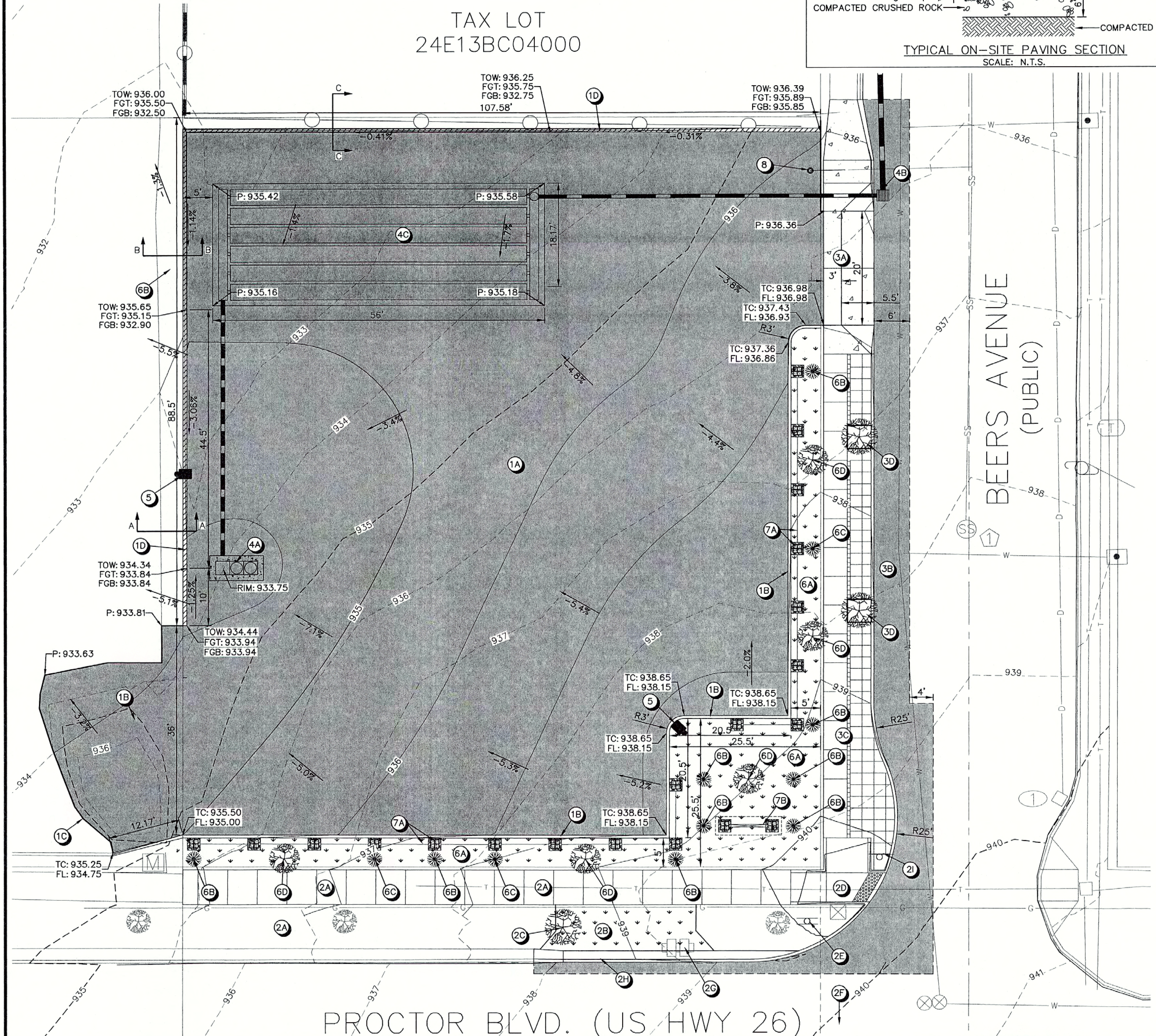
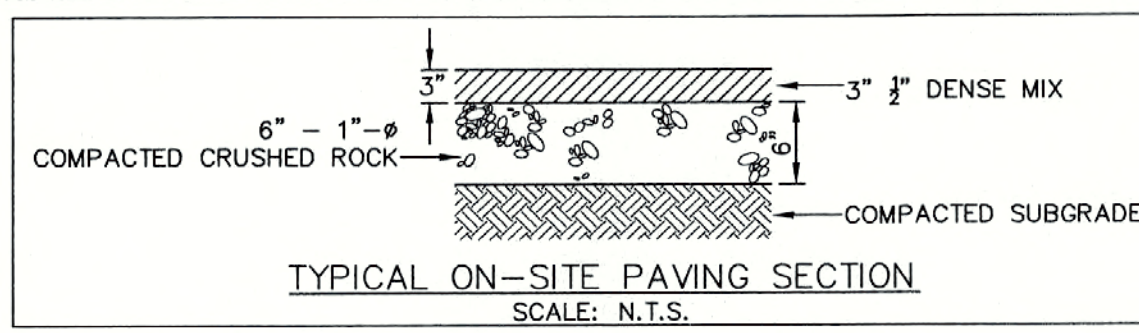
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359 E. HISTORIC COLUMBIA RIVER HWY
TRUXTONDALE, OREGON 97060
BUS: (503) 668-3737 • FAX: (503) 668-3788

FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

EXISTING CONDITIONS & DEMO PLAN

C2
11

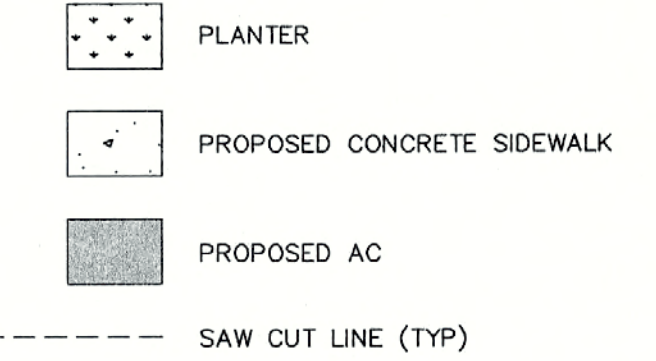
TAX LOT
24E13BC04000



KEY NOTES

- 1A CONSTRUCT NEW ASPHALT PAVING SURFACE PER SECTION THIS SHEET
- 1B CONSTRUCT STANDARD TYPE "C" CURB, SEE SHEET C7 FOR DETAIL
- 1C MATCH LIMITS OF EXIST AC ON ADJACENT PROPERTY FOR REMOVAL OF CONCRETE VEHICLE RAMP
- 1D CONSTRUCT 196 L.F. RETAINING WALL FROM EXISTING GROUND ELEVATION TO FINISHED GRADE ELEVATION
- 2A CONSTRUCT NEW 6" CONCRETE SIDEWALK. MATCH EXIST. CONCRETE SIDEWALK AT LIMITS OF REMOVAL OF EXISTING DRIVECUT. SEE DETAIL ON SHEET C9
- 2B INSTALL PLANTER IN-LINE WITH EXISTING PLANTER, ADJACENT TO SIDEWALK
- 2C PLANT ONE CHANTICLEER PEAR TREE, 7 FEET TALL AND/OR 1 1/2" CALIPER
- 2D CONSTRUCT SINGLE ADA ACCESS FROM SIDEWALKS ON NORTH SIDE OF PROCTOR BLVD AND WEST SIDE OF BEERS AVE TO CROSSWALK ACROSS BEERS AVENUE. SEE DETAIL VIEW ON THIS SHEET
- 2E INSTALL ODOT CROSSWALK CLOSED SIGN, SEE SHEET C9 FOR DETAIL
- 2F INSTALL ODOT NO CROSSWALK SIGN ON SOUTH SIDE OF PROCTOR BLVD., SEE SHEET C9 FOR DETAIL
- 2G ADJUST EXISTING MAILBOX TO PROPOSED FINISH GRADE IN SAME LOCATION AFTER CONSTRUCTION OF NEW CURB
- 2H INSTALL STANDARD CURB WITH CONCRETE APRON TO MATCH EXISTING, SEE SHEET C9 FOR DETAIL
- 2I RE-INSTALL EXISTING STOP SIGN
- 3A CONSTRUCT COMMERCIAL DRIVEWAY APPROACH. SEE DETAIL SHEET C7
- 3B WIDEN EXISTING AC SURFACE OF BEERS AVE. TO MEET EDGE OF CURB. SEE TYPICAL SECTION SHEET C5
- 3C CONSTRUCT NEW 8" CONCRETE SIDEWALK AND STANDARD 6" CURB. MATCH EXIST CONCRETE SIDEWALK AND CURB PAST PROPERTY LINE ON WEST SIDE OF BEERS AVE. SEE TYPICAL SECTION ON SHEET C5
- 3D INSTALL 48" X 48" TREE WELL WITH RED BARRON CRABAPPLE TREE, 7 FEET TALL AND/OR 1 1/2" CALIPER
- 4A INSTALL CONCRETE CATCHBASIN STORMFILTER. FOR DESIGN INFORMATION: SEE UTILITIES PLAN ON SHEET C6 AND CATCHBASIN DETAIL ON SHEET C11
- 4B INSTALL NEW STORMWATER CATCHBASIN WITH CONNECTIONS FROM PROPOSED STORMWATER DETENTION CHAMBERS AND TO DOWNSTREAM MANHOLE. SEE UTILITY PLAN ON SHEET C6
- 4C INSTALL 42 STORMTECH SC-310 CHAMBERS (300 L.F.) IN A SERIES OF 50-FT LENGTHS. SEE TECHNICAL SPECIFICATIONS AND INSTALLATION DETAILS ON SHEET C11
- 5 INSTALL AREA LIGHT ON POST
- 6A PLANT KINNICKINICK GROUNDCOVER AND PLACE BARK DUST
- 6B PLANT DWARF BURNING BUSH (EUONYMUS ALATUS 'COMPACTA'), 5 GAL
- 6C PLANT J.P.M. RHODODENDRON, 5 GAL
- 6D PLANT KOUSA DOGWOOD, 7 FEET TALL AND/OR 1 1/2" CALIPER
- 7A INSTALL DECORATIVE RAIL AND COLUMN FENCE, SEE DETAILS ON SHEET C10
- 7B FUTURE SIGN LOCATION "SANDY STYLE" SIGN WITH COLUMNS TO MATCH DECORATIVE FENCE COLUMNS AND METAL ROOF TO MATCH THE EXISTING FUNTIME RV BUILDING ROOF
- 8 RELOCATE SANITARY CLEANOUT TO PROPERTY LINE

LEGEND

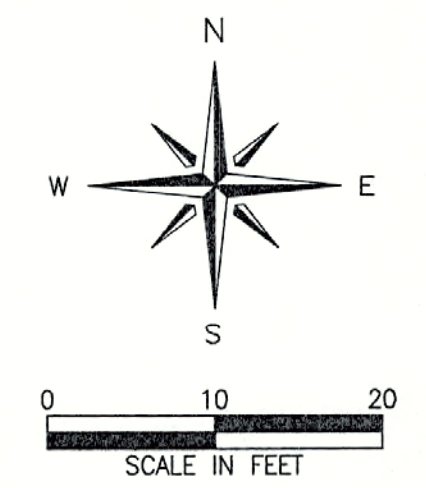
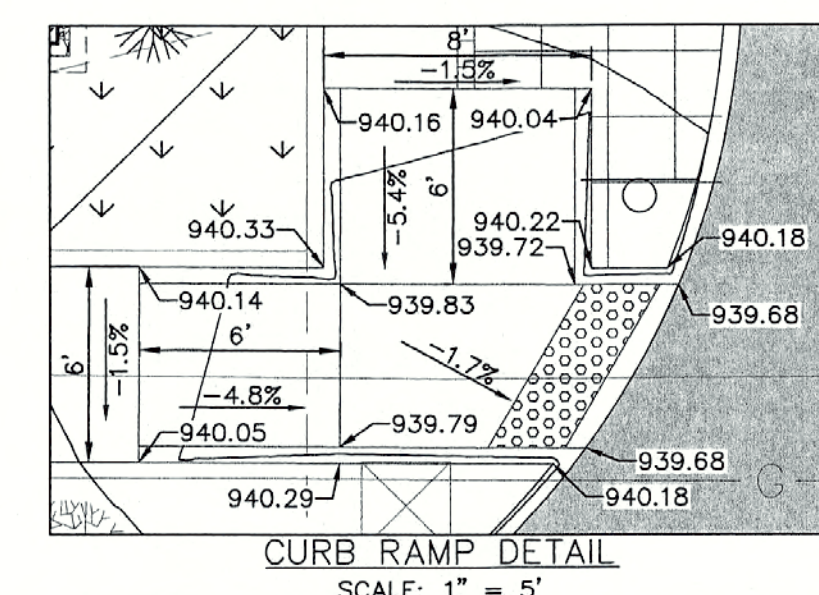
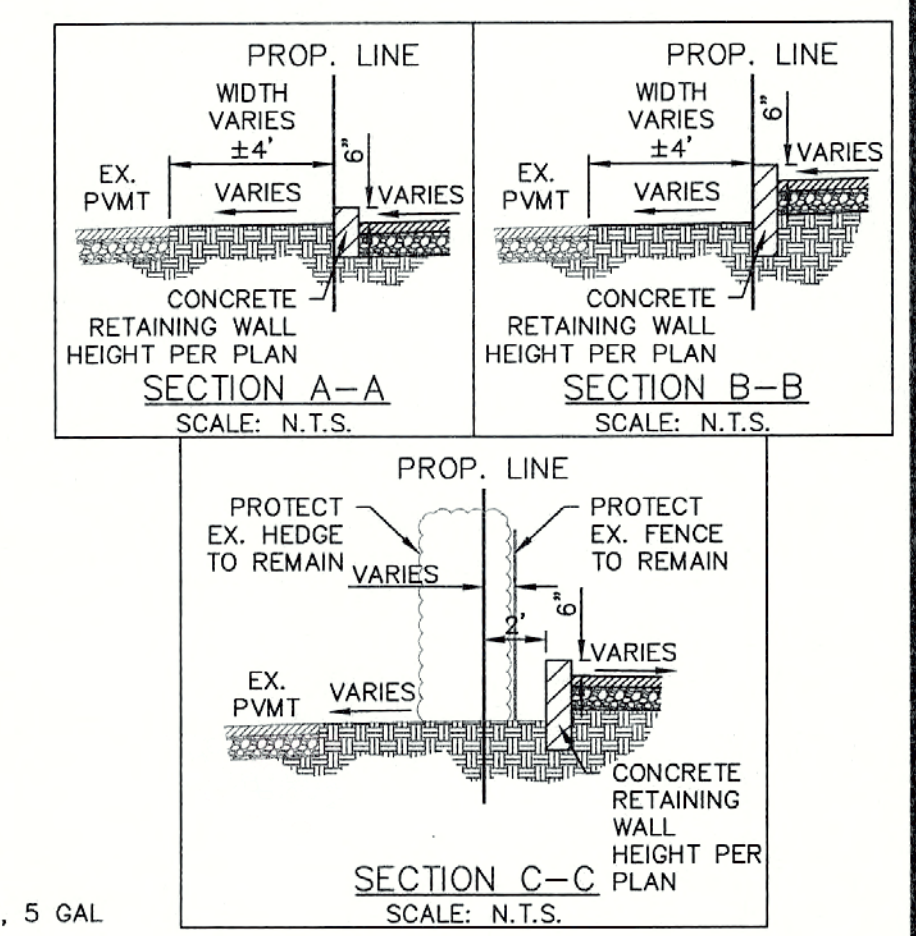


GRADING QUANTITIES (ESTIMATED):

CUT = 140 CY
FILL = 400 CY
PAVEMENT SECTION VOLUME = 445 CY
DETENTION SECTION VOLUME = 105 CY
NET = 290 CY EXPORT

LANDSCAPING AREAS:

SITE AREA: 13,985 SF
REQUIRED LANDSCAPED AREA (10% OF SITE AREA): 1,399 SF
6A PROPOSED LANDSCAPED AREA: 1,399 SF



REGISTERED PROFESSIONAL
ENGINEER
67149PE
OREGON
JAN 14, 2009
KELLI A. GROVER
EXPIRES: 06/30/21
SIGNATURE DATE:
PRELIMINARY

DATE:	NO.	REVISION

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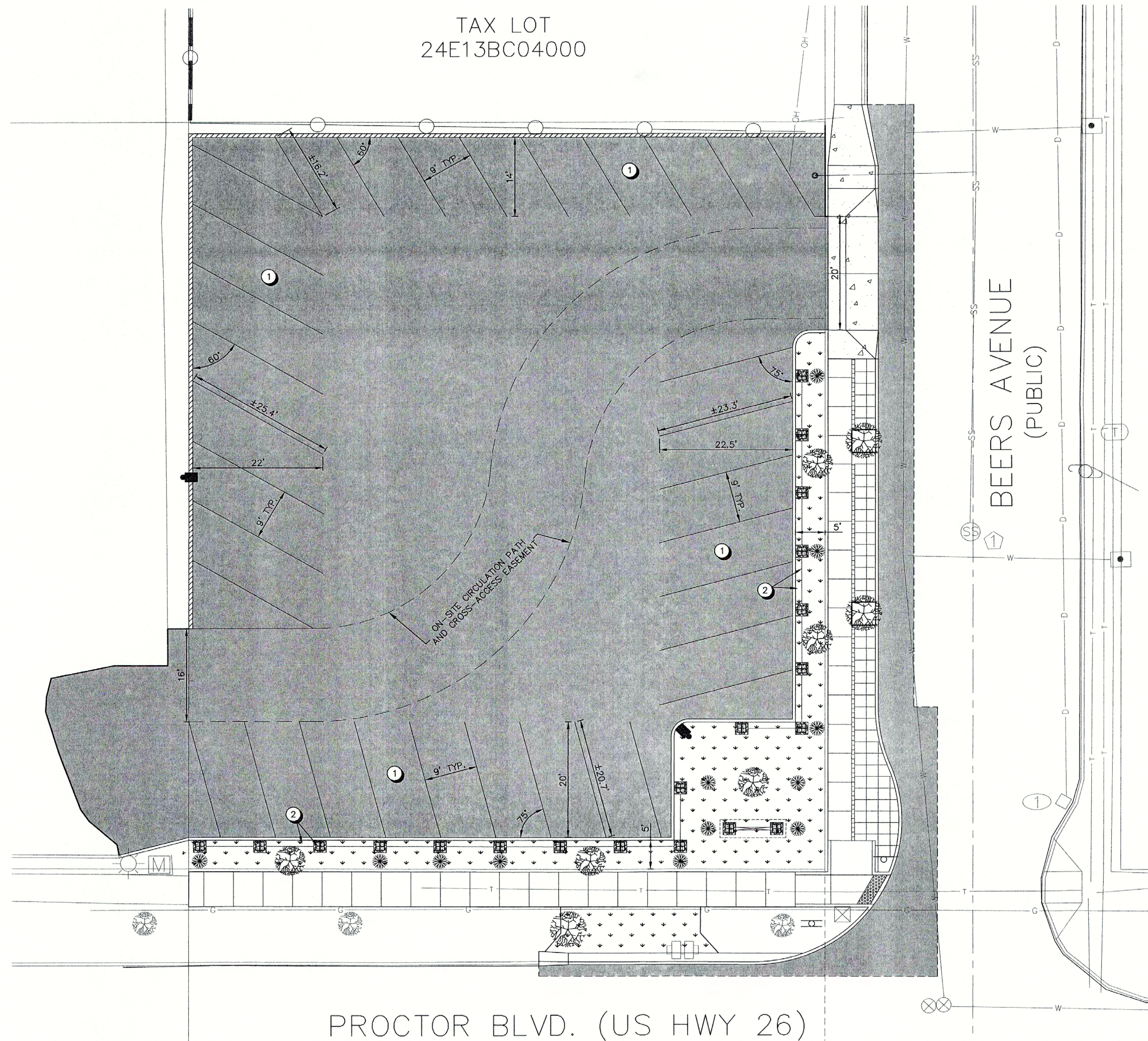
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FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

SITE, GRADING, & LANDSCAPING PLAN

C3
11

TAX LOT
24E13BC04000

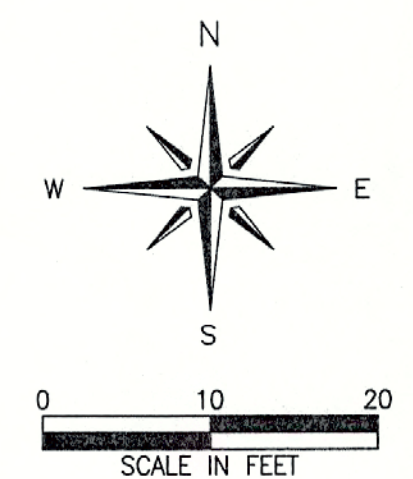


KEY NOTES

- ① CONCEPT RV DISPLAY SPACE (TYPICAL)
DISPLAY SPACES WILL NOT BE STRIPED
- ② DECORATIVE RAIL AND COLUMN FENCE

RV DISPLAY NOTES

1. THE LAYOUT SHOWN ON THIS PLAN IS CONCEPTUAL IN NATURE AND NOT INTENDED TO LIMIT THE SITE OWNER'S DISCRETION IN RECREATIONAL VEHICLE DISPLAY CONFIGURATION.
2. RECREATIONAL VEHICLES ON DISPLAY SHALL BE PLACED IN SUCH A MANNER AS TO NOT ENCROACH ON OR DAMAGE SITE LIGHTING FIXTURES.
3. LANDSCAPING IN THE 5' LANDSCAPING BUFFER WILL BE PROTECTED FROM ENCROACHMENT AND DAMAGE BY DISPLAY VEHICLES BY THE DECORATIVE RAIL AND COLUMN FENCE.



SIGNATURE DATE: _____
PRELIMINARY

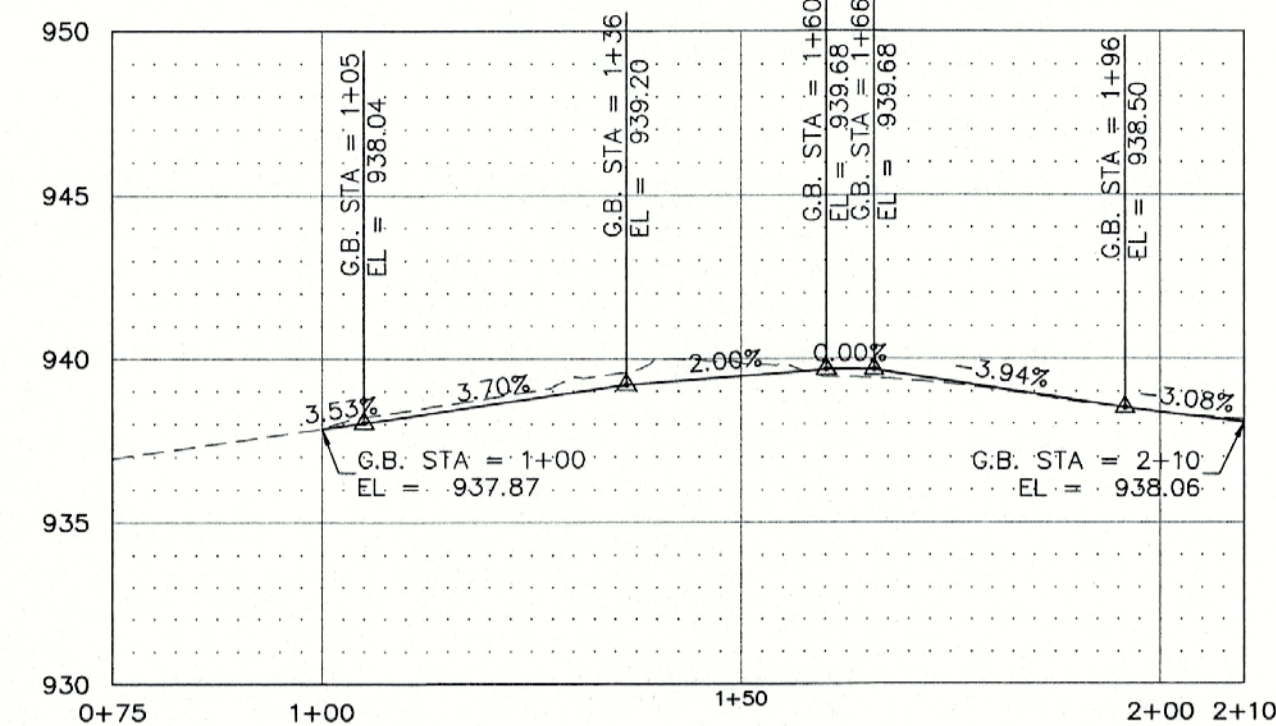
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SCALE: AS SHOWN	DATE: JULY 23, 2019	
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DATE:	NO.	REVISION

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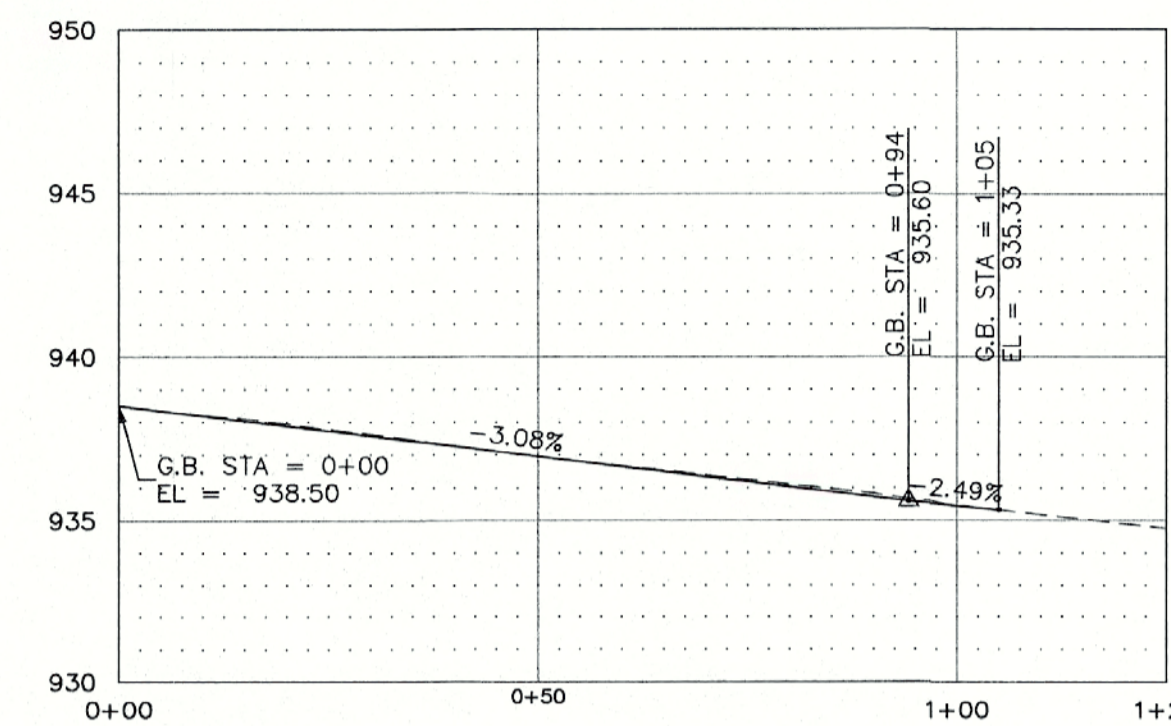
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NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

CONCEPT RV DISPLAY LAYOUT

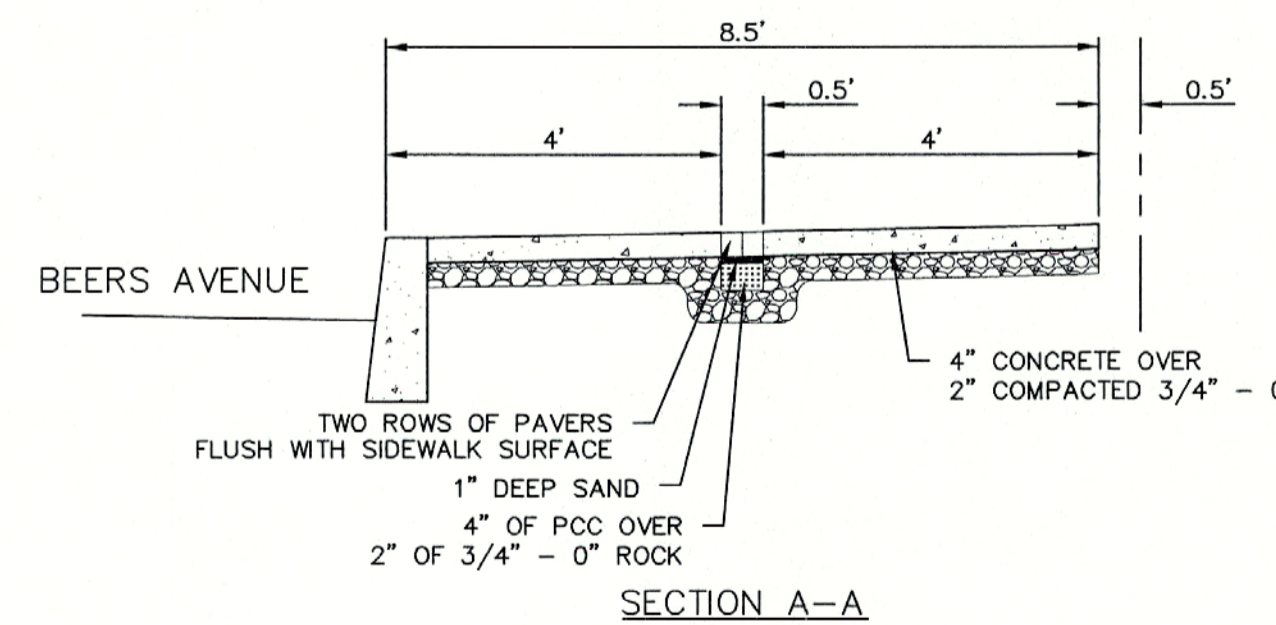
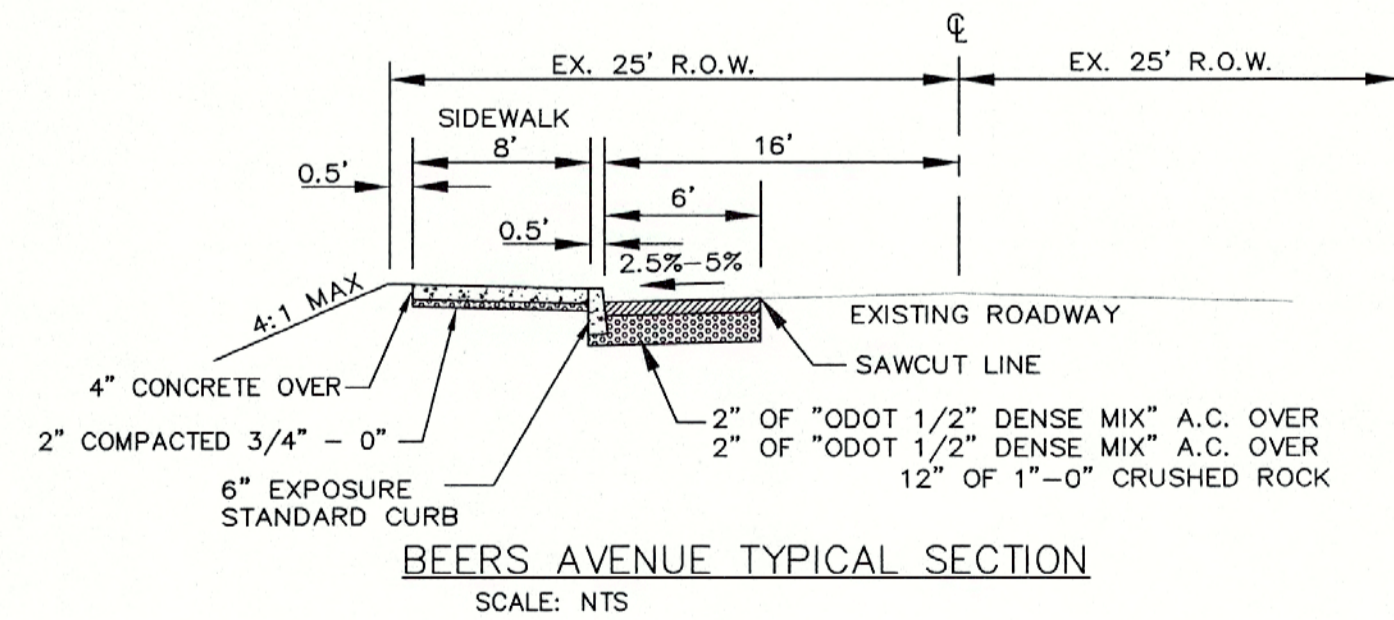
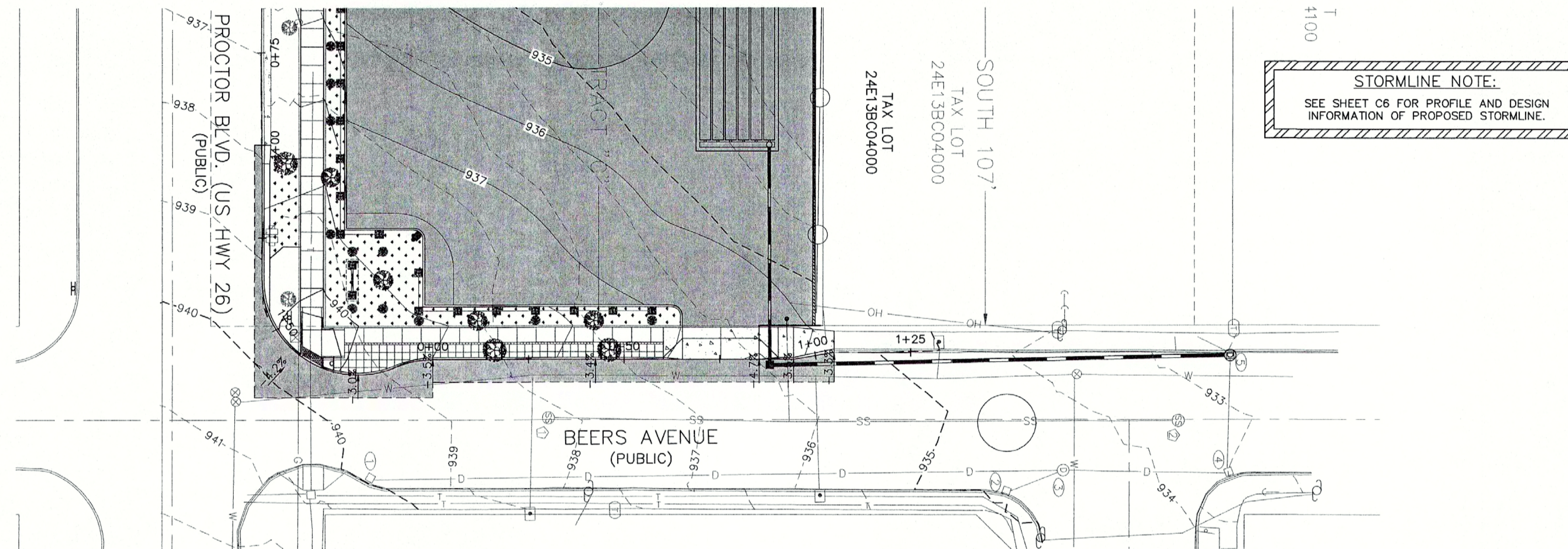
C4
11



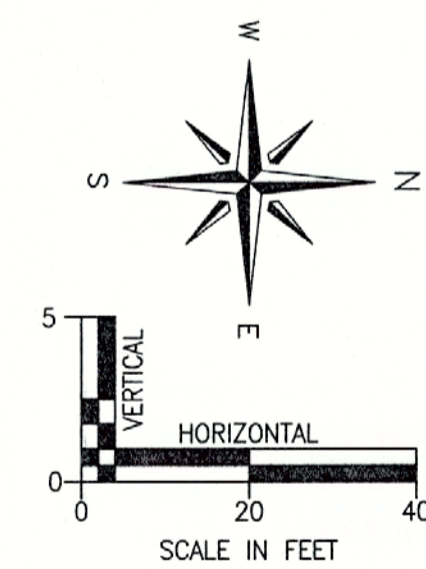
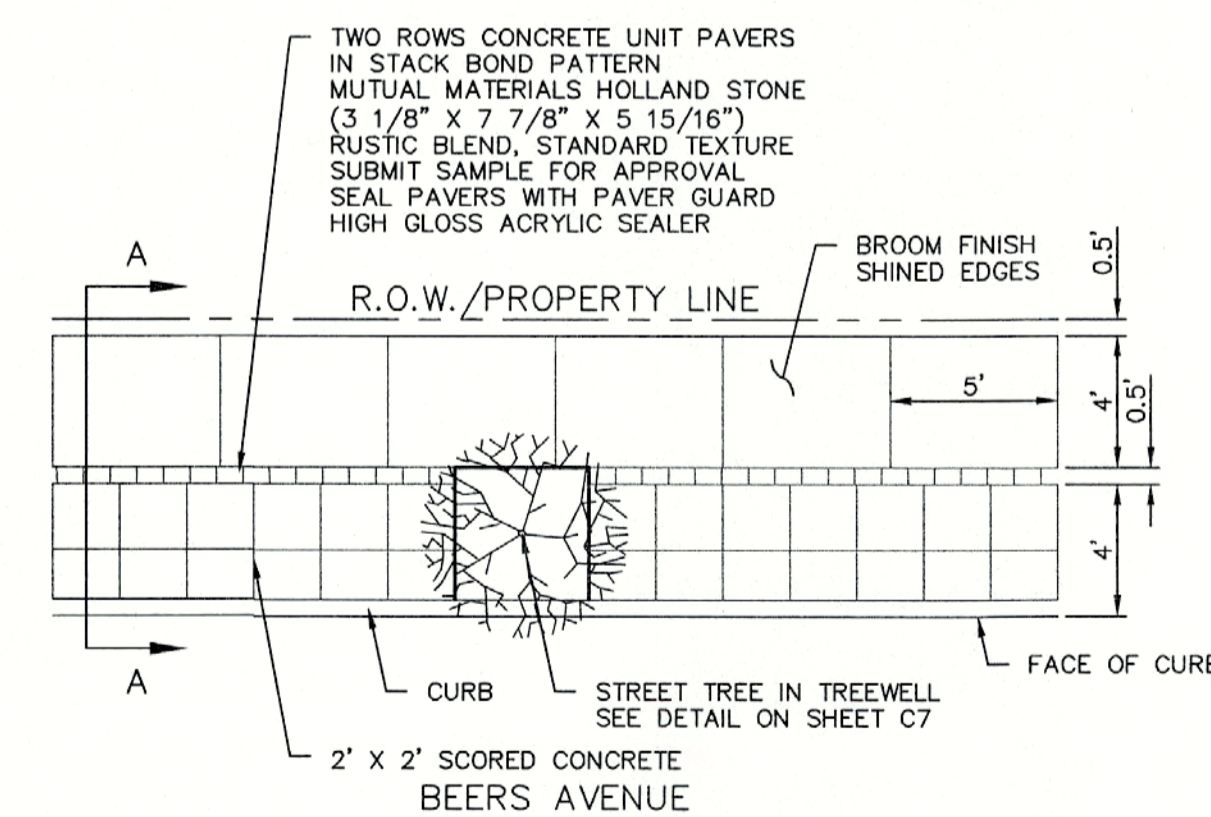
CURB RETURN PROFILE
SCALE: SEE SCALE BAR



BEERS AVENUE FLOWLINE PROFILE
SCALE: SEE SCALE BAR



BEERS AVENUE SIDEWALK SURFACING DETAIL
SCALE: N.T.S.



EXPIRES: 06/30/21
SIGNATURE DATE:
PRELIMINARY

DATE:	NO.	REVISION

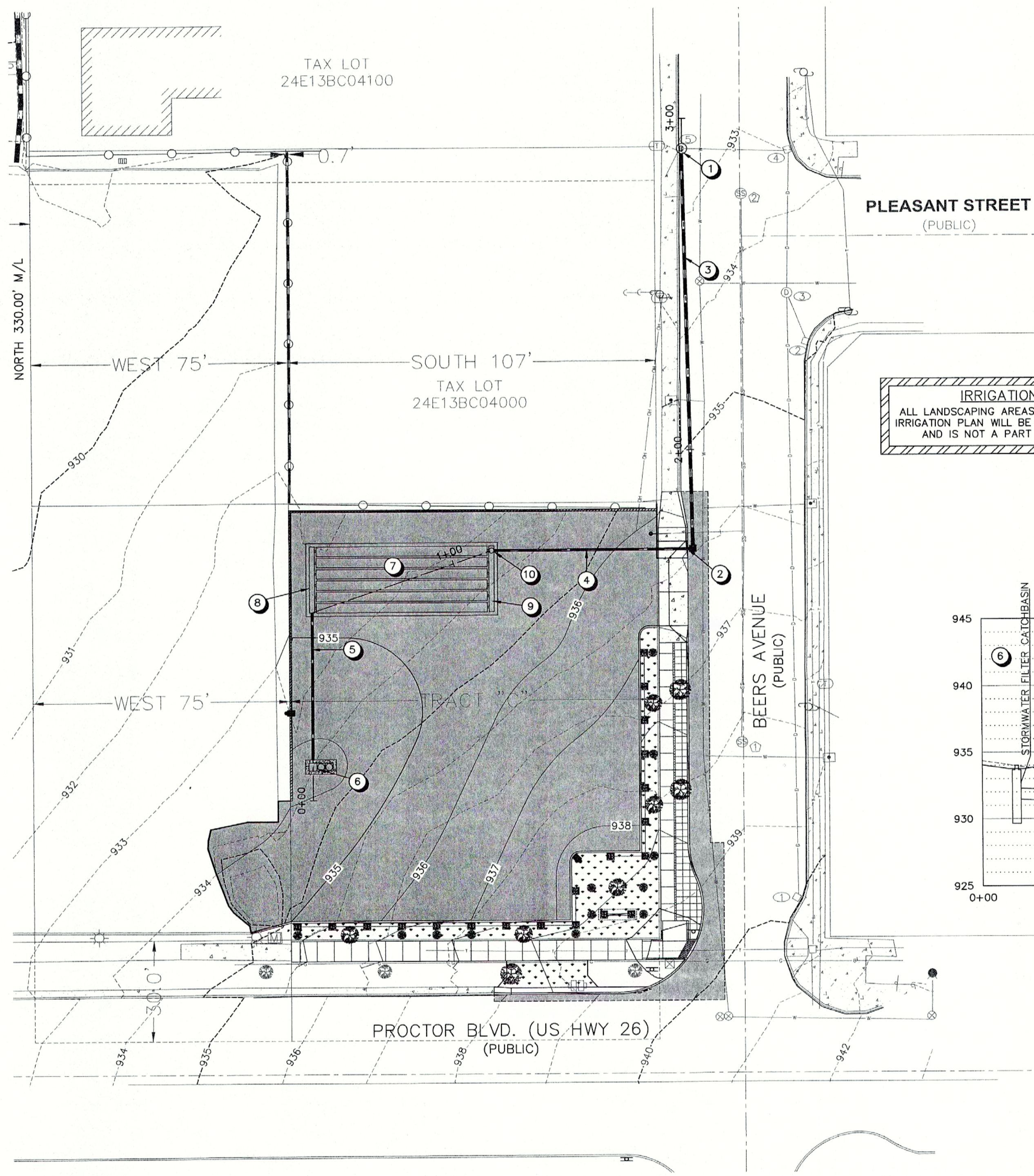
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TROUTDALE, OREGON 97060
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FUNTIME RV, INC
NE BEERS AVE & PROCTOR BLVD
SANDY, OREGON 97055

BEERS AVENUE PLAN & PROFILE

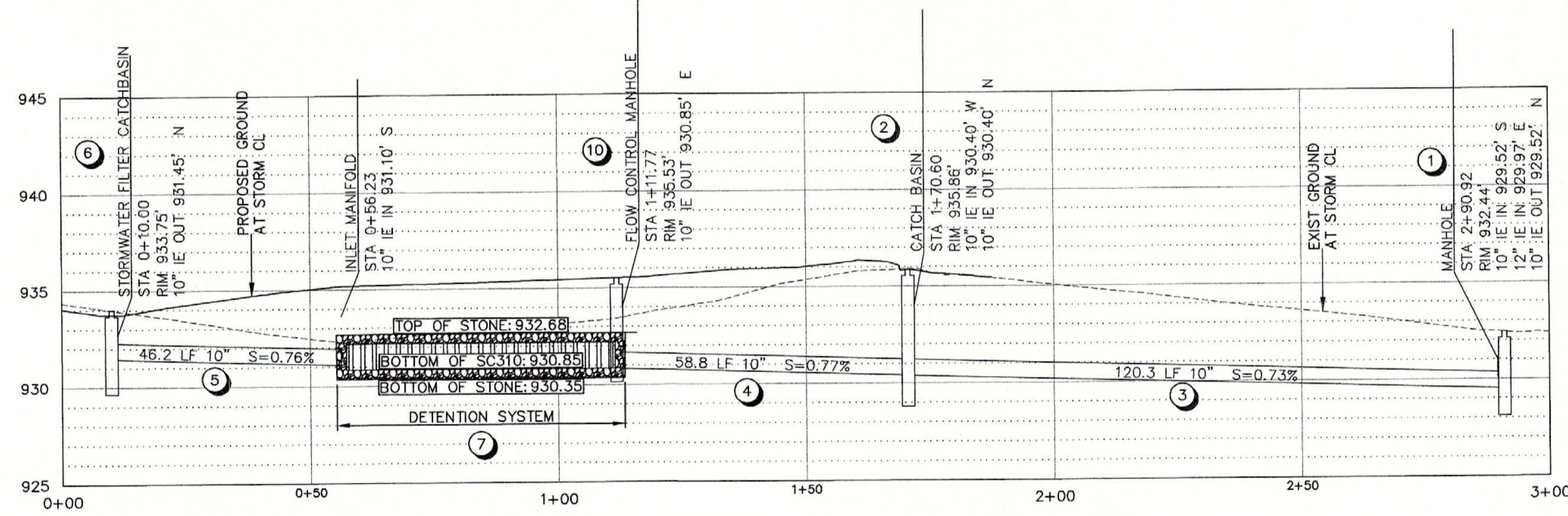
C5
11



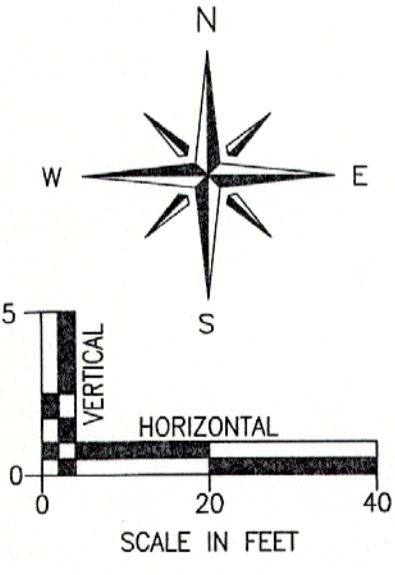
IRRIGATION NOTE:
 ALL LANDSCAPING AREAS SHALL BE IRRIGATED.
 IRRIGATION PLAN WILL BE PREPARED BY OTHERS
 AND IS NOT A PART OF THESE PLANS.

KEY NOTES

- REMOVE EXISTING CATCH BASIN AND INSTALL MANHOLE
 SEE DETAIL SHEET C8
 RIM = 932.35
- ① 6" IN W. (EXIST) = 930.51
 12" IN E. (EXIST) = 929.97
 12" OUT N. (EXIST) = 929.52
 10" IN S. = 929.52
- CATCH BASIN SEE DETAIL SHEET C8
 RIM = 935.86
- ② 10" IN W. = 930.95
 10" OUT N. = 930.95
- ③ 10" HDPE STORM LINE
 120.3 L.F., S = 0.0073
- ④ 10" HDPE STORM LINE
 58.8 L.F., S = 0.0077
- ⑤ 10" HDPE STORM LINE
 46.2 L.F., S = 0.0076
- STORMWATER FILTER CATCHBASIN SEE DETAIL SHEET C11
 RIM ELE = 933.75
 10" OUT N. = 932.03
- ⑦ STORMWATER DETENTION, 300 LF STORMTECH CHAMBERS
 SEE DETAIL SHEET C11
- 18" ECCENTRIC HEADER 16.2 LF.
 10" IN = 931.10
 CONNECT STORMTECH CHAMBERS TO HEADER PER
 MANUFACTURER REQUIREMENTS
- ⑧ 18" ECCENTRIC HEADER 16.2 LF.
 10" OUT = 930.85
 CONNECT STORMTECH CHAMBERS TO HEADER PER
 MANUFACTURER REQUIREMENTS
- ⑨ INSTALL FLOW CONTROL MANHOLE SEE DETAIL SHEET C8
 RIM ELE = 935.44
 10" OUT = 931.54



STORMLINE PROFILE
 SCALE: SEE SCALE BAR



REGISTERED PROFESSIONAL
 ENGINEER
 67149PE
FOR REVIEW
 OREGON
 APR 14 2005
 KELLI A. GORTER

EXPIRES: 06/30/21
 SIGNATURE DATE:
PRELIMINARY

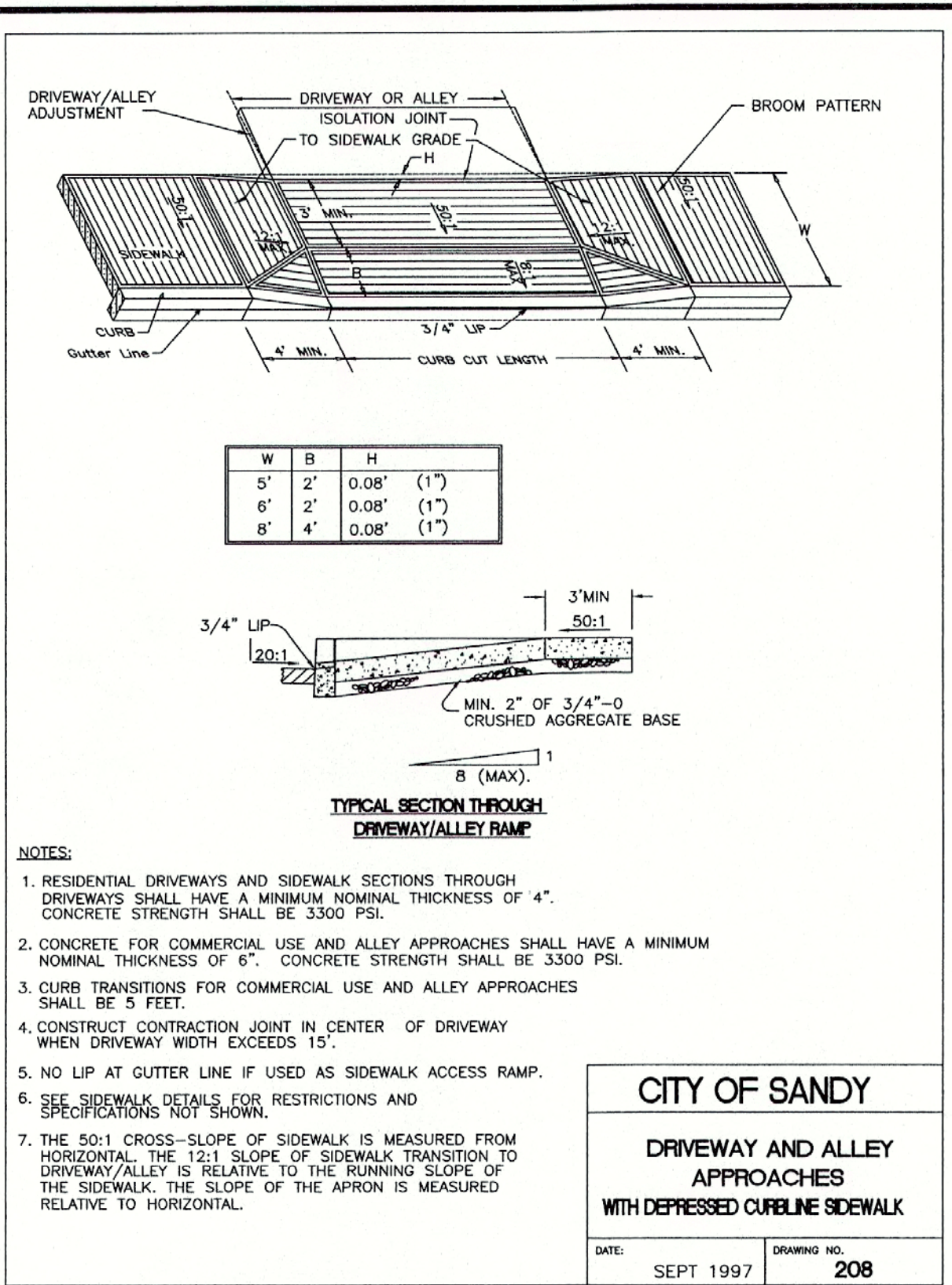
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SCALE: AS SHOWN	DATE: JULY 23, 2019	
PROJECT NO. E17-060		
DATE:	NO.	REVISION

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 SURVEYING + ENGINEERING + PLANNING
 369 E. HISTORIC COLUMBIA RIVER HWY
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 BUS: (503) 668-3737 + FAX: (503) 668-3788

FUNTIME RV, INC
 NE BEERS AVE & PROCTOR BLVD
 SANDY, OREGON 97055

UTILITY PLAN

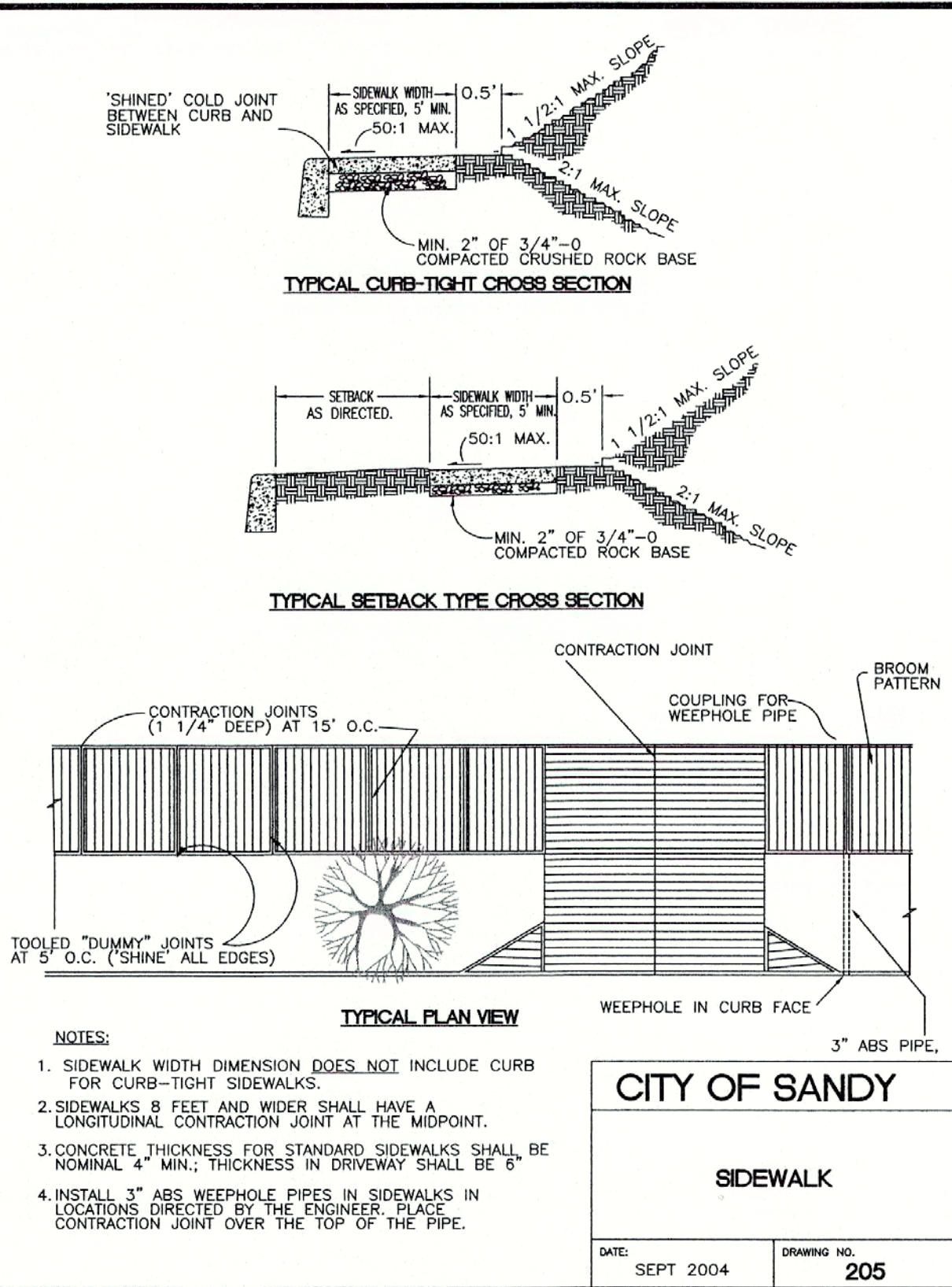
C6
 11



NOTES:

- RESIDENTIAL DRIVEWAYS AND SIDEWALK SECTIONS THROUGH DRIVEWAYS SHALL HAVE A MINIMUM NOMINAL THICKNESS OF 4" CONCRETE STRENGTH SHALL BE 3300 PSI.
- CONCRETE FOR COMMERCIAL USE AND ALLEY APPROACHES SHALL HAVE A MINIMUM NOMINAL THICKNESS OF 6" CONCRETE STRENGTH SHALL BE 3300 PSI.
- CURB TRANSITIONS FOR COMMERCIAL USE AND ALLEY APPROACHES SHALL BE 5 FEET.
- CONSTRUCT CONTRACTION JOINT IN CENTER OF DRIVEWAY WHEN DRIVEWAY WIDTH EXCEEDS 15'.
- NO LIP AT CUTTER LINE IF USED AS SIDEWALK ACCESS RAMP.
- SEE SIDEWALK DETAILS FOR RESTRICTIONS AND SPECIFICATIONS NOT SHOWN.
- THE 50:1 CROSS-SLOPE OF SIDEWALK IS MEASURED FROM HORIZONTAL. THE 12:1 SLOPE OF SIDEWALK TRANSITION TO DRIVEWAY/ALLEY IS RELATIVE TO THE RUNNING SLOPE OF THE SIDEWALK. THE SLOPE OF THE APRON IS MEASURED RELATIVE TO HORIZONTAL.

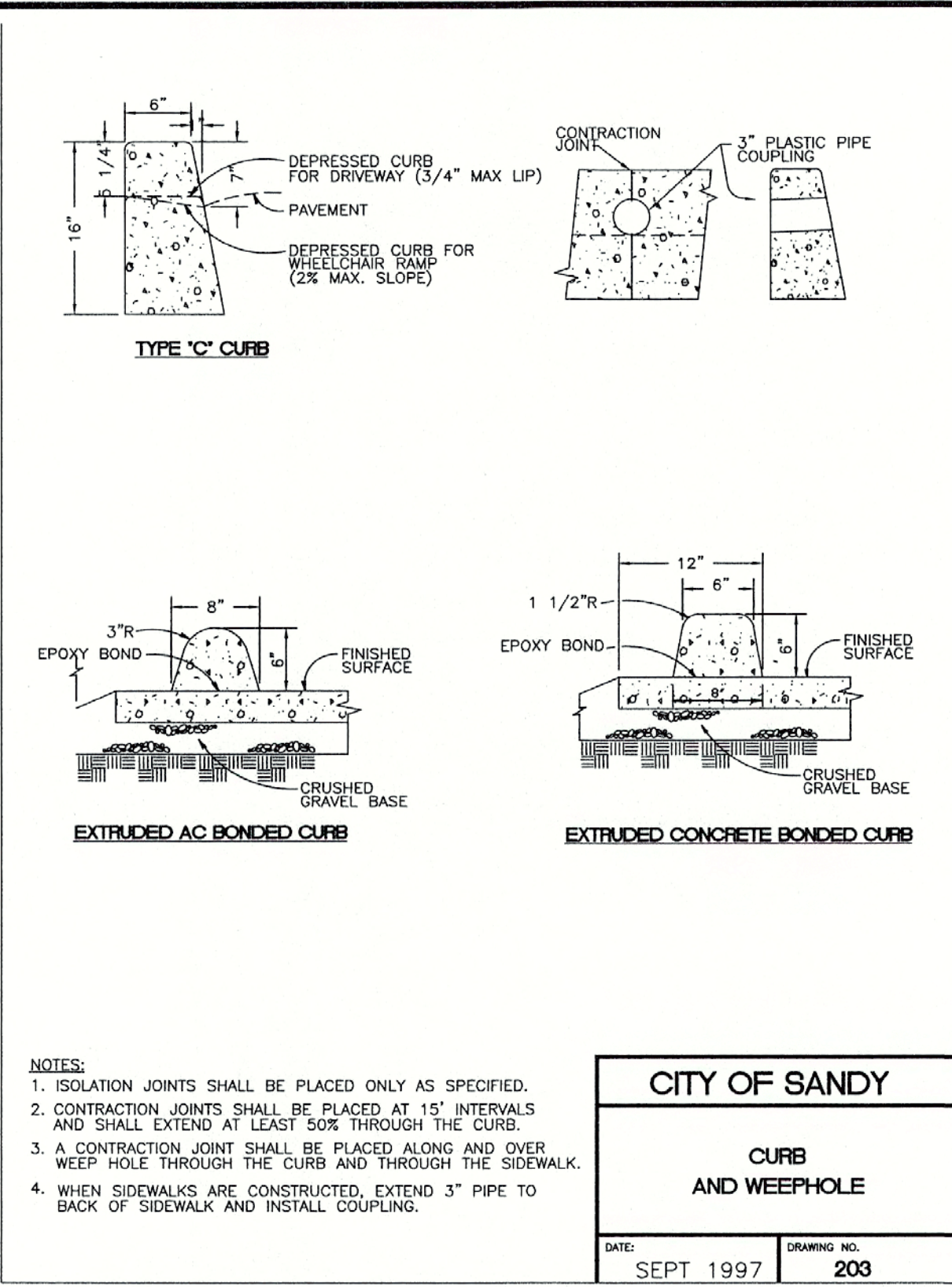
CITY OF SANDY
DRIVEWAY AND ALLEY APPROACHES WITH DEPRESSED CURBLINE SIDEWALK
 DATE: SEPT 1997 DRAWING NO. 208



NOTES:

- SIDEWALK WIDTH DIMENSION DOES NOT INCLUDE CURB FOR CURB-TIGHT SIDEWALKS.
- SIDEWALKS 8 FEET AND WIDER SHALL HAVE A LONGITUDINAL CONTRACTION JOINT AT THE MIDPOINT.
- CONCRETE THICKNESS FOR STANDARD SIDEWALKS SHALL BE NOMINAL 4" MIN.; THICKNESS IN DRIVEWAY SHALL BE 6"
- INSTALL 3" ABS WEEPHOLE PIPES IN SIDEWALKS IN LOCATIONS DIRECTED BY THE ENGINEER. PLACE CONTRACTION JOINT OVER THE TOP OF THE PIPE.

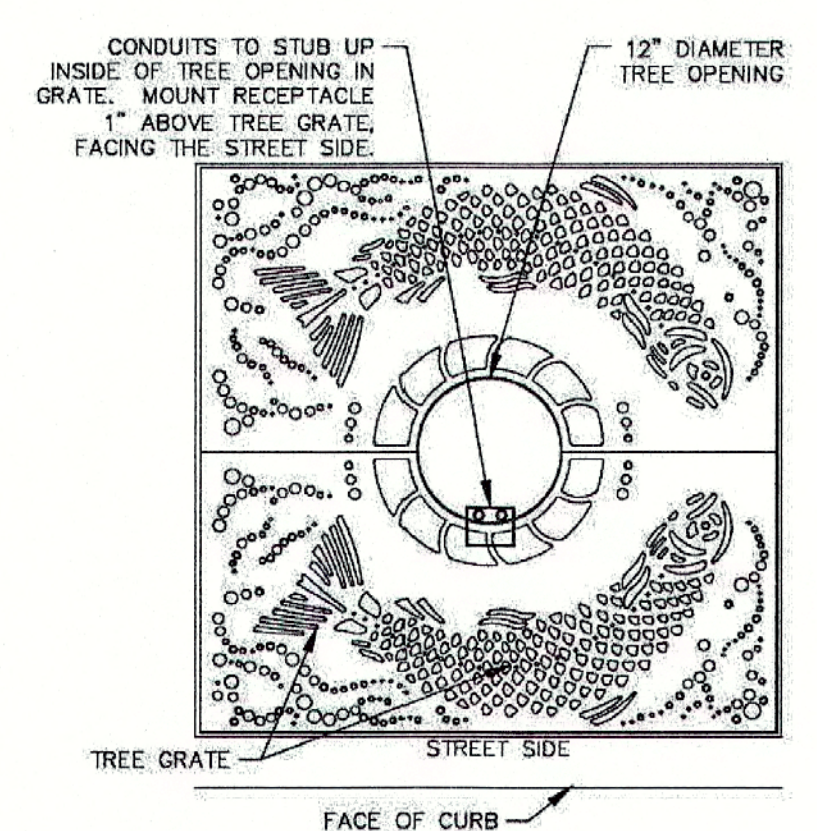
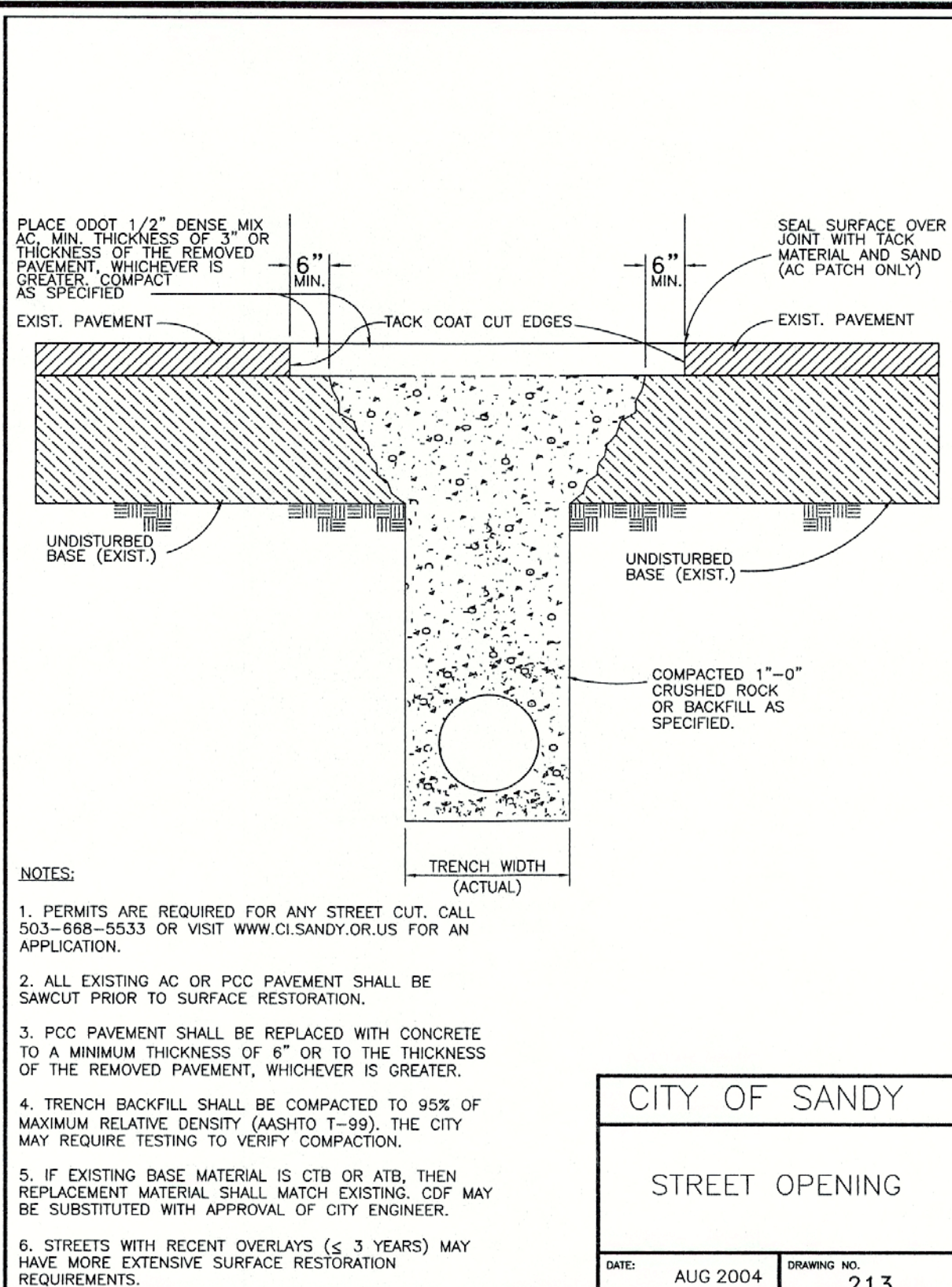
CITY OF SANDY
SIDEWALK
 DATE: SEPT 2004 DRAWING NO. 205



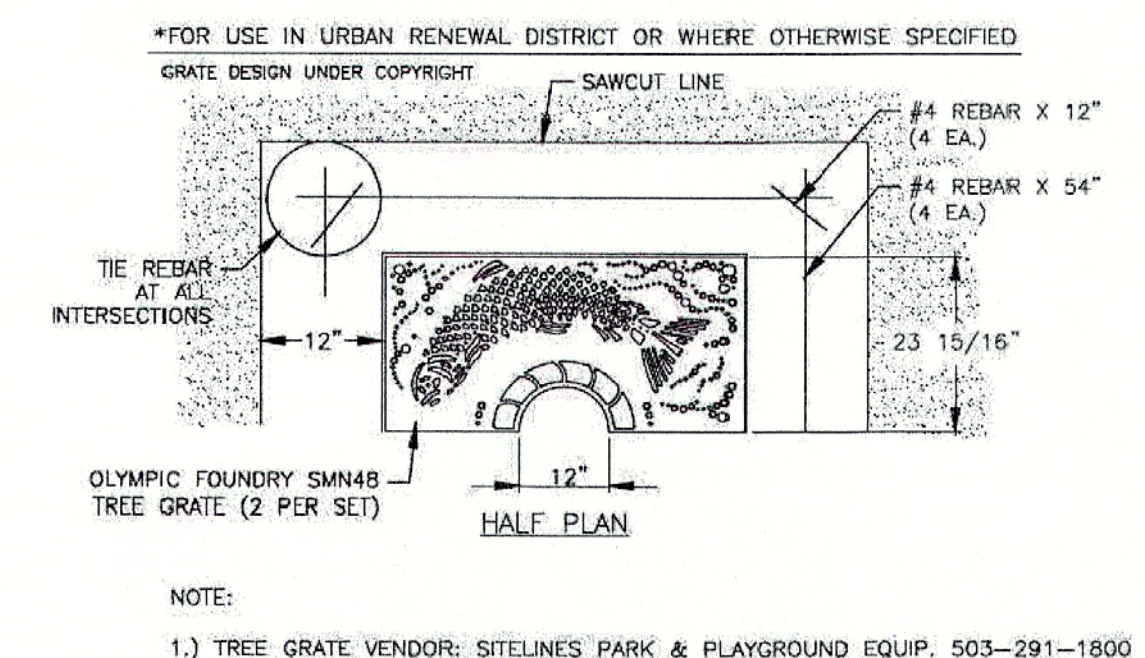
NOTES:

- ISOLATION JOINTS SHALL BE PLACED ONLY AS SPECIFIED.
- CONTRACTION JOINTS SHALL BE PLACED AT 15' INTERVALS AND SHALL EXTEND AT LEAST 50% THROUGH THE CURB.
- A CONTRACTION JOINT SHALL BE PLACED ALONG AND OVER WEEP HOLE THROUGH THE CURB AND THROUGH THE SIDEWALK.
- WHEN SIDEWALKS ARE CONSTRUCTED, EXTEND 3" PIPE TO BACK OF SIDEWALK AND INSTALL COUPLING.

CITY OF SANDY
CURB AND WEEPHOLE
 DATE: SEPT 1997 DRAWING NO. 203



TREE WELL & GRATE DETAIL
 SCALE: N.T.S.



EXPIRES: 06/30/21
 SIGNATURE DATE: _____
PRELIMINARY

DATE	NO.	REVISION

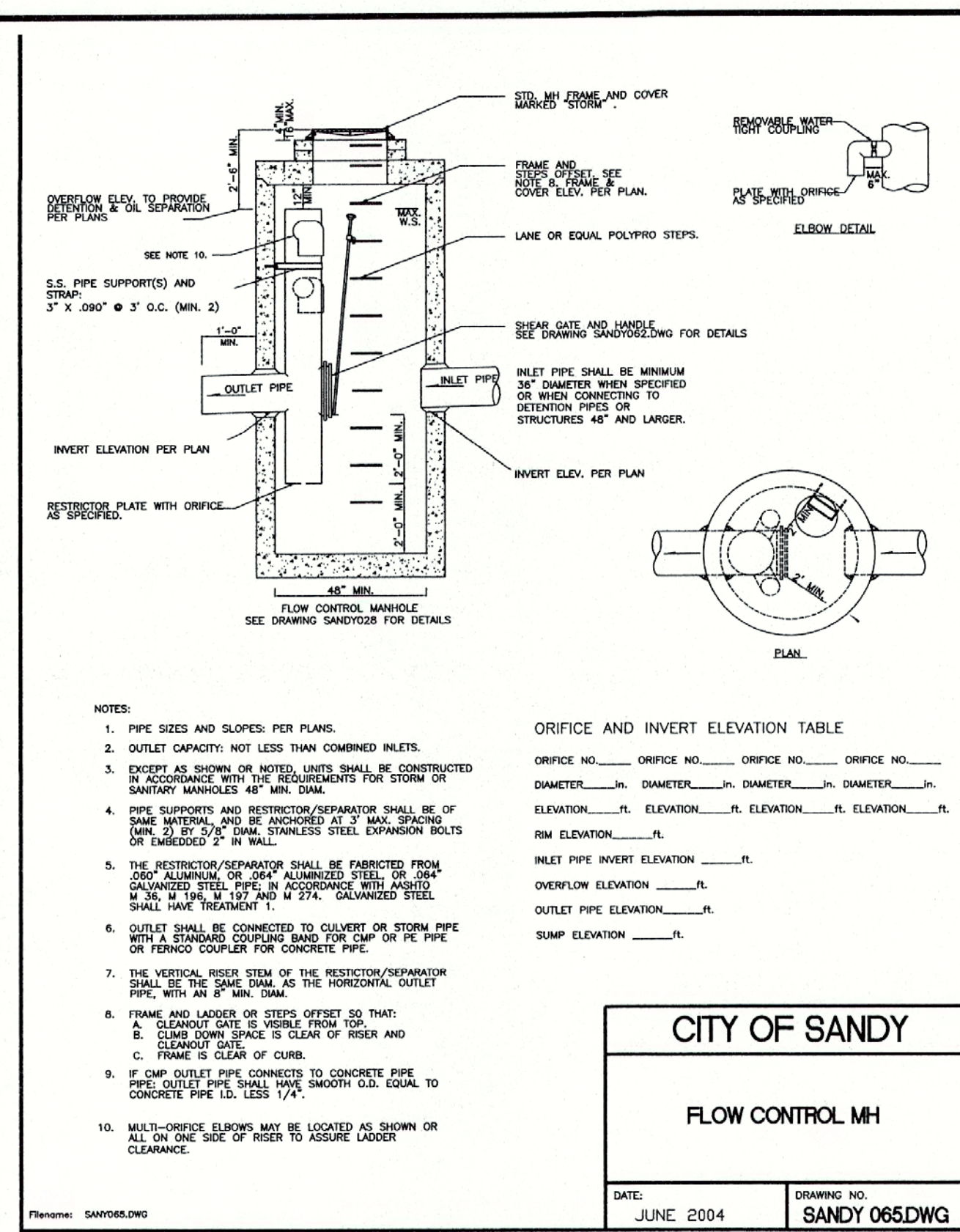
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 Firwood Design Group, LLC
 SURVEYING + ENGINEERING + PLANNING

359 E. HISTORIC COLUMBIA RIVER HWY
 TROUTDALE, OREGON 97060
 BUS: (503) 668-3737 • FAX: (503) 668-3788

FUNTIME RV, INC
 NE BEERS AVE & PROCTOR BLVD
 SANDY, OREGON 97055

DETAILS SHEET - CITY OF SANDY

C7
11

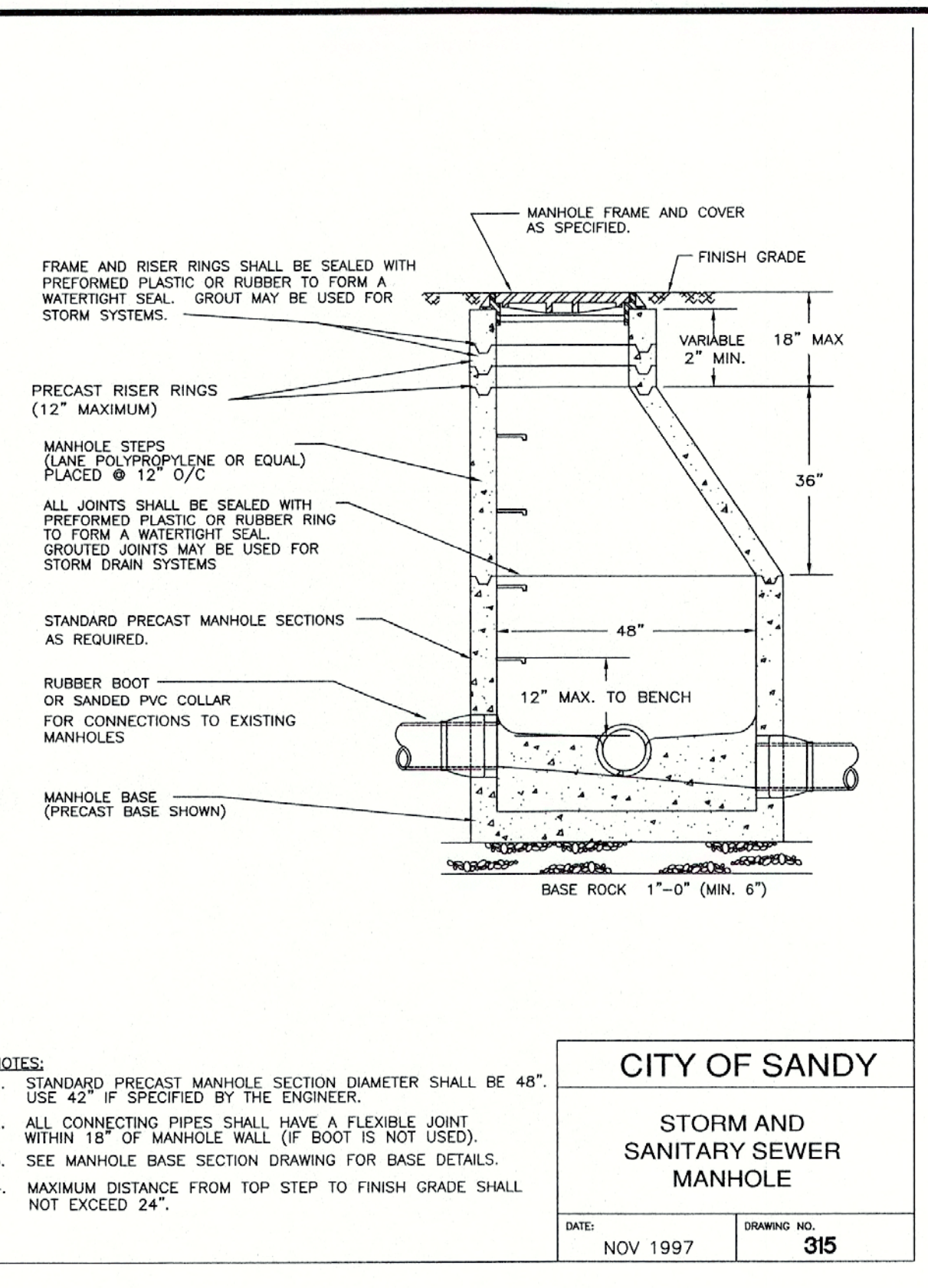


- NOTES:
- PIPE SIZES AND SLOPES PER PLANS.
 - OUTLET CAPACITY NOT LESS THAN COMBINED INLETS.
 - EXCEPT AS SHOWN OR NOTED, LIMITS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS FOR STORM OR SANITARY MANHOLES 48" MIN. DIA.
 - PIPE SUPPORTS AND RESTRICTOR/SEPARATOR SHALL BE OF SAME MATERIAL AND BE ANCHORED AT 3' MAX SPACING (MIN. 20" BY 2 1/2" DIA. STAINLESS STEEL EXPANSION BOLTS OR EMBEDDED 2" IN WALL).
 - THE RESTRICTOR/SEPARATOR SHALL BE FABRICATED FROM 304 ALUMINUM OR 20# ALUMINIZED STEEL OR 304 STAINLESS STEEL. IT SHALL BE CONFORM TO A SMOOTH SURFACE WITH A FINISH OF 274. GALVANIZED STEEL SHALL HAVE TREATMENT 1.
 - OUTLET SHALL BE CONNECTED TO CURB OR STORM PIPE WITH A STANDARD COUPLING BAND FOR CMP OR PE PIPE OR FERRIS COUPLER FOR CONCRETE PIPE.
 - THE VERTICAL RISER STEM OF THE RESTRICTOR/SEPARATOR SHALL BE THE SAME DIA. AS THE HORIZONTAL OUTLET PIPE, WITH A 2" MIN. DIA.
 - FRAME AND LADDER OR STEPS OFFSET SO THAT:
 - CLEAROUT GATE IS VISIBLE FROM TOP.
 - CLEAROUT GATE IS CLEAR OF RISER AND CLEAROUT GATE.
 - FRAME IS CLEAR OF CURB.
 - IF CMP OUTLET PIPE CONNECTS TO CONCRETE PIPE, PREL. SLOPE SHALL HAVE SMOOTH G.L.S. EQUAL TO CONCRETE PIPE I.D. LESS 1/4".
 - MULTI-ORFICE ELBOWS MAY BE LOCATED AS SHOWN OR ALL ON ONE SIDE OF RISER TO ASSURE LADDER CLEARANCE.

ORFICE AND INVERT ELEVATION TABLE

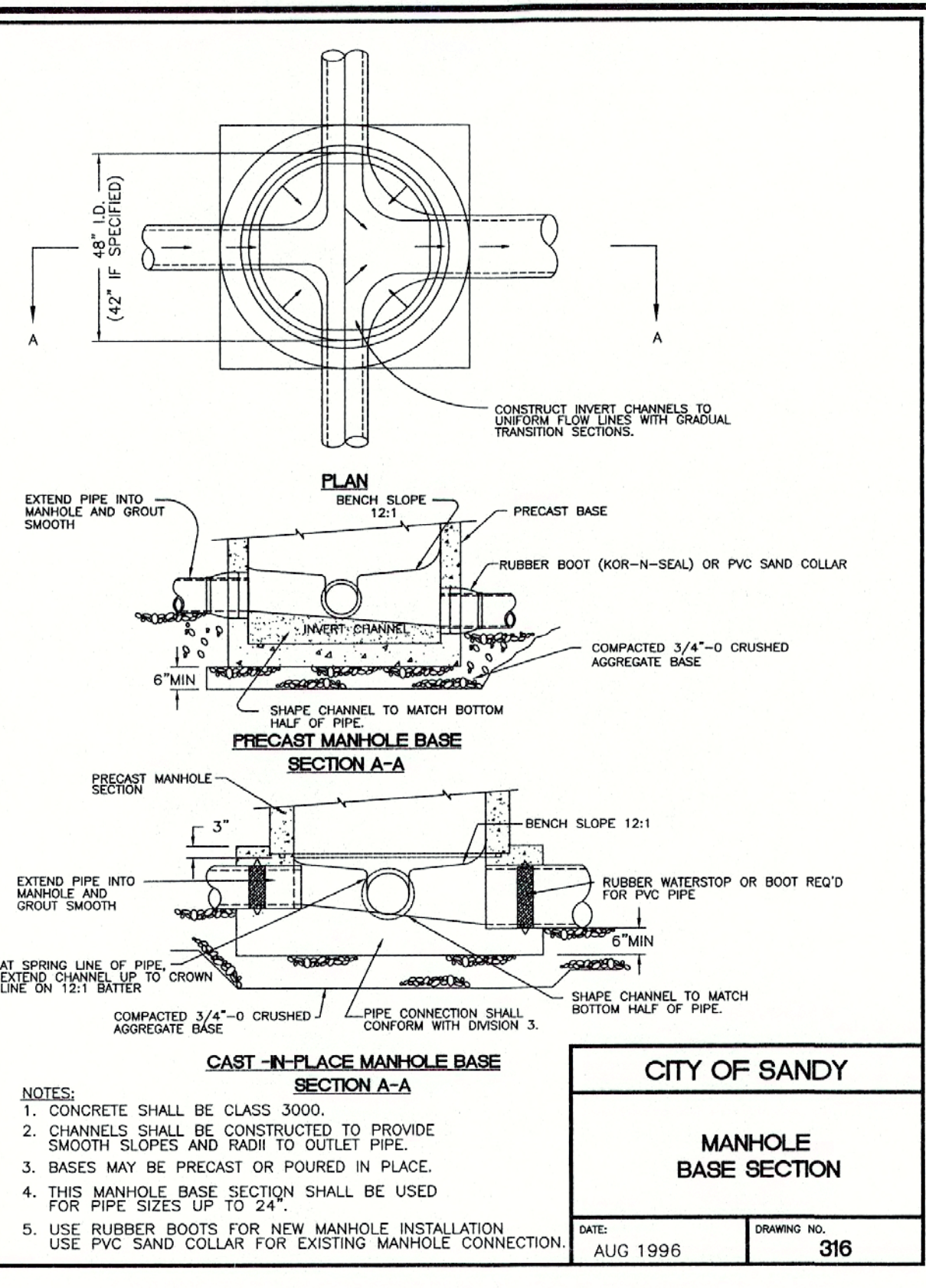
ORFICE NO.	ORFICE NO.	ORFICE NO.	ORFICE NO.
DIAMETER	DIAMETER	DIAMETER	DIAMETER
ELEVATION	ELEVATION	ELEVATION	ELEVATION
RIM ELEVATION	RIM ELEVATION	RIM ELEVATION	RIM ELEVATION
INLET PIPE INVERT ELEVATION	INLET PIPE INVERT ELEVATION	INLET PIPE INVERT ELEVATION	INLET PIPE INVERT ELEVATION
OVERFLOW ELEVATION	OVERFLOW ELEVATION	OVERFLOW ELEVATION	OVERFLOW ELEVATION
OUTLET PIPE ELEVATION	OUTLET PIPE ELEVATION	OUTLET PIPE ELEVATION	OUTLET PIPE ELEVATION
SUMP ELEVATION	SUMP ELEVATION	SUMP ELEVATION	SUMP ELEVATION

CITY OF SANDY
FLOW CONTROL MH
 DATE: JUNE 2004
 DRAWING NO. SANDY 065.DWG



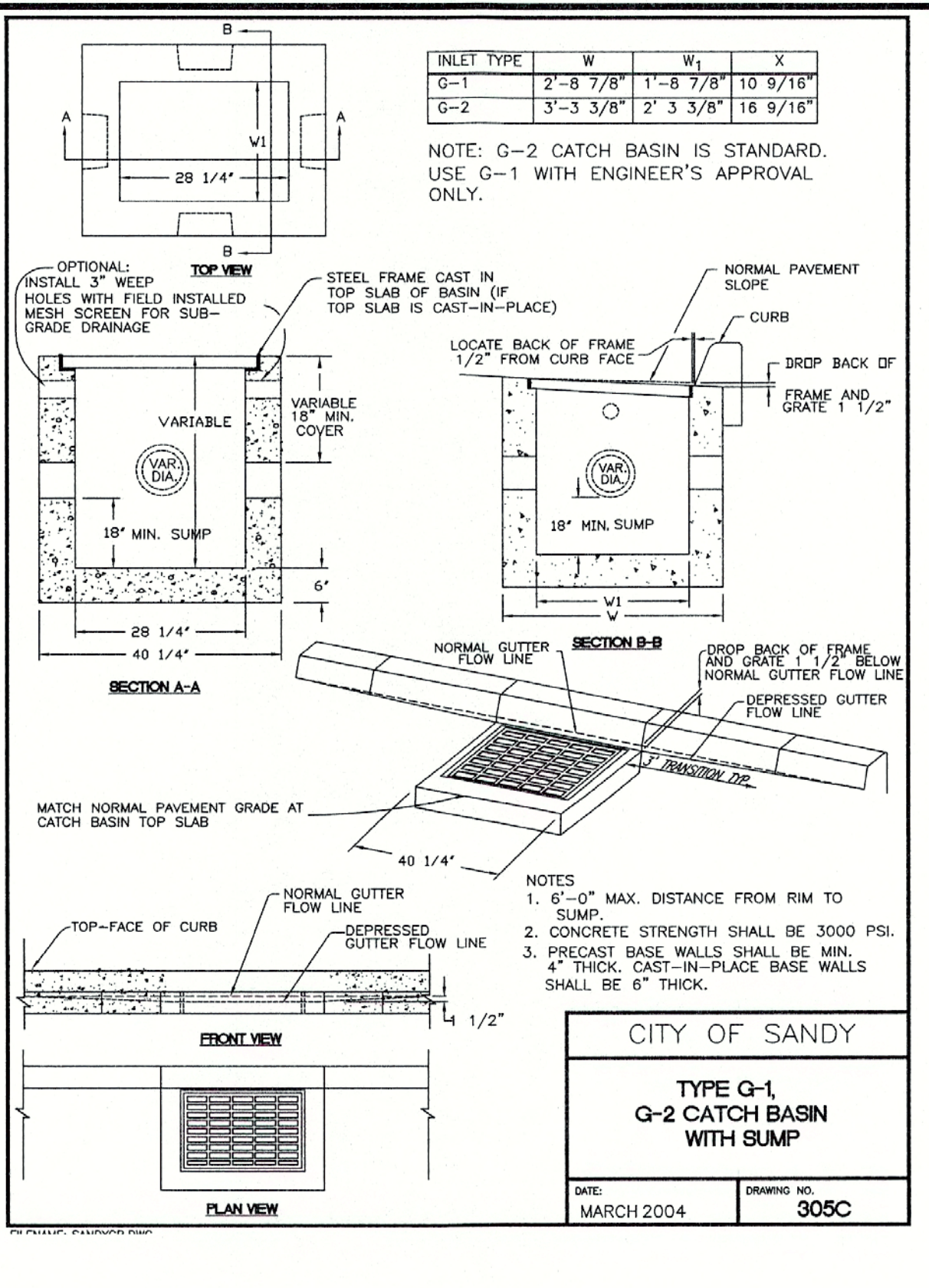
- FRAME AND RISER RINGS SHALL BE SEALED WITH PREFORMED PLASTIC OR RUBBER TO FORM A WATER TIGHT SEAL. GROUT MAY BE USED FOR STORM SYSTEMS.
- PRECAST RISER RINGS (12" MAXIMUM)
- MANHOLE STEPS (LANE POLYPROPYLENE OR EQUAL) PLACED @ 12" O/C
- ALL JOINTS SHALL BE SEALED WITH PREFORMED PLASTIC OR RUBBER RING TO FORM A WATER TIGHT SEAL. GROUTED JOINTS MAY BE USED FOR STORM DRAIN SYSTEMS.
- STANDARD PRECAST MANHOLE SECTIONS AS REQUIRED.
- RUBBER BOOT OR SANDED PVC COLLAR FOR CONNECTIONS TO EXISTING MANHOLES
- MANHOLE BASE (PRECAST BASE SHOWN)

CITY OF SANDY
STORM AND SANITARY SEWER MANHOLE
 DATE: NOV 1997
 DRAWING NO. 315



- CONSTRUCT INVERT CHANNELS TO UNIFORM FLOW LINES WITH GRADUAL TRANSITION SECTIONS.
- EXTEND PIPE INTO MANHOLE AND GROUT SMOOTH
- PRECAST MANHOLE SECTION
- PRECAST MANHOLE SECTION
- EXTEND PIPE INTO MANHOLE AND GROUT SMOOTH
- AT SPRING LINE OF PIPE, EXTEND CHANNEL UP TO CROWN
- COMPACTED 3/4" - 0 CRUSHED AGGREGATE BASE
- PIPE CONNECTION SHALL CONFORM WITH DIVISION 3.
- SHAPE CHANNEL TO MATCH BOTTOM HALF OF PIPE.
- RUBBER WATERSTOP OR BOOT REQ'D FOR PVC PIPE
- COMPACTED 3/4" - 0 CRUSHED AGGREGATE BASE
- SHAPE CHANNEL TO MATCH BOTTOM HALF OF PIPE.

CITY OF SANDY
MANHOLE BASE SECTION
 DATE: AUG 1996
 DRAWING NO. 316



- | INLET TYPE | W | W1 | X |
|------------|-----------|-----------|----------|
| G-1 | 2'-8 7/8" | 1'-8 7/8" | 10 9/16" |
| G-2 | 3'-3 3/8" | 2' 3 3/8" | 16 9/16" |
- NOTE: G-2 CATCH BASIN IS STANDARD. USE G-1 WITH ENGINEER'S APPROVAL ONLY.
- OPTIONAL: INSTALL 3" WEEP HOLES WITH FIELD INSTALLED MESH SCREEN FOR SUB-GRADE DRAINAGE
- STEEL FRAME CAST IN TOP SLAB OF BASIN (IF TOP SLAB IS CAST-IN-PLACE)
- NORMAL PAVEMENT SLOPE
- CURB
- LOCATE BACK OF FRAME 1/2" FROM CURB FACE
- DROP BACK OF FRAME AND GRATE 1 1/2"
- 18" MIN. SUMP
- 18" MIN. SUMP
- SECTION A-A
- SECTION B-B
- NORMAL GUTTER FLOW LINE
- DROPPED GUTTER FLOW LINE
- DEPRESSED GUTTER FLOW LINE
- 40 1/4"
- 40 1/4"
- 1 1/2"
- TOP-FACE OF CURB
- NORMAL GUTTER FLOW LINE
- DEPRESSED GUTTER FLOW LINE
- FRONT VIEW
- PLAN VIEW

CITY OF SANDY
TYPE G-1 G-2 CATCH BASIN WITH SUMP
 DATE: MARCH 2004
 DRAWING NO. 305C

DATE	NO.	REVISION

FDG
 Firwood Design Group, LLC
 SURVEYING + ENGINEERING + PLANNING

359 E. HISTORIC COLUMBIA RIVER HWY
 TROUTDALE, OREGON 97060
 BUS: (503) 668-3737 • FAX: (503) 668-3788

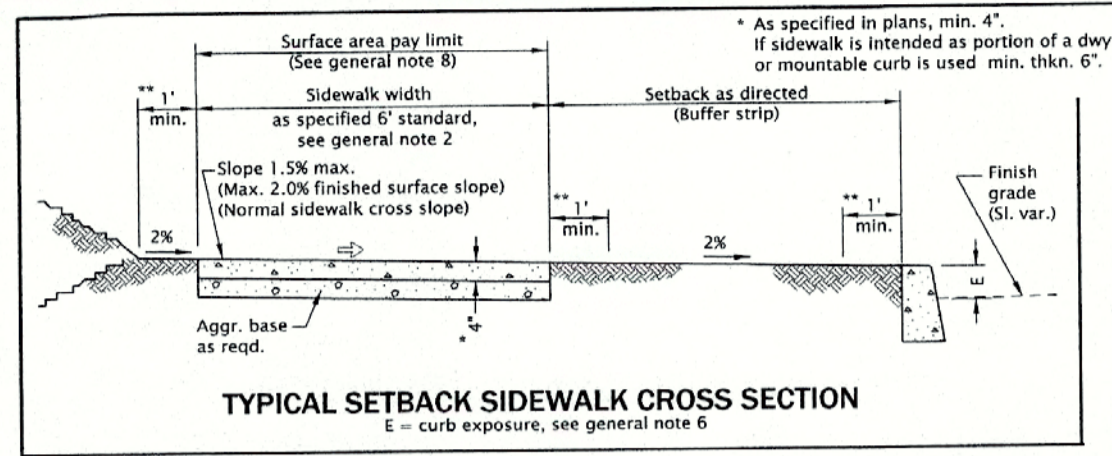
FUNTIME RV, INC
 NE BEERS AVE & PROCTOR BLVD
 SANDY, OREGON 97055

DETAILS SHEET - CITY OF SANDY

C8
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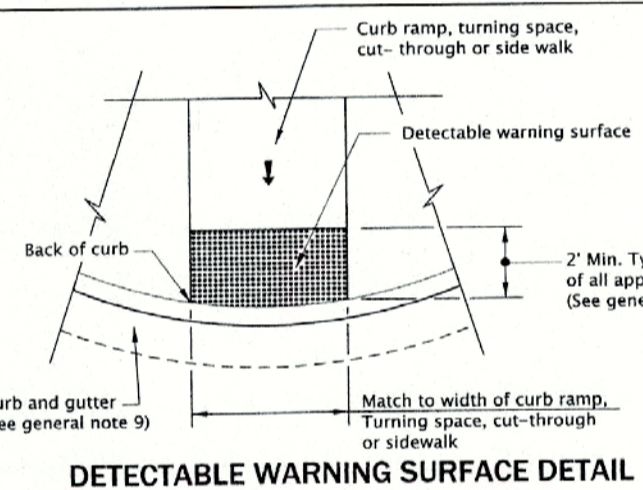
EXPIRES: 06/30/21
 SIGNATURE DATE: _____
 PRELIMINARY



- GENERAL NOTES FOR ALL DETAILS:**
1. Include additional paved or unpaved 2' clearance to vertical faces higher than 5' such as retaining walls, sound walls, fences and buildings.
 2. Curb type and sidewalk width as shown on plans or as directed.
 3. On sidewalks 8' and wider, provide a longitudinal joint at the midpoint.
 4. Const. expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveway.
 5. Const. contraction joints at 15' maximum spacing, and at ends of each driveway and curb ramp.
 6. For curb details, see Std. Dwg. RD700 & RD701.
 7. Sidewalk details are based on ODOT applicable standards.
 8. For driveway details not shown, see Std. Dwg. RD725, RD730, RD735, RD740, RD745 & RD750.
 9. See project plans for details not shown.

CALC. BOOK NO. N/A	BASELINE REPORT DATE 16-JUL-2018
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
OREGON STANDARD DRAWINGS	
SIDEWALKS	
2018	
DATE	REVISION DESCRIPTION
07-2018	REVISION
07-2018	REVISION

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



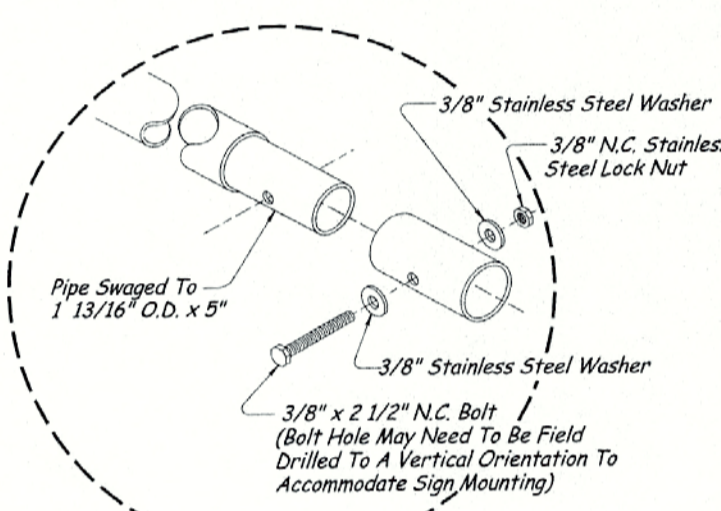
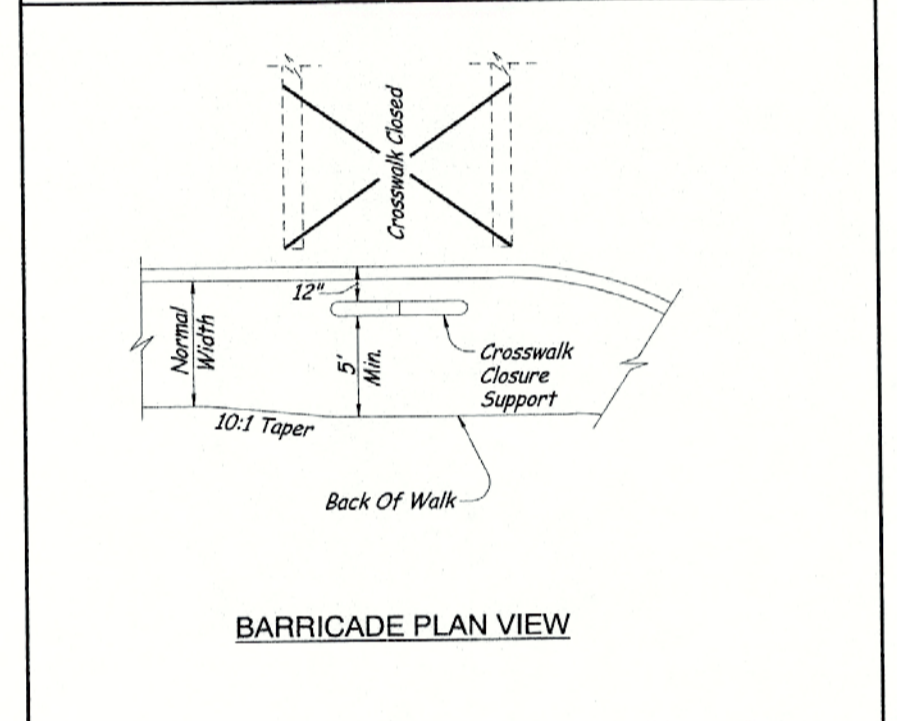
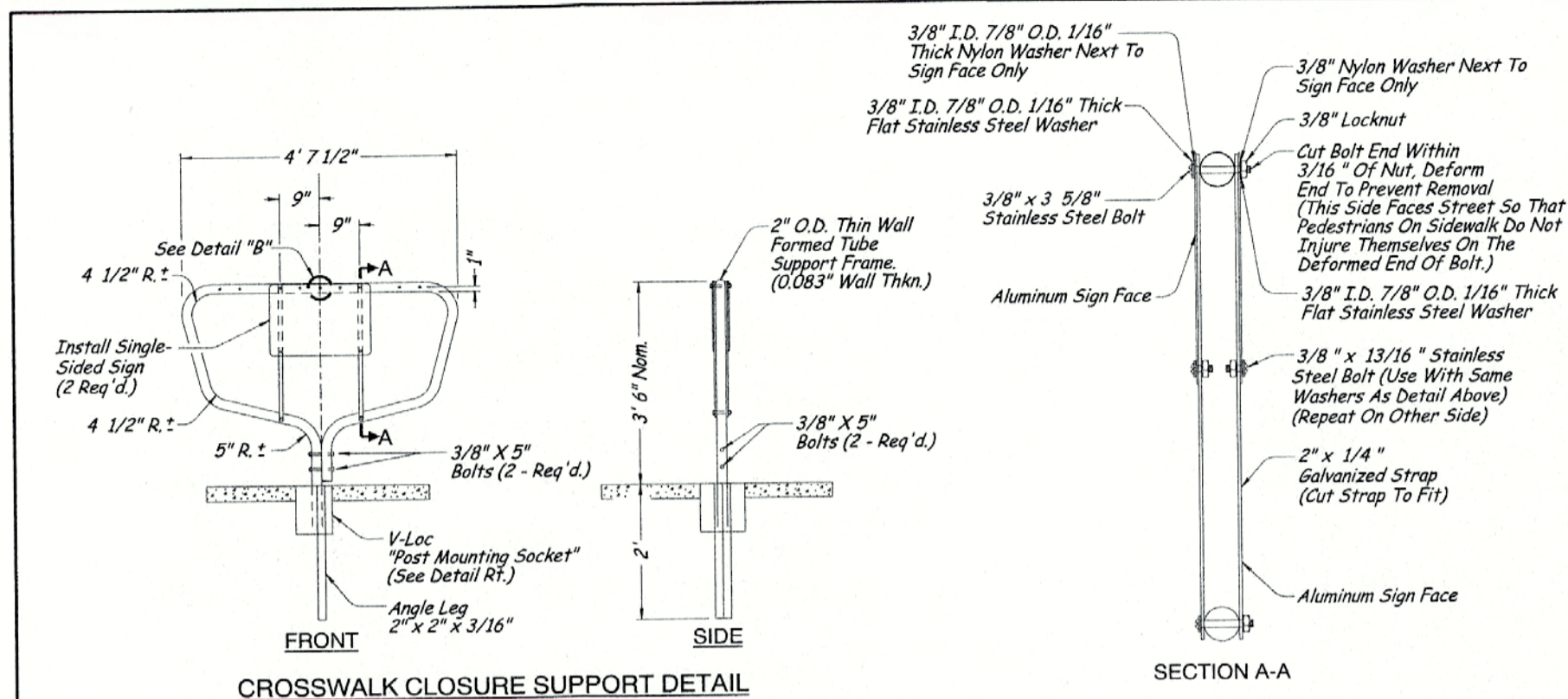
	A	B	C	D	E
MIN.	1.60'	0.65'	0.45'	0.90'	0.20'
MAX.	2.40'	--	0.91'	1.40'	0.20'

TRUNCATED DOME SPACING

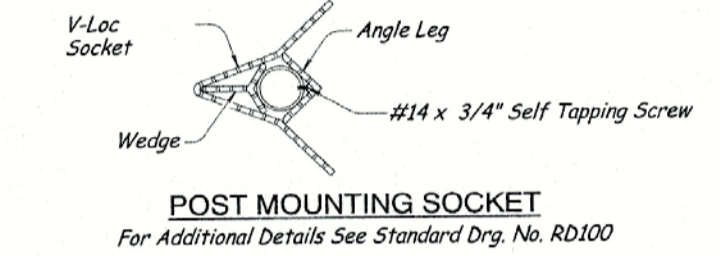
TRUNCATED DOME

- GENERAL NOTES FOR ALL DETAILS:**
1. Detectable warning surface details & locations are based on ODOT applicable Standards.
 2. See project plans for details not shown.
 3. See Std. Dwg. RD700 & RD701 for curbs.
 4. See Std. Dwg. RD720 for sidewalks.
 5. See Std. Dwg. TMS03 & TMS30 for crosswalk markings, widths, etc.
 6. See Std. Dwg. RD705 & RD710 for islands.
 7. The detectable warning surface shall extend the full width of the curb ramp, or other roadway entrance as applicable. A gap of up to 2 inches on each side of the detectable warning surface is permitted.
 8. Detectable warning surface shall typically be placed at the back of curb across the roadway entrance. Detectable warning surface placement for perpendicular ramps vary as shown. Detectable warning surface may be rectangular, but must comply with the truncated dome size and spacing standards. Detectable warning surface may be cut to meet necessary shape as shown in plans. Color to be safety yellow if no color specified in construction note.
 9. On or along state highways, curb and gutter is required at curb ramps.
 10. Grade breaks at the top and bottom of curb ramp runs shall be perpendicular to the direction of the ramp run. Grade breaks shall not be permitted on the surface of ramp runs and turning spaces. Surface slopes that meet at grade breaks shall be flush.

Detectable warning surface Slope 7.5% max. (Max. 8.3% finished surface slope) Slope 1.5% max. (Max. 2.0% finished surface slope) (Normal sidewalk cross slope)	BASELINE REPORT DATE 16-JUL-2018 NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications. OREGON STANDARD DRAWINGS DETECTABLE WARNING SURFACE DETAILS & PLACEMENT LOCATIONS 2018
The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.	
DATE	REVISION DESCRIPTION



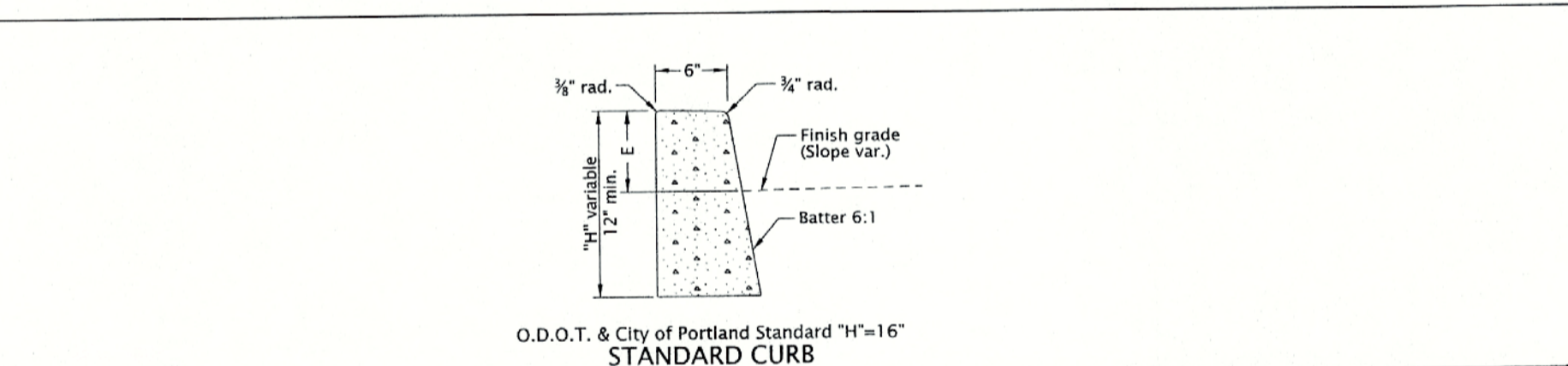
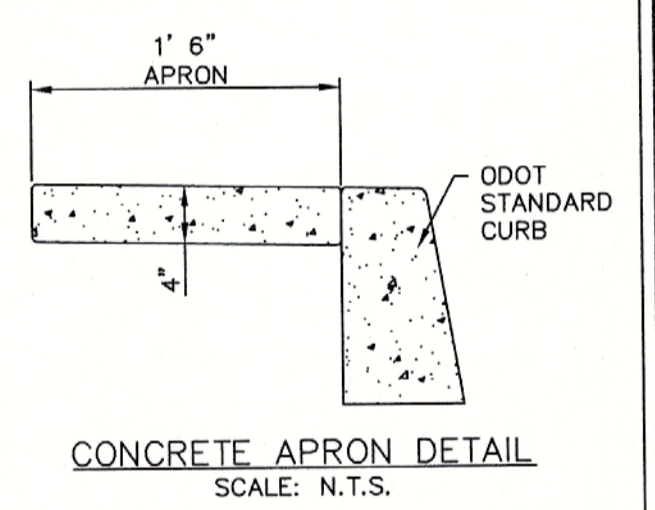
- GENERAL NOTE:**
1. All Holes In The Tube Support Frame To Be Pre-Drilled By The Manufacturer. (1/32" Larger Than Mounting Bolt)
 2. Pipe Swaged By The Manufacturer.



NOTE: Care Shall Be Taken That No Concrete Is Placed Within Mounting Socket.

CALC. BOOK NO. N/A	BASELINE REPORT DATE 07-01-16
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
OREGON STANDARD DRAWINGS	
CROSSWALK CLOSURE DETAIL	
2015	
DATE	REVISION DESCRIPTION
07-01-16	Changed Pedestrian Passage from 4' to 5'

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



- GENERAL NOTES FOR ALL DETAILS:**
1. Curb exposure "E" = 6" to 9", as measured vertically from flowline to highest point on curb. Vary as shown on plans or as directed. O.D.O.T standard "E"=7".
 2. Const. expansion joints at 200' maximum spacing, and at points of tangency, and at ends of each driveways.
 3. Const. contraction joints at 15' maximum spacing, and at ends of each inlet and ramp.
 4. Transitions shall be used to connect curbs of different exposures "E". ("E" is the total vertical dimension of those curb surfaces having a slope of 1:1 or steeper). Minimum desirable transition length shall be 20' for each 1" difference in "E".
 5. Tops of all curbs shall slope toward the roadway at 1.5% max. (Max. 2.0% finished surface slope), unless otherwise shown, or as directed.
 6. Dimensions are nominal, vary to conform with curb machine approved by the engineer.
 7. Dimensions adjacent to radii are measured to the point of intersection of curb surfaces.
 8. For sidewalk details, and monolithic curb & sidewalk, see Std. Drg. RD720.
 9. For drainage curbs, see Std. Drg. RD701.
 10. For sidewalk ramp details, see Std. Drg. RD755.

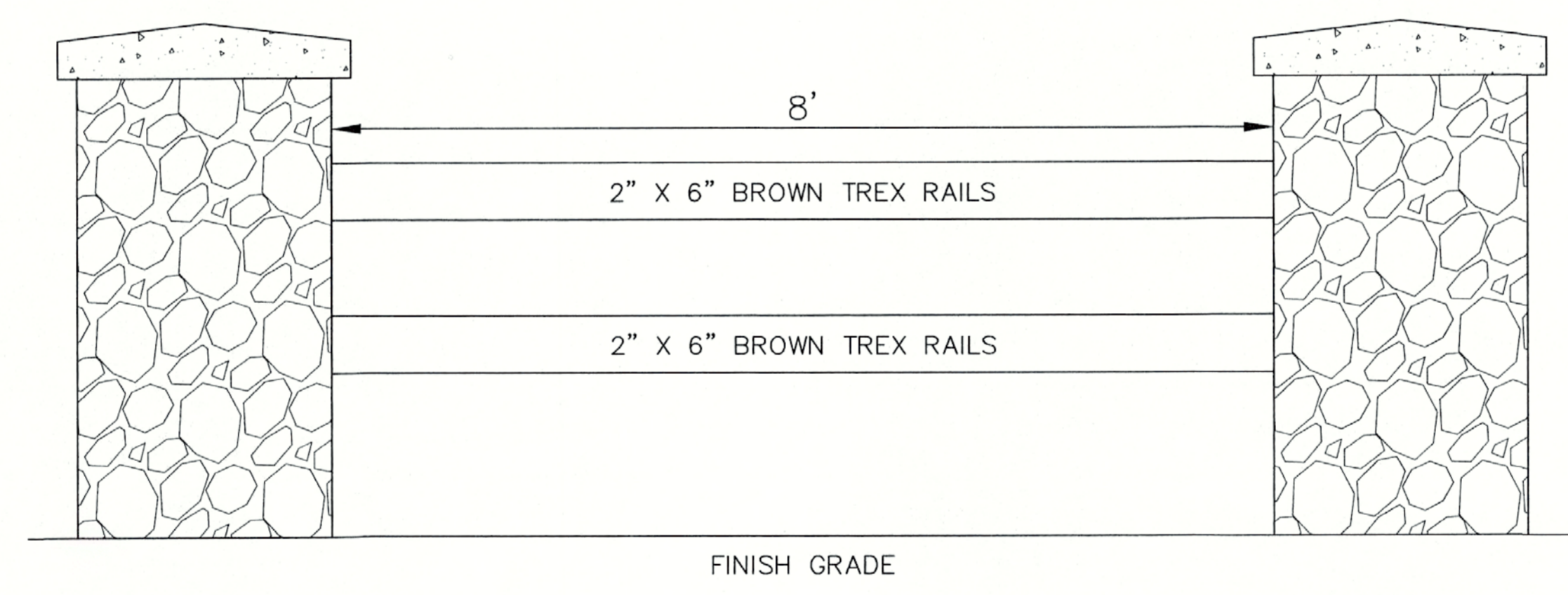
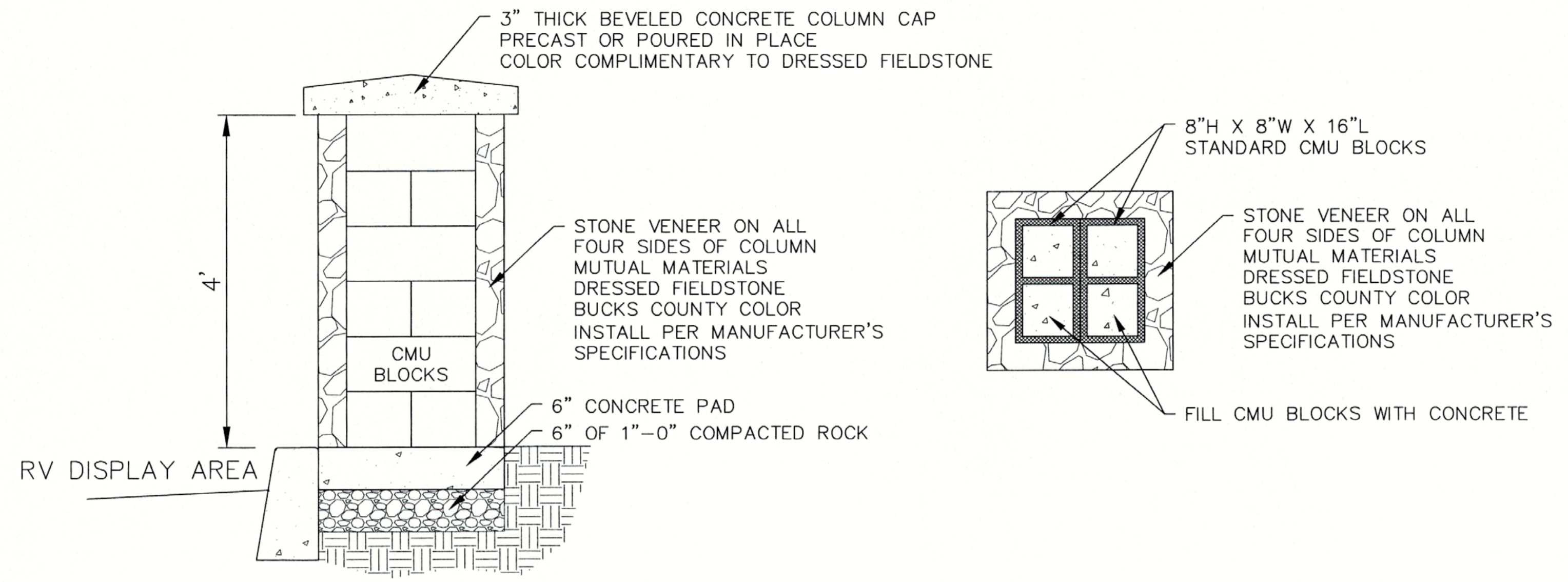
NOTE: All material and workmanship shall be in accordance with the current Oregon Standard Specifications	
OREGON STANDARD DRAWINGS	
CURBS	
2015	
DATE	REVISION DESCRIPTION
07-2015	ADDED NOTE
07-2015	REVISION
07-2016	REVISION
07-2018	REVISION
07-2018	REVISION
07-2017	ADDED & REVISED NOTE

The selection and use of this Standard Drawing, while designed in accordance with generally accepted engineering principles and practices, is the sole responsibility of the user and should not be used without consulting a Registered Professional Engineer.



EXPIRES: 06/30/21
SIGNATURE DATE: _____
PRELIMINARY

	DRAWN: _____	DESIGNED: _____	CHECKED: _____	<p>FDG Firwood Design Group, LLC SURVEYING + ENGINEERING + PLANNING</p>	<p>FUNTIME RV, INC NE BEERS AVE & PROCTOR BLVD SANDY, OREGON 97055</p>	<p>DETAILS SHEET - ODOT</p>	<p>C9 11</p>	
	SCALE: AS SHOWN	DATE: JULY 23, 2019	PROJECT NO. E17-060		<p>359 E. HISTORIC COLUMBIA RIVER HWY TROUTDALE, OREGON 97060 BUS: (503) 668-3737 • FAX: (503) 668-3788</p>			
DATE: NO.	REVISION							

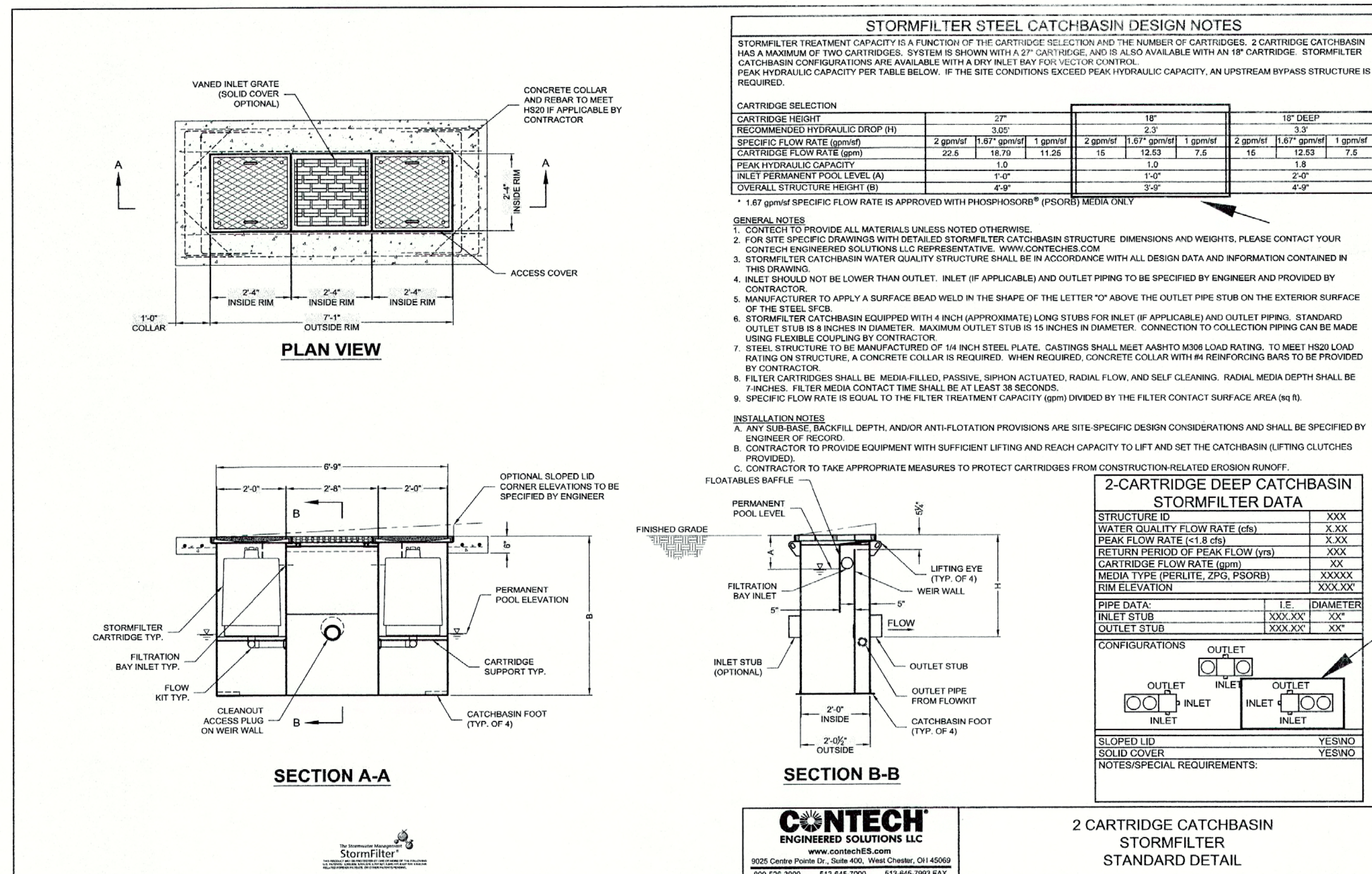


TYPICAL DECORATIVE RAIL AND COLUMN FENCE
SCALE: N.T.S.

REGISTERED PROFESSIONAL
ENGINEER
67149PE
FOR REVIEW
OREGON
JUN 14, 2005
KELLY A. GROVER

EXPIRES: 06/30/21
SIGNATURE DATE: _____
PRELIMINARY

DRAWN:		DESIGNED:	CHECKED:	 359 E. HISTORIC COLUMBIA RIVER HWY TROUTDALE, OREGON 97060 BUS: (503) 688-3737 • FAX: (503) 688-3788	FUNTIME RV, INC NE BEERS AVE & PROCTOR BLVD SANDY, OREGON 97055	DETAILS SHEET - SITE	C10 11
SCALE: AS SHOWN		DATE: JULY 23, 2019					
DATE:	NO.	REVISION		PROJECT NO. E17-060			



2-CARTRIDGE DEEP CATCHBASIN STORMFILTER DATA

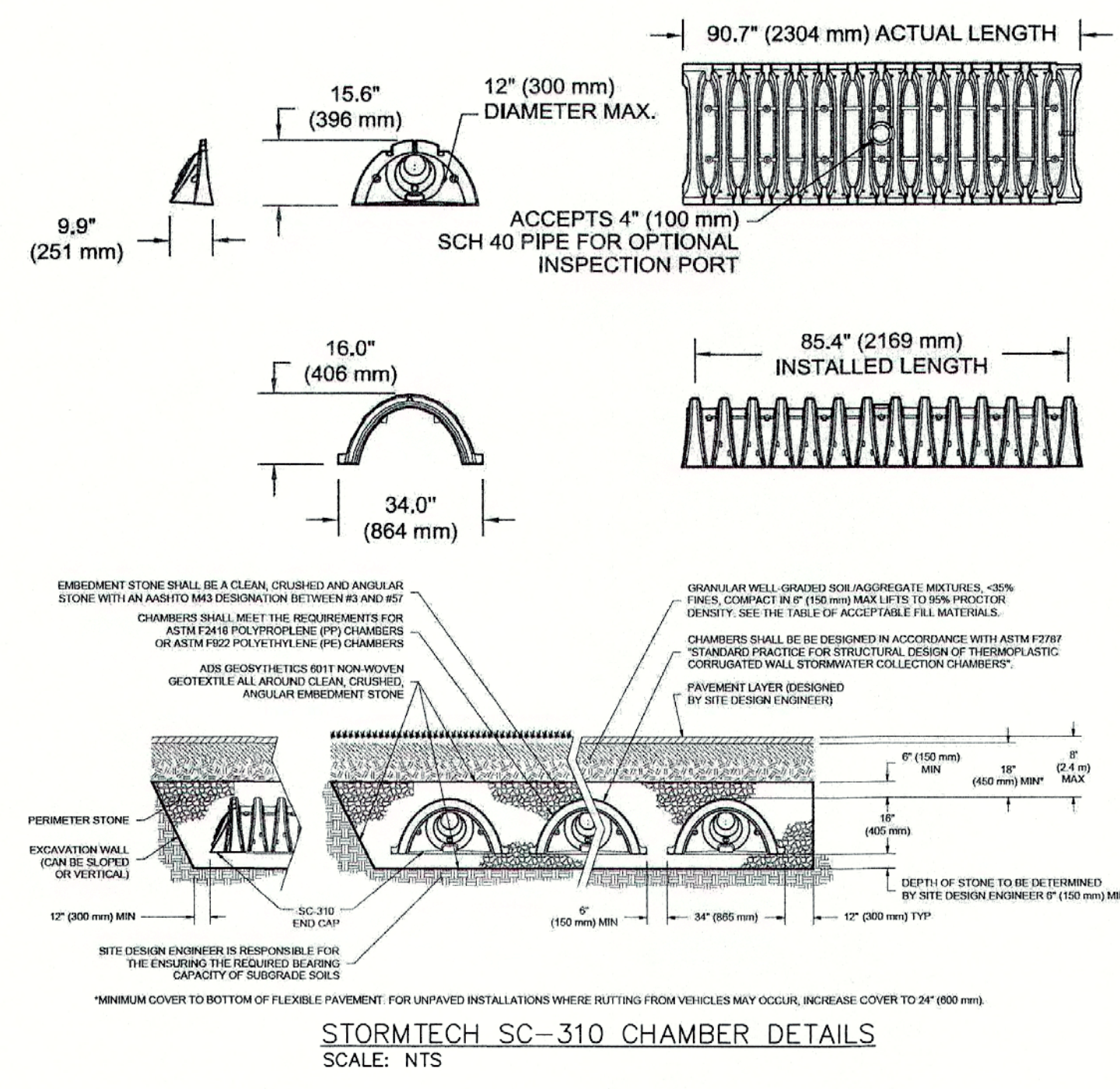
STRUCTURE ID	XXX
WATER QUALITY FLOW RATE (cfs)	X.XX
PEAK FLOW RATE (cfs)	X.XX
RETURN PERIOD OF PEAK FLOW (yrs)	XXX
CARTRIDGE FLOW RATE (gpm)	XX
MEDIA TYPE (PERLITE, ZPG, PSORB)	XXXXX
RIM ELEVATION	XXXXXX

PIPE DATA: I.E. DIAMETER
 INLET STUB: XXXXX" XX"
 OUTLET STUB: XXXXX" XX"

CONFIGURATIONS:

SLOPED LID: YES/NO
 SOLID COVER: YES/NO
 NOTES/SPECIAL REQUIREMENTS:

2 CARTRIDGE CATCHBASIN STORMFILTER STANDARD DETAIL



EXPIRES: 06/30/21
 SIGNATURE DATE: _____
 PRELIMINARY

DATE	NO.	REVISION

FDG
 Firwood Design Group, LLC
 SURVEYING + ENGINEERING + PLANNING

359 E. HISTORIC COLUMBIA RIVER HWY
 TROUTDALE, OREGON 97050
 BUS: (503) 668-3737 + FAX: (503) 668-3788

FUNTIME RV, INC
 NE BEERS AVE & PROCTOR BLVD
 SANDY, OREGON 97055

DETAILS SHEET - DETENTION FACILITY

C11
 11